

# BEAM PARK

## Transport Statement

Phase 2A Reserved Matters Application (Works within the London Borough of Havering) -  
Submission to the GLA

July 2019



**COUNTRYSIDE**  
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**MAYOR OF LONDON**

# **Countryside Properties PLC and Quadrant Housing Trust (L&Q)**

## **Beam Park Phase 2A Reserved Matters Planning Application**

### **Transport Statement**

**July 2019**

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**Figure 1        -    Site Location Plan**

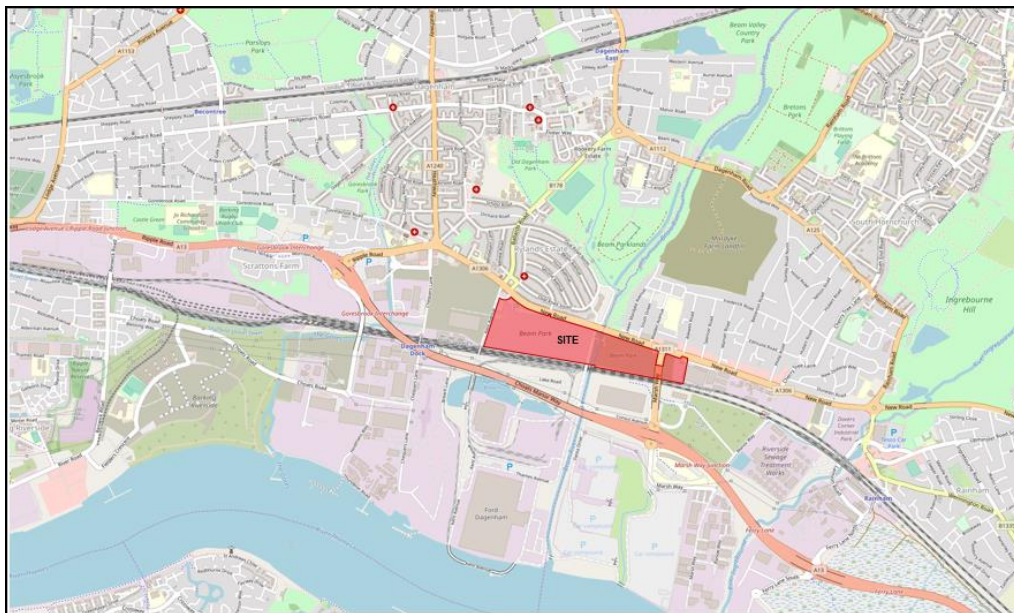
## **Appendices**

- Appendix A        -    Architects Site Layout Plan**
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# 1 INTRODUCTION

- 1.1 Vectos has been commissioned by Countryside Properties PLC (Countryside) and London and Quadrant Housing Trust (L&Q), (hereafter collectively referred to as the Applicants), to provide transport and traffic advice for the planning application associated with the residential led redevelopment of Phase 2A at Beam Park (formerly known as Assembly Plant Ford Motor Company Estate).
- 1.2 The Beam Park Site as a whole (all development phases) and herein referred to as the ‘Masterplan Site’ is primarily situated in the London Borough of Barking and Dagenham (LBBD) to the west, with the eastern remainder within the London Borough of Havering. Beam River runs from north to south through the centre of the Masterplan Site, separating the London Borough’s. The strategic location of the Masterplan Site is shown in **Figure 1**.

**Figure 1: Masterplan Site Development Location (All Phases)**



- 1.3 Phase 2A of the development (herein referred to as ‘the Site’ by which this application addresses) is bordered by Phases 1 and 2b of the Beam Park Masterplan Site to the east and west respectively; the A1306 New Road to the north; and the High Speed 1, London, Tilbury and Southend Railway lines to the south. Thames Avenue and Cortina Drive at present pass through the Site.
- 1.4 Consequently, due to the Masterplan Site crossing the administrative boundaries of two authorities’, two duplicate planning applications were submitted to each of the London

Borough's (planning references: 17/01307/OUT and P1242.17 respectively) in July 2017, with a number of revisions subsequently being made in February and August 2018.

- 1.5 Planning permission was granted on 07 February 2019 (LBBD Planning Ref: 17/01307/OUT, and LBH Planning Ref: P1242.17), for the following development:

Outline application:

*"Cross boundary hybrid planning application for the redevelopment of the site to include 3,000 residential units (50% affordable); two 3 form entry primary schools and nursery (Use Class D1); railway station; supporting uses including retail, healthcare, multi faith worship space, leisure, community uses and estate management space (Use Classes A1, A2, A3, A4, B1, D1 and D2); energy centres; open space with localised flood lighting; public realm with hard and soft landscaping; children's play space; flood compensation areas; car and cycle parking; highway works and site preparation/ enabling works."*

- 1.6 This Transport Statement (TS) supports the Reserved matters application relative to Phase 2A of the Beam Park development, connected to hybrid planning permission GLA 2933a/ LBBD 17/01307/OUT/ LBH P1242.17).

- 1.7 The proposed Reserved Matters Phase 2A application description is outlined below:

*"Reserved matters application relative to phase 2A of the Beam Park development connected to hybrid planning permission GLA 2933a/ LBBD 17/01307/OUT/ LBH P1242.17, seeking agreement to detailed site access, appearance, landscaping, layout and scale. It also seeks to address the terms of planning conditions 18 (levels), 19 (design code compliance), 27 (energy compliance) and 70 (Fire Strategy), which all necessitate submissions as part of any reserved matters application/s."*

- 1.8 It also seeks to illustrate the broad approach to address the terms of planning conditions: 10 (access to phases), 13 (accessibility to public realm), 15 and 16 (cycle parking), 23 (refuse and recycling), and 78 (parking) and details of blue badge spaces in respect to planning condition 11 (Accessibility and Management Plan- Residential).

- 1.9 Conditions 14 and 72 are part of the outline applications and therefore will be dealt with separately.

## Report Structure

1.10 The remainder of this report is structured as follows:

- Section 2 provides a review of relevant transport policies;
- Section 3 details the proposed development;
- Section 4 sets out the highway layout in detail including pedestrian and cycle provision, car park access, and access / servicing provision.
- Section 5 provides a summary and conclusions to this report.

## 2 POLICY REVIEW

- 2.1 This section presents a full review of relevant transport and land use planning policies within national and local guidance.

