

BEAM PARK

Delivery and Servicing Plan

Phase 2A Reserved Matters Application (Works within the London Borough of Havering) -
Submission to the GLA

July 2019



COUNTRYSIDE
Places People Love



SUPPORTED BY
MAYOR OF LONDON

Countryside Properties PLC and Quadrant Housing Trust (L&Q)

Beam Park Phase 2a Reserved Matters Planning Application

Delivery & Servicing Management Plan

July 2019

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1 INTRODUCTION

1.1 Vectos is retained by Countryside Properties PLC and Quadrant Housing Trust (L&Q) as joint applicants to provide traffic and transport advice relating to their proposed redevelopment of Phase 2a of the Beam Park Site (formerly known as Assembly Plant Ford Motor Company Estate).

1.2 The Beam Park Site as a whole (all development phases) and herein referred to as the 'Masterplan Site' is primarily situated in the London Borough of Barking and Dagenham (LBBD) to the west, with the eastern remainder within the London Borough of Havering. Beam River runs from north to south through the centre of the Masterplan Site, separating the London Borough's. The strategic location of the Masterplan Site is included at **Figure 1**.

1.3 GLA approved application for the redevelopment of the Beam Park site on 07th February 2019 for:

Outline application:

"Cross boundary hybrid planning application for the redevelopment of the site to include 3,000 residential units (50% affordable); two 3 form entry primary schools and nursery (Use Class D1); railway station; supporting uses including retail, healthcare, multi faith worship space, leisure, community uses and estate management space (Use Classes A1, A2, A3, A4, B1, D1 and D2); energy centres; open space with localised flood lighting; public realm with hard and soft landscaping; children's play space; flood compensation areas; car and cycle parking; highway works and site preparation/ enabling works."

1.4 This report has therefore been prepared to support the submission of the Reserved matters application relative to phase 2A of the Beam Park development (herein referred to as 'the Site', connected to hybrid planning permission GLA 2933a/ LBBD17/01307/OUT/ LBH P1242.17).

1.5 The proposed Reserved Matters Phase 2a application description is outlined below:

"Reserved matters application relative to phase 2A of the Beam Park development connected to hybrid planning permission GLA 2933a/ LBBD 17/01307/OUT/ LBH P1242.17, seeking agreement to detailed site access, appearance, landscaping, layout and scale. It also seeks to

address the terms of planning conditions 18 (levels), 19 (design code compliance), 27 (energy compliance) and 70 (Fire Strategy), which all necessitate submissions as part of any reserved matters application/s.”

- 1.6 It also seeks to illustrate the broad approach to address the terms of planning conditions: 10 (access to phases), 11 (accessibility plan), 13 (accessibility to public realm), 14 (car park management), 15 and 16 (cycle parking), 23 (refuse and recycling), 73 (delivery and servicing plan) and 78 (parking).
- 1.7 Conditions 14 and 73 are part of the outline applications and therefore will be dealt with separately.
- 1.8 This Delivery and Servicing Management Plan (DSMP) has been produced to accompany the new planning application. It should be noted that this document is an evolving document and will continue to be developed as the operational aspects of the development are progressed.
- 1.9 The plan will manage deliveries and servicing to the premises in order to ensure the successful operation of the servicing (including refuse storage and collection) for all elements of the scheme. Effective management will ensure that the potential for vehicle conflicts is avoided and that the proposals have the minimum impact on both the surrounding highway and pedestrian network.

2 OBJECTIVES

What is a Delivery and Servicing Plan?

- 2.1 DSPs provide a framework for managing all types of freight vehicle movement to and from individual developments.
- 2.2 DSPs are one of the four measures to improve freight and servicing. The other measures include the Freight Operator Recognition Scheme (FORS), and Construction Logistics Plans (CLP).

Benefits of a Delivery and Servicing Plan

- 2.3 The 'Managing Freight Effectively: Delivery and Servicing Plans' document identifies the benefits of DSPs to local authorities, residents, building developers, businesses and freight operators.
- 2.4 In summary, DSPs will:
 - Help developers and local authority planning officials comply with:
 - The National Planning Policy Framework (NPPF); and,
 - The Traffic Management Act and any borough specific policies, such as road safety and air quality action plans.
 - Demonstrate that goods and services can be delivered, and waste removed in a safe, efficient and environmentally friendly way;
 - Identify deliveries that could be reduced, re-timed or even consolidated, particularly during busy periods;
 - Help cut congestion and ease pressure on the environment;
 - Improve the reliability of deliveries to the site concerned;
 - Reduce the operating costs of building occupants and freight companies; and
 - Reduce the impact of freight upon local residents.

Delivery and Servicing Plan Objectives

2.5 The overall objective of this DSP is:

To minimise the impacts of freight movements and facilitate sustainable freight travel to and from the proposed development.

2.6 To support the realisation of this overarching objective, several sub-objectives have been set out, and include:

- Promoting smarter operations that reduce the need for freight travel overall or that reduce or eliminate trips particularly in peak periods;
- Encouraging greater use of sustainable freight modes;
- Encouraging the use of greener vehicles;
- Managing the on-going development and delivery of the DSP;
- Communication of the site servicing/delivery facilities (through dissemination of information) to staff and suppliers;
- Communication of the DSP and its constituent measures to site occupiers; and,
- Encouraging the most efficient use of freight vehicles and servicing/delivery trips.