### National Policy

#### National Planning Policy Framework (NPPF) 2019

- 2.2 The National Planning Policy Framework (NPPF) was published by the Ministry of Housing, Communities and Local Government in February 2019 and replaces the previous version of the NPPF which was published in July 2018.
- 2.3 The NPPF sets out national policy for delivering sustainable growth and development in England and how these are expected to be applied. The three overarching objectives to achieve sustainable development outlined within the NPPF are detailed within Paragraph 8 and include:
- a) ***an economic objective*** – *to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure;*
  - a) ***a social objective*** – *to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering a well-designed and safe built environment, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural well-being; and*
  - b) ***an environmental objective*** – *to contribute to protecting and enhancing our natural, built and historic environment; including making effective use of land, helping to improve biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.*
- 2.4 Chapter 9 covers the promotion of 'Sustainable Transport' and Paragraph 105 states in relation to parking standards:



*"If setting local parking standards for residential and non-residential development, policies should take into account:*

- a) the accessibility of the development;*
- b) the type, mix and use of development;*
- c) the availability of and opportunities for public transport;*
- d) local car ownership levels; and*
- e) the need to ensure an adequate provision of spaces for charging plug-in and other ultra-low emission vehicles.*

2.5 It goes on in Paragraph 106 to state that *"Maximum parking standards for residential and non-residential development should only be set where there is a clear and compelling justification that they are necessary for managing the local road network, or for optimising the density of development in city and town centres and other locations that are well served by public transport (in accordance with chapter 11 of this Framework). In town centres, local authorities should seek to improve the quality of parking so that it is convenient, safe and secure, alongside measures to promote accessibility for pedestrians and cyclists."*

2.6 Within Paragraph 110 of Chapter 9 'Promoting Sustainable Transport' it is stated that all applications for developments should:

- "a) give priority first to pedestrian and cycle movements, both within the scheme and with neighbouring areas; and second – so far as possible – to facilitating access to high quality public transport, with layouts that maximise the catchment area for bus or other public transport services, and appropriate facilities that encourage public transport use;*
- b) address the needs of people with disabilities and reduced mobility in relation to all modes of transport;*
- c) create places that are safe, secure and attractive – which minimise the scope for conflicts between pedestrians, cyclists and vehicles, avoid unnecessary street clutter, and respond to local character and design standards;*
- d) allow for the efficient delivery of goods, and access by service and emergency vehicles; and*
- e) be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible and convenient locations"*

2.7 Lastly, the test of acceptability of a scheme is set out within Paragraph 109:

*“Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe”.*

## **Regional Policy**

### **The London Plan, 2016**

- 2.8 The London Plan, Spatial Development Strategy for Greater London was adopted in July 2011 and has been subject to several alterations since.
- 2.9 The Revised Early Minor Alterations to the London Plan was published in October 2013 which aimed to ensure that the London Plan is fully consistent with NPPF. Following this, The Draft Further Alterations to the London Plan was adopted in March 2015 to address key housing and employment issues emerging from analysis of Census 2011 data. In March 2016, the Mayor published the Housing Standards and the Parking Standards Minor Alterations to the London Plan (MALPs) to form the consolidated version. From this date, these alterations are operative as formal alterations to the London Plan and form part of the development plan for London.
- 2.10 The London Plan sets out an integrated economic, environmental, transport and social framework for the development of London over the next 20-25 years.
- 2.11 The London Plan is the overall strategic plan for London setting out the framework for development of London over the next 20-25 years. It is the strategic, London-wide policy context within which boroughs should set their detailed local plan policies.
- 2.12 Policy 6.1 states that:-
- “The Mayor will work with all relevant partners to encourage the closer integration of transport and development through the schemes and proposals shown in Table 6.1 and by encouraging patterns and nodes of development that reduce the need to travel, especially by car”.*
- 2.13 Policy 6.13 relates to parking and states that:

*“The Mayor wishes to see an appropriate balance being struck between promoting new development and preventing excessive car parking provision that can undermine cycling, walking and public transport use”*

- 2.14 It is recognised within paragraph 6.44 *“that developments should always include parking provision for disabled people. Despite improvements to public transport, some disabled people require the use of private cars.”*

#### London Riverside Designated Area 22

- 2.15 The Site forms part of the London Riverside Opportunity Area, the designated area 22 as part of the Current London Plan. The designated area comprises of 3,000 ha of land, with an indicative employment capacity of 16,000 jobs and a minimum of 26,500 new homes. Phase 2 of the Beam Park development also forms part of the Rainham and Beam Park Housing Zone, which extends north and east from Phase 1.

#### **The Mayor’s Transport Strategy (MTS) (2010)**

- 2.16 The Mayor’s Transport Strategy (MTS) was published in May 2010 and was developed in conjunction with the London Plan and the Economic Development Strategy as part of a strategic policy framework to support the development of London over the next 20 year period. The MTS outlines the Mayor’s vision and how TfL and its partners aim to achieve this.

- 2.17 The Mayor’s vision states that:

*“London’s transport system should excel among those of global cities, providing access to opportunities for all its people and enterprises, achieving the highest environmental standards and leading the world in its approach to tackling urban transport challenges of the 21st Century.”*

- 2.18 The MTS sets out six goals which are designed to achieve the vision. These are as follows:

- Support economic development and population growth
- Enhance the quality of life of all Londoners
- Improve the safety and security of all Londoners
- Improve transport opportunities for all Londoners
- Reduce transport’s contribution to climate change, and improve its resilience
- Support delivery of the London 2012 Olympic and Paralympic Games and its legacy

- 2.19 The MTS stresses the importance of integrating development with transport infrastructure and locating development in areas that are highly accessible to sustainable travel modes.

## **Local Policy**

### **The London Borough of Havering Core Strategy and Development Control Policies (2008)**

- 2.20 The London Borough of Havering's Core Strategy and Development Control Policies Plan was adopted in 2008 and is an important part of the Council's Local Development Framework, providing a benchmark for strategic decisions on new developments throughout the borough. It remains as the strategic policy statement for future development and land-use planning.

- 2.21 The transportation policies set in the Structure Plan relevant to this development are:

#### DC32 – The Road Network

- 2.22 Planning permission for new road schemes will only be allowed where they:

- *“Are consistent with the Council's road hierarchy*
- *Improve conditions for pedestrians and cyclists and disabled people by providing safe and convenient facilities*
- *Improve public transport accessibility*
- *Have net environmental benefits*
- *Improve safety for all users*
- *Contribute to regeneration objectives*
- *Are consistent with the Council's Local Implementation Plan and the Mayor of London's Transport Strategy*
- *Allocate street space in accordance with the London Plan Contributions may be sought from developers towards new road schemes or road improvements.”*

#### DC33 – Car Parking

- 2.23 Car parking provision within new developments should not exceed the maxima set out which are based on those provided in the London Plan. They should also provide Motor-cycle Parking and Disabled Parking to the standards. In applying these standards developers need to satisfy the following criteria in addition to those set out:

- *“For major commercial developments within town centres demonstrate that the level of car parking would not adversely affect that centre or a neighbouring centre’s vitality and viability*
- *for private off-street car parking achieve secure by design standard (or equivalent methodology)*
- *for development with material transport implications require a submission of a Transport Assessment with the planning application in line with the guidance in PPG13.”*

#### DC34 – Walking

2.24 In the design and location of access and circulation arrangements within, and between, development and local pedestrian destinations, developers will be required to:

- *“Take account of the needs of pedestrians*
- *address ‘desire lines’ to local shops, services and schools, including safer routes to school, and public transport nodes, lighting, rest facilities, safety and security, and barriers to local movement.”*

#### DC35 – Cycling

2.25 The design and layout of developments will be required to take account of the needs of cyclists by:

- *“Encouraging safe and secure cycle parking and changing and shower facilities to be provided appropriate to the nature and scale and location of the development*
- *encouraging the design and location of access and circulation arrangement of the development to take account of the needs of cyclists*
- *in major new development encouraging cycle priority measures which link with existing routes and networks*
- *where appropriate seeking contributions towards off-site improvements to the cycle network and cycle facilities including facilities at key public transport nodes and destinations and contributions towards the London Cycle Network Plus and Thames Chase Forest Circle.”*

## LBH Draft Local Plan (2016-2031);

- 2.26 The Havering Local Plan (2016-31) and supporting documents were submitted to the Secretary of State for the Ministry of Housing, Communities and Local Government for independent examination on 27 March 2018. This will in due course replace the Local Development Framework. The proposed modifications to policy have been reviewed as follows.

### PM3

*“The employment area to the south of Rainham and Beam Park will have retained its role as a strategically important industrial area and will have benefited from transformational new developments and an intensification of employment uses in a safe and secure environment.”*

### PM29

*“There are a number of development opportunities within public and private ownership. The GLA are an important landowner in the area and are bringing forward the Beam Park site for residential and mixed-use development with Persimmon Countryside Properties PLC.”*

### PM38

Source of Supply	Net additional homes 0-5 years	Net additional homes 5-10 years	Total 10-year supply
Large sites within the Rainham and Beam Park Strategic Development Area	(superseded 951) 1,011	(superseded 2,071) 2,329	(superseded 3,022) 3,340

### PM106

*“The Council’s approach to off street car parking standards is to ensure that parking is not over provided at destinations served by good public transport (maximum levels of provision), but to recognise and respect the decision many residents make to continue to own a car and ensure that adequate levels of off-street parking are provided in new residential development in areas with lower levels of accessibility to public transport. The Council considers this is justified and appropriate in Having because the borough is not well provided with public transport services for some journeys and there are no practical alternatives to making journeys by car.”*

## PM107

*“The Council's Residential Car Parking Standards Report (2017) draws together a range of evidence to help inform the car parking standards for Havering. The report demonstrates that a significant proportion of the borough has a PTAL of 0-2 reflecting the lack access to rail or underground stations. Havering also has one of the highest levels of car ownership which is above the average for outer London boroughs. Unsurprisingly, the highest levels of car ownership per household are in the areas with the poorest public transport accessibility. In accordance with the London Plan the Council considers it appropriate to set minimum car parking standards for those areas with the lowest levels of public transport accessibility (see Table 4 below). A lower standard may be appropriate for site with a PTAL of 2 within the Romford Strategic Development Area where this can be justified on a case by case basis.*

## **Summary**

- 2.27 In summary, there is strong support for the redevelopment of the Masterplan Site through the national, regional and local policies. There is strong backing as part of the Regional Policy, with the Site being contained within the London Riverside Designated Area 22, marked as a suitable area for employment and residential development.
- 2.28 The detailed proposed development amendments that are subject of this Reserved Matters Application are also supported by planning policy as they provide a layout that promotes walking, cycling and public transport whilst making appropriate provision for parking and servicing.

### 3 PROPOSED DEVELOPMENT

3.1 The Site is located to the centre of the Masterplan Site and to the east of the River Beam and Thames Avenue, within the boundary of LBH. The Site will provide only residential accommodation, with associated highways, parking and servicing arrangements.

3.2 The architects Site Layout Plan is provided at **Appendix A**.

3.3 The full breakdown of land uses and floor areas provided within the Site are as follows:

- Residential: **184 Units** within the LBH Phase 2A RMA; (Buildings I, 16, 13, and T)

3.4 The breakdown of residential units outlined in **Table 3.1** below:

**Table 3.1: Residential Tenure Mix**

	1 Bed	2 Bed	3 Beds	4 Beds	Total
<b>Number of Units</b>	54	60	46	24	<b>184</b>

3.5 It should be noted that the proposed 184 units provided a net reduction of 10 units for Phase 2A compared to the outline permission.

3.6 Units are further delegated into a mixture of private and affordable apartments (Buildings T and I) and housing (Buildings 13 and 16). Further details of the Tenure and mix can be located within the Patel Taylor Sketchbook.

3.7 The Spine Road, which forms the principal road within the Masterplan Site, runs through the Site.

3.8 The layout of The Spine and internal roads are detailed in Section Four of this report.

#### **Car Parking**

3.9 Condition 78 of the Outline Consent sets out the maximum parking provision across the Masterplan Site. For residential parking, a maximum of 1,314 spaces across the Masterplan Site, including 98 visitor spaces and a minimum of 10 car club spaces is permitted.



- 3.10 The residential parking ratio for Phase 2A is 0.77 spaces per unit, with a total of 142 parking spaces, including visitors. From the Phase 2A total parking allocation, 7 spaces will be allocated as accessible spaces, meeting the minimum of 3% provision from the total parking spaces (5 spaces) for Phase 2A. If required, there is potential to meet 10% disabled persons car park provision (18 spaces) by either amending the landscape amenity or by combining spaces to provide extra disabled spaces.
- 3.11 Total parking allowances are in accordance with the Current London Plan (2016) and Havering Local Plan Policy DC33. In addition, 1 car club parking space will be provided. Active and passive Electric Vehicle Charging Points (EVCPs) will be provided in accordance with the London Plan (2016) requirements.
- 3.12 Parking will be provided at all 4 plots of the Phase 2A Site. All of the residential parking spaces will be in the form of driveways, or clearly delineated, marked and signed on-street perpendicular spaces, adjacent to the carriageway. This comprises of predominately on plot private parking and managed private parking through resident permits.
- 3.13 There will be visitor car parking across the Site, predominately on-street although some will be on-plot. The hours of operation are to be confirmed, but it is expected that the visitor car parking spaces would be available to those without a residential parking permit during the daytime only, and those with permits able to park in the 'visitor' spaces at any time.
- 3.14 There will also be pay and display on-street parking with visitor restrictions to be confirmed.
- 3.15 As illustrated by the architect's car parking layout plan at **Appendix B**, Plot T will consist of 16 on-plot managed (resident permit) private parking. Following this, Plot 13 will comprise of private house parking within the private road to the centre of the plot, and driveways to the eastern and western perimeter. There will be further off plot managed (residents permit) private parking to the north, west and south of Plot 13, and dedicated visitor spaces to the south-west. This will provide a total of 61 spaces located within and surrounding Plot 13. Plot 16 will follow a similar format to that of Plot 13 excluding access via a private road, with a total of 43 spaces. Lastly, Plot I will provide 22 spaces through a mixture of secured private spaces and managed private (resident permit) spaces.
- 3.16 A Car Parking Management Plan will be conditioned separately under Condition 14 on occupation of the units.

## Cycle Parking

- 3.17 The London Plan (2016) sets out the Cycle Parking requirements for developments in London. A summary of these standards for each of the proposed uses are provided in **Table 3.2** below.

**Table 3.2: Adopted London Plan Cycle Parking Standards**

Unit Type	Proposed Units	Long Stay		Short Stay	
		Standard (1 space per...)	Requirement	Standard (1 space per...)	Requirement
1 Bed	54 units	1 space per 1 Bed	54	40 units	1
2+ Bedroom Units	130 units	2 spaces per all other dwellings	260	40 units	3
<b>TOTAL</b>	<b>184 units</b>	<b>314</b>		<b>4</b>	

- 3.18 As detailed in the table above, the proposed development requires a minimum of 314 long-stay and 4 short-stay cycle parking spaces, equating to a total of 318 cycle parking spaces.
- 3.19 In accordance with these standards, a total of 350 cycle parking spaces will be provided. 341 residential long stay cycle parking spaces and 9 visitor short stay spaces will be allocated from this total.
- 3.20 Cycle parking for residents at the Site will be provided in secure bike stores at Plots I (135 spaces) and T (76 spaces). Plots 13 and 16 will provide 130 cycle parking spaces in the back and front gardens for residents, whilst visitor parking will be provided within the public realm in accessible areas by plots I and T. The architect's Cycle Parking Plan has been provided at **Appendix C**.
- 3.21 A Cycle Parking Management Plan will be conditioned separately on occupation of the units under condition 14.

## **4 HIGHWAY LAYOUT**

- 4.1 This Section of the TS sets out the detailed strategy for all modes of travel within the Site.

### **Highway Layout**

- 4.2 The layout of the Spine Road and the internal highways layout remain largely as the approved outline planning consent. These details are set out below.

### **Junction of the Spine Road and Highways Layout**

- 4.3 Vehicular access into Beam Park for Phase 2A will be via the proposed Phase 1 junction in the north of the development running south off A1306 New Road, east of the Thames Avenue existing access and the Beam Park River.
- 4.4 Via the Phase 1 access junction, vehicles can head directly south via one road running to the southern perimeter of the Site, providing access to Buildings 16, I and the Mews. This road predominately lies within Phase 1 of the development to the eastern border of the Site.
- 4.5 Alternatively, drivers can head south towards the first junction, whereby the Spine Road lies to the west, south of Building T. This Spine Road runs across the northern quarter of the Site. From the Spine Road, the layout further divides into secondary and tertiary roads in a square formation. From here, drivers can access Building 13 via the secondary road running south in the western quarter of the Site, and Buildings 16 and I to the south and south-east.
- 4.6 The Spine Road meets further secondary roads to the east of the Site at the junction south of the Phase 1 access, therefore allowing vehicles to route from the Site into the Masterplan Site within the jurisdiction of LBH. This will provide connections to Phase 1 of the development (directly east of Phase 2A), whereby rail facilities are provided.
- 4.7 From the south of the development by Plot 16, vehicles are directed south-westerly out of the Site passing beneath Thames Avenue, and back into the wider Masterplan Site, whereby the road layout provides access to the future phases of the wider development within the jurisdiction of LBBD.
- 4.8 The main spine route through the Site has been designed to be 5.5m wide to accommodate large servicing vehicles, whilst providing sufficient space for cyclists in line with the London Cycling Design Guidance (LCDS).

- 4.9 The junction layout proposed seeks to incorporate more placemaking principles into the highway layout by removing the road markings and using different surface materials. There will be both horizontal and vertical deflections, such as raised tables, across the Site to reduce vehicle speeds and improve the environment for pedestrians and cyclists.
- 4.10 This change in environment will inform drivers that they are in a more active area of the Site with a mix of uses and other road users including pedestrians and cyclists, thereby promoting lower vehicle speeds and an environment that is not 'highway dominated'. Pedestrian and cyclist safety are of the utmost importance particularly given the presence of two primary schools and a nursery within the wider Site development. This is reflected in the ratio of parking provided also, as not to detriment the public realm.
- 4.11 Refer to Brand Consulting Engineers technical drawings for swept path analysis drawings and further details surrounding the highways layout.
- 4.12 **Appendix A** provides an overview illustration of the highway's layout. However, please refer to Brand Consulting Engineers technical drawings for swept path analysis drawings and for further details surrounding the Highways Layout Plan.

### **Pedestrians**

- 4.13 The main pedestrian access points into the Masterplan Site will be via New Road and Kent Avenue. Separate pedestrian provision will be alongside all vehicular access roads on both sides throughout the Site, with formalised footways.
- 4.14 Intermittent crossing points will be provided within the Site to assist pedestrians to cross the roads. Within the Site, pedestrians will be segregated from vehicles by barriers. Where possible, the internal layout has been designed to separate pedestrians and vehicles in the interest of placemaking and safety.
- 4.15 The design provides a pedestrian bridge across the River Beam, creating connections from the Site to the wider Masterplan Site to the west of the development.
- 4.16 There are also discussions as to future pedestrian connections with the land beneath Thames Avenue, which could potentially increase routes to Dagenham Dock Station and an informal pedestrian connection between Beam Park station and the Masterplan site.

## **Cyclists**

- 4.17 Cyclists will enter the Site via the priority junction new Site access, to the east of Thames Avenue and Building T within Phase 1 of the Masterplan Site.
- 4.18 Due to the development use being primarily residential, it is expected that the speed limit of The Spine at 30mph and the other connecting internal roads throughout the Site shall be safe and an acceptable speed limit for cyclists to share the road with other vehicles.
- 4.19 In addition, cyclists will also be able to utilise the pedestrian pathways within the Beam River perimeter.
- 4.20 The majority of the internal roads have low movement functions and have been designed to reflect this with minimum carriageway widths, adhering to the London Cycling Design Guidance (LCDS).

## **Car Park Access**

- 4.21 As set out in Section 3, parking will be provided for all 4 plots of the Phase 2A site, with further parking on-street.
- 4.22 The first car park is located to the east of Thames Avenue via the external access junction in the northern quarter of the development. Plot T provides off-street parking within a dedicated car park directly to the west of the building, at surface level. Drivers access this parking area via an informal junction south of the access point.
- 4.23 Directly south, Plot 13 can be located via secondary roads running south of the development from The Spine, whereby on plot driveways for private house parking are provided for residents to the east and west perimeter of the plot. Further managed private (resident permit) parking are also provided off plot to the west and south of Plot 13.
- 4.24 To the south of Plot 13 further residential parking can be accessed via tertiary, secondary roads and The Mews surrounding Plots 16 and I. Parking is at surface level surrounding the entire perimeter of the Plot 16 and to the north, south and west of Plot I.
- 4.25 As advised, the parking layout can be found at **Appendix B**.

## **Servicing and Refuse**

- 4.26 Servicing and loading would be undertaken from within the carriageway.
- 4.27 Household waste will be collected by LBH services, whereby bin stores will be provided within Plots 13, T and I at ground level. Bin stores will be appropriately sized to accommodate sufficient waste storage for the number of dwellings.
- 4.28 The refuse strategy will ensure that pulling distances for refuse vehicle operators are no more than 25 metres. The waste storage locations will be strategically placed to ensure no requirement for refuse vehicles to reverse. However, in instances where this is not possible, the maximum reversing distance will be 12 metres. The collection point will be at Plot 13. This is demonstrated within the refuse plan included at **Appendix D**.
- 4.29 The street design in terms of width and appropriate provision for loading has been undertaken in accordance with Manual for Streets and is therefore suitable for all servicing, delivery and emergency vehicle access.
- 4.30 For further details regarding refuse and servicing including Swept path analysis drawings please refer to Brand Consulting Engineers drawings submitted with this application.
- 4.31 A Waste Management Plan will be issued on occupation of the units and will be secured by a suitably worded condition.

## 5 SUMMARY AND CONCLUSIONS

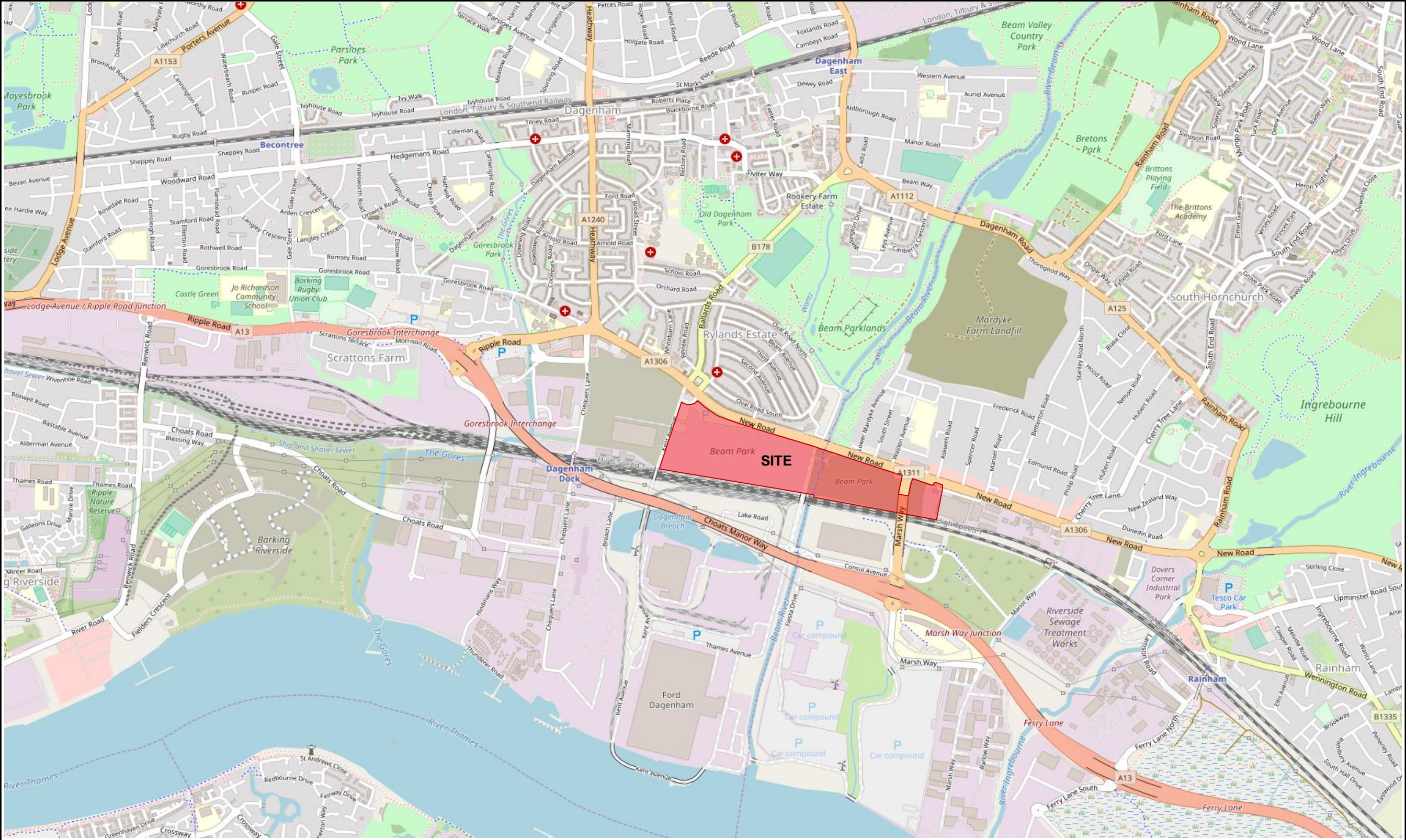
- 5.1 Vectos has been commissioned by Countryside Properties PLC (Countryside) and London and Quadrant Housing Trust (L&Q), (hereafter collectively referred to as the Applicants), to provide transport and traffic advice for the planning application associated with the residential led redevelopment of Phase 2A at Beam Park (formerly known as Assembly Plant Ford Motor Company Estate).
- 5.2 Following the granting of outline planning permission, Reserved Matters planning permission is now being put forward for Phase 2A (LBH Planning Ref: P1242.17) of the wider Beam Park development. The reserved matters includes details of access, layout, scale and design within the Site including key junctions and pedestrian and cycle provision, seeking to illustrate the broad approach to address the terms of planning conditions in the outline planning permission.
- 5.3 The Site will consist of residential uses on 4 plots in a square formation.
- 5.4 The geometry of the Spine Road has been refined to reduce the overall footprint of the junction and carriageway width on approach. This has been designed to promote lower vehicle speeds and increased driver awareness to other road users.
- 5.5 Within the Site, the central park and green spaces surrounding the plots has been designed to provide significant space for pedestrian movements and activity. Pedestrian movement has also been at the forefront of design within the Beam River Park, which lies in the Phase 2A boundary (centre Site). A pedestrian bridge connects Phase 2A of the development across the river to the west of the development, allowing for safe and easy access for pedestrians across the phases of the masterplan Site. There are also a multitude of open space and 0-11yr Play Areas within the Beam River Park area.
- 5.6 Four car parks are proposed as part of the Site. This comprises of residential parking only, located on each plot at surface level, with a mixture of driveways and separated parking spaces. Parking is accessed via the Site access to the north, and The Spine and further tertiary roads running in a square formation further south of the development.
- 5.7 Total car and cycle parking have been provided in accordance with the London Plan (2016) requirements.


- 5.8 Servicing and Refuse will predominantly be undertaken from the Phase 1 Site access road that runs north to south to the east of Phase 2A. The collection point for LBH authorities will be at Plot 13, whereby servicing vehicles will not be required to reverse. Further details and drawings regarding Refuse and Servicing are available through Brand Consulting Engineers.
- 5.9 The design and layout of the Site are considered to provide a high-quality environment that provides excellent facilities for pedestrians, cyclists whilst also providing safe and suitable access for all vehicles including parking and servicing.
- 5.10 It is therefore considered that the proposals accord with relevant planning policies and there are no transport or highways reasons why the reserved matters application should not be granted planning permission.



## FIGURES



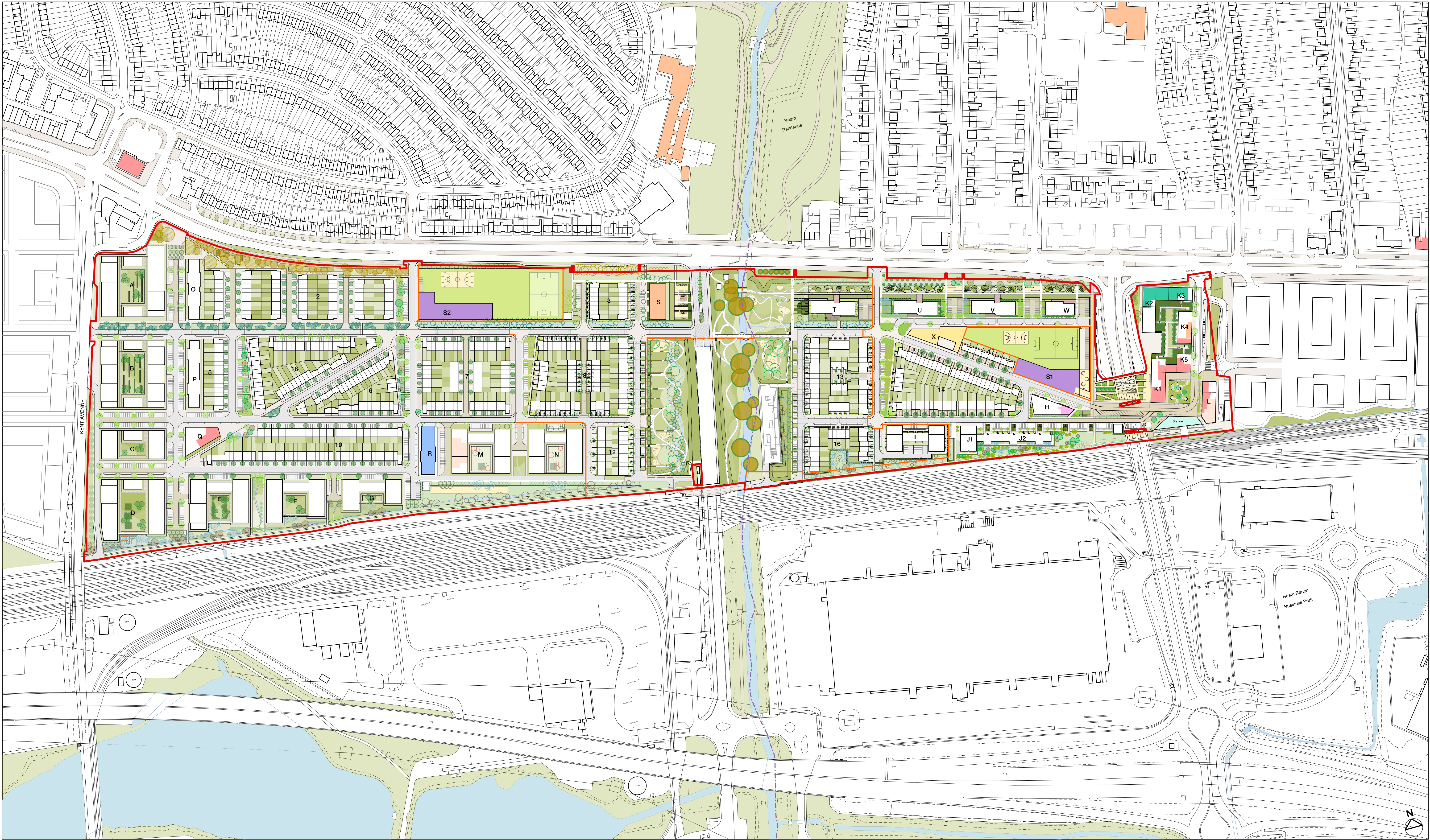


<div>Key:</div> <div><div></div><div>Masterplan Site Location</div></div>	Beam Park, Dagenham				Countryside Properties PLC and Quadrant Housing Trust (L&Q)	
	Strategic Masterplan Site Location				<div></div> <div>Network Building, 97 Tottenham Court Road, London W1T 4TP Tel: 020 7580 7373 Email: london@vectos.co.uk www.vectos.co.uk</div>	
	DRAWN: EG	CHECKED: AP	DATE: 15/03/2019	SCALES: NTS		DRAWING REFERENCE: Figure 1



## **APPENDIX A**





**General Notes**

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Contractors must ensure that cross referenced drawings and specifications noted on these drawings are checked on a regular basis to ensure that the latest revisions are used.

**Key**

- Consented planning application boundary
- Borough boundary
- Phase 2 boundary (2a LBH area, 2b LBBD area)
- Phase 2A GLA Reserved Matters Application boundary

**Colour Code**

Leisure Use	Nursery
Commercial Areas	School
Medical Centre	Station
Managment Office	Energy Centre
Community Space	

**Key plan - Phase 2A RMA application**

**Client**

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The Drive, Great Warley  
Brentwood CM13 3AT  
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**Architect**

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**Title**

Illustrative masterplan  
GLA Phase 2A  
Reserved Matters Application

**Project**

Beam Park

**Scale**

1:2000 @ A1 1:4000 @ A3

**Issue Record**

PL10 For Information

**By**

TS SR 26.07.2019

**Drawing Number**

448-PT-MP-PL-1101

**Revision**

PL10

**Status**

For Information

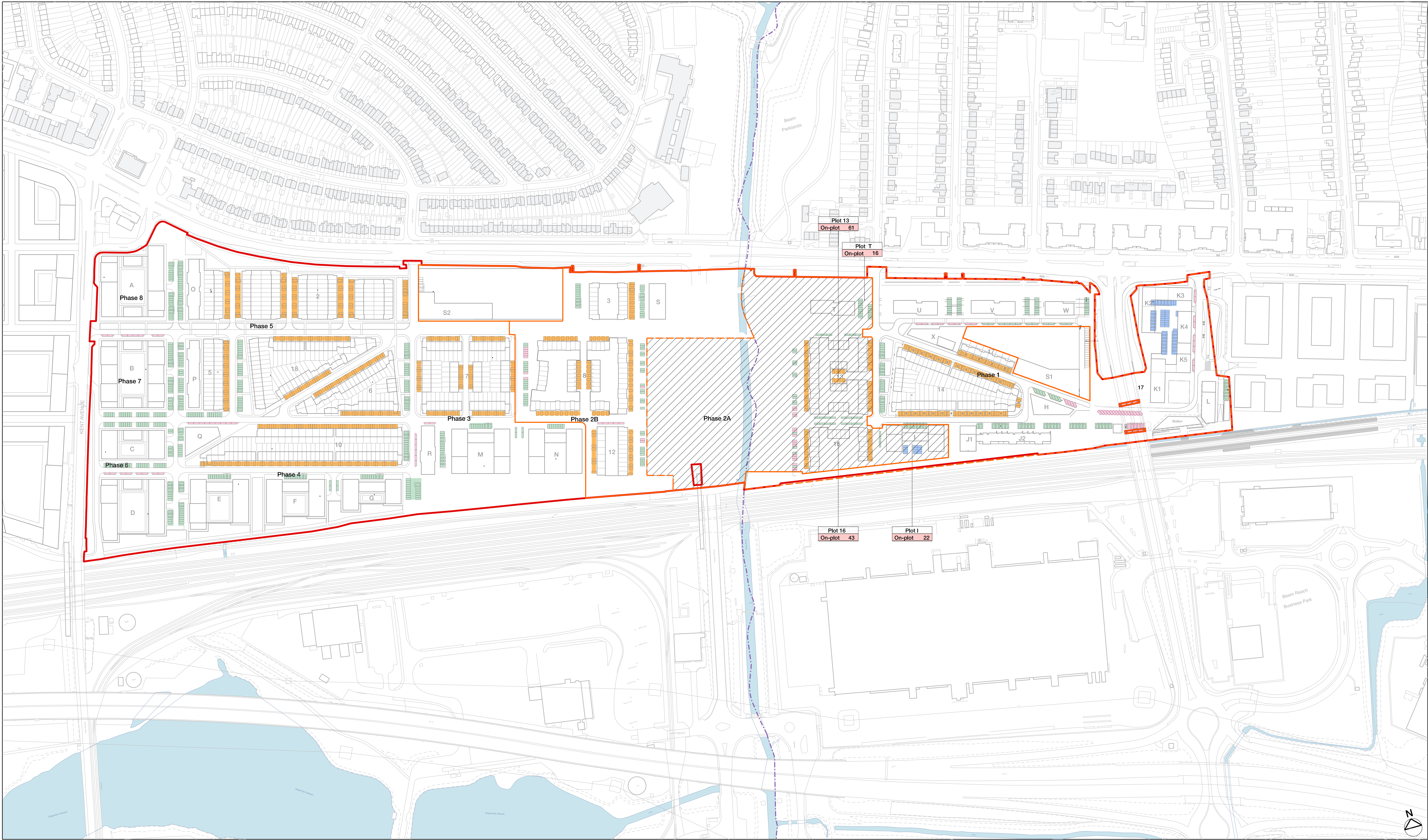
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## **APPENDIX B**





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**Key**

Consented planning application boundary

Borough boundary

Phase 2 boundary (2a LBH area, 2b LBBD area)

Phase 2A GLA Reserved Matters Application boundary

**Key**

A Apartment development zone letter

11 House development zone number

S1 S2 School development zone alphanumeric code

XX:XX Plot: Number of on-street parking spaces

Plot X	Plot
On-Plot XX	Number of on-plot parking spaces

**Colour code**

Private house parking

Visitor - Restrictions TBC

Secured private (apts.)

Managed private (residents permit)

On-street parking - Station

Additional spaces

**Scale 1:2000 @ A1**

0 20m 50m 100m 150m

**Client**

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**Title**

Masterplan car parking provision  
GLA Phase 2A  
Reserved Matters Application  
(All built development within LBH)

**Project**

Beam Park

**Scale**

1:2000 @ A1 1:4000 @ A3

**Issue Record**

PL7	For Information	By	Chk	Date
EP	SR			26.07.2019

**Drawing Number**

448-PT-MP-PL-1114

**Revision**

PL7

**Status**

For Information

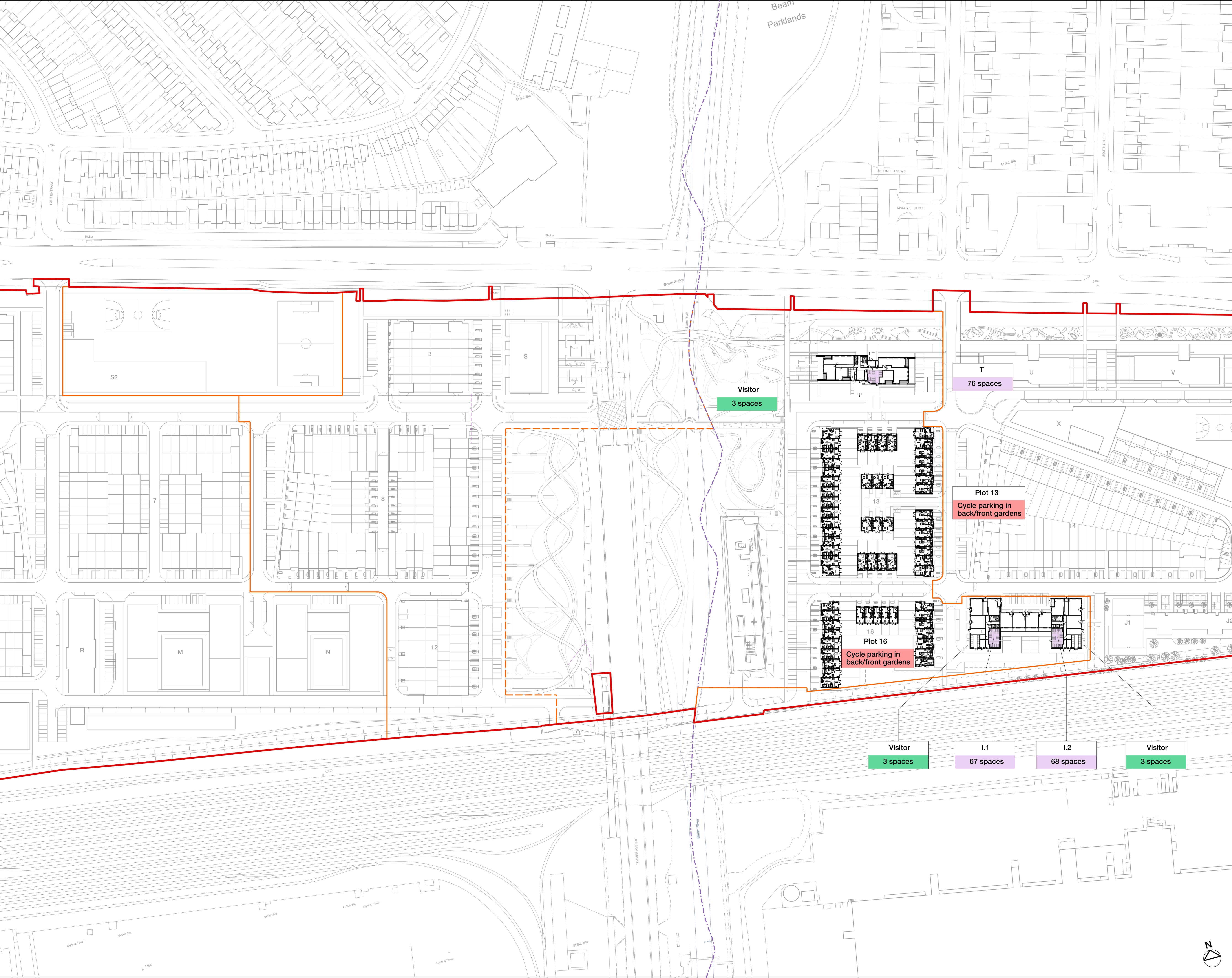
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## APPENDIX C



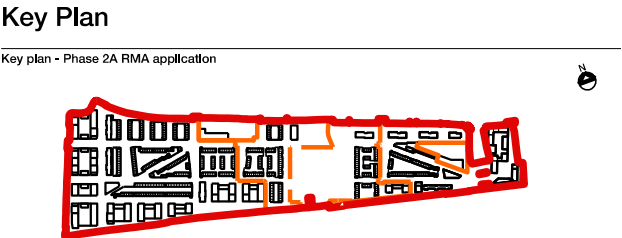


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- Key**
- Consented planning application boundary
  - Borough boundary
  - Phase 2 boundary (2a LBH area, 2b LBBD area)
  - Phase 2A GLA Reserved Matters Application boundary

**Drawing Notes**

For building specific layouts refer to the individual building / plot plans.



**Issue Record** By Chk Date

PL1 For Information SDM TS 26.07.2019

**Title**  
Landscape plan  
Cycle parking  
GLA Phase 2A RMA

**Project**  
Beam Park

**Scale**  
1:1000 @ A1 1:2000 @ A3

**Status**  
For Information

**Drawing Number** 448-PTA-LA-00-DR-A-2233 **Revision** PL1

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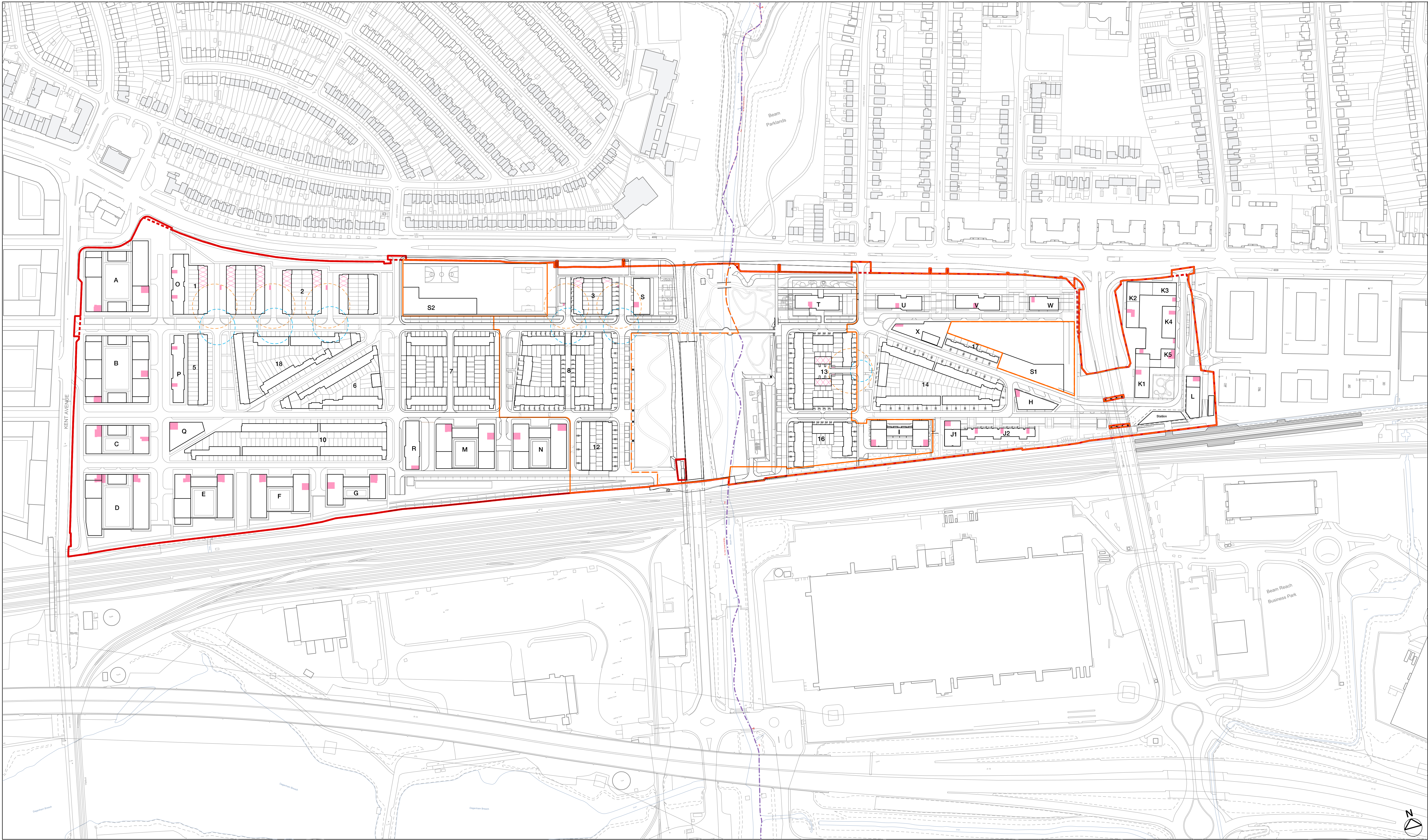


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## **APPENDIX D**





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Contractors must ensure that cross referenced drawings and specifications noted on these drawings are checked on a regular basis to ensure that the latest revisions are used.

**Key**

- Consented planning application boundary
- Borough boundary
- Phase 2 boundary (2a LBH area, 2b LBBD area)
- Phase 2A GLA Reserved Matters Application boundary

**Key**

- Refuse areas
- Houses served by refuse area
- Refuse vehicle reverse distance (20m LBBD, 12m LBH)
- 25m Refuse collection distance

**Scale 1:2000**

0 20m 50m 100m 150m

**Client**

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**Title**

Refuse locations  
GLA Phase 2A  
Reserved Matters Application  
(All built development within LBH)

**Project**

Beam Park

**Scale**

1:2000 @ A1 1:4000 @ A3

**Issue Record**

PL4	For Information	By	Chk	Date
EP	SR			26.07.2019

**Drawing Number**

448-PT-MP-PL-1116

**Revision**

PL4

**Status**

For Information

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