3 PLANNING POLICY

DSP Guidance

Managing Freight Effectively: Delivery and Servicing Plans (DSPs)

- 3.1 DSPs provide a framework to better manage all types of freight vehicle movement to and from individual buildings. A DSP is similar to that of a residential travel plan but focuses on the sustainable movement of freight as opposed to residents.
- 3.2 DSPs will improve the safety, efficiency and reliability of deliveries and increase building operational efficiency by reducing delivery and servicing impacts to premises, especially CO2 emissions, congestion and collisions.
- 3.3 DSPs aim to reduce delivery trips, particularly during peak periods, and increase availability and use of safe and legal loading facilities, using a range of approaches including consolidation and out-of-hours deliveries. DSPs will also identify unnecessary journeys and deliveries that could be made by more sustainable modes to help reduce congestion and minimise the environmental impact of freight activity.
- 3.4 The document outlines the benefits of DSPs to local authorities and residents, building developers, businesses and freight operators, including:

Local Authorities and Residents

- Less congestion on local roads;
- Reduced emissions, and use of more sustainable modes where possible, to contribute towards CO2 reduction targets;
- Fewer goods vehicle journeys lowering the risk of collisions;
- Opportunity to reduce parking enforcement activity costs – more deliveries will use legal loading facilities, so less traffic and parking infringements should occur; and,
- Improved quality of life for local residents through reduced noise and intrusion and lower risk of accidents.

Building Developers and Businesses

- Reduced delivery costs and improved security;
- More reliable deliveries resulting in less disruption to normal business practices;
- Time-savings by identifying unnecessary deliveries;
- Less noise and intrusion; and,
- Opportunity to feed into a Corporate Social Responsibility programme and ensure operations comply with health and safety legislation.

Freight Operators

- Legal loading areas will mean less risk of receiving penalty charge notices;
- Fuel savings through reduced, re-timed or consolidated deliveries; and
- More certainty over delivery times.

Freight Operators Recognition Scheme (FORS)

- 3.5 FORS is a unique, industry-led, free membership scheme to help van and lorry operators in the Capital become safer, more efficient and more environmentally friendly.
- 3.6 The FORS has three membership levels; Bronze, Silver and Gold. Bronze members must meet the following requirements:
- Drivers and vehicle management;
 - Vehicle maintenance and fleet management;
 - Transport operations; and,
 - Assessing the performance of company policies.
- 3.7 Silver and Gold level members need to provide data to enable benchmarked values to be produced per million kilometres for each type of vehicle for:
- Fuel use;
 - CO² and emissions;
 - Vehicle incidents; and,
 - Penalty Charge notice and fines

4 SERVICING & REFUSE COLLECTION ARRANGEMENT

- 4.1 As per the consented scheme, all deliveries and servicing will be undertaken within the site, as will refuse collection.
- 4.2 Buildings 13, T and I will be provided with dedicated bin stores at ground floor level. Bin stores will be appropriately sized to accommodate sufficient waste storage for the number of dwellings. On collection days, refuse collection operatives will collect bins directly from the stores, which will be provided with doors opening towards the internal access road at Plot 13. This is demonstrated within the architect's plan included within **Appendix A**.
- 4.3 The refuse strategy will ensure that pulling distances for refuse vehicle operators are no more than 25 metres. The waste storage locations will be strategically placed to ensure no requirement for refuse vehicles to reverse. However, in instances where this is not possible, the maximum reversing distance will be 12 metres.
- 4.4 For further details regarding refuse including Swept path analysis drawings demonstrating how a refuse vehicle will access the site to collect refuse and subsequently exit in a forward gear, please refer to Brand Consulting Engineers technical drawings.
- 4.5 There will be no loading bays. However, a dedicated servicing area will be located via the road between Building I and J1, and the road south of Building I. These areas will be used to undertake deliveries to the residential units.
- 4.6 The street design in terms of width and appropriate provision for loading has been undertaken in accordance with Manual for Streets and is therefore suitable for all servicing, delivery and emergency vehicle access.
- 4.7 For further details regarding refuse and servicing including Swept path analysis drawings please refer to Brand Consulting Engineers technical drawings.

5 KEY FEATURES OF THE SERVICING MANAGEMENT PLAN

5.1 On-site management will be employed to oversee the operation of the site including servicing and deliveries to ensure its smooth operation.

5.2 In order to meet the key objectives of the plan (i.e. to minimise service vehicle / car conflict and safeguard the amenity of pedestrians), the following initiatives will be adopted:

- On-site management team will be employed to manage servicing activity at the site;
- All commercial operators will be provided with written/emailed instructions on how to book deliveries and the procedures to be adopted;
- Residents will be encouraged to arrange the use of cars and transit vehicles to deliver goods where possible;
- The management team will ensure that deliveries remain in the vicinity of the site for as little time as required and that vehicle engines are switched off while stationary (where possible);
- The management team will have the ability to receive goods from courier deliveries (i.e. online retailers such as Amazon) if a resident is not available to minimise the number of repeat trips on the network; and
- The on-site management team will seek to minimise, where possible, deliveries during the peak hours.

6 MONITORING & REVIEW

6.1 The on-site management team will maintain a record of servicing activity which will include the following information:

- Day
- Date
- Delivery Arrival / Departure Time
- Type of Vehicle
- Goods Delivered
- Any other comments

6.2 The on-site management team will constantly monitor and review the success of the DSMP. If considered necessary and appropriate the management team to the DSMP will propose changes which will need to be approved in writing by the London Borough of Havering (LBH).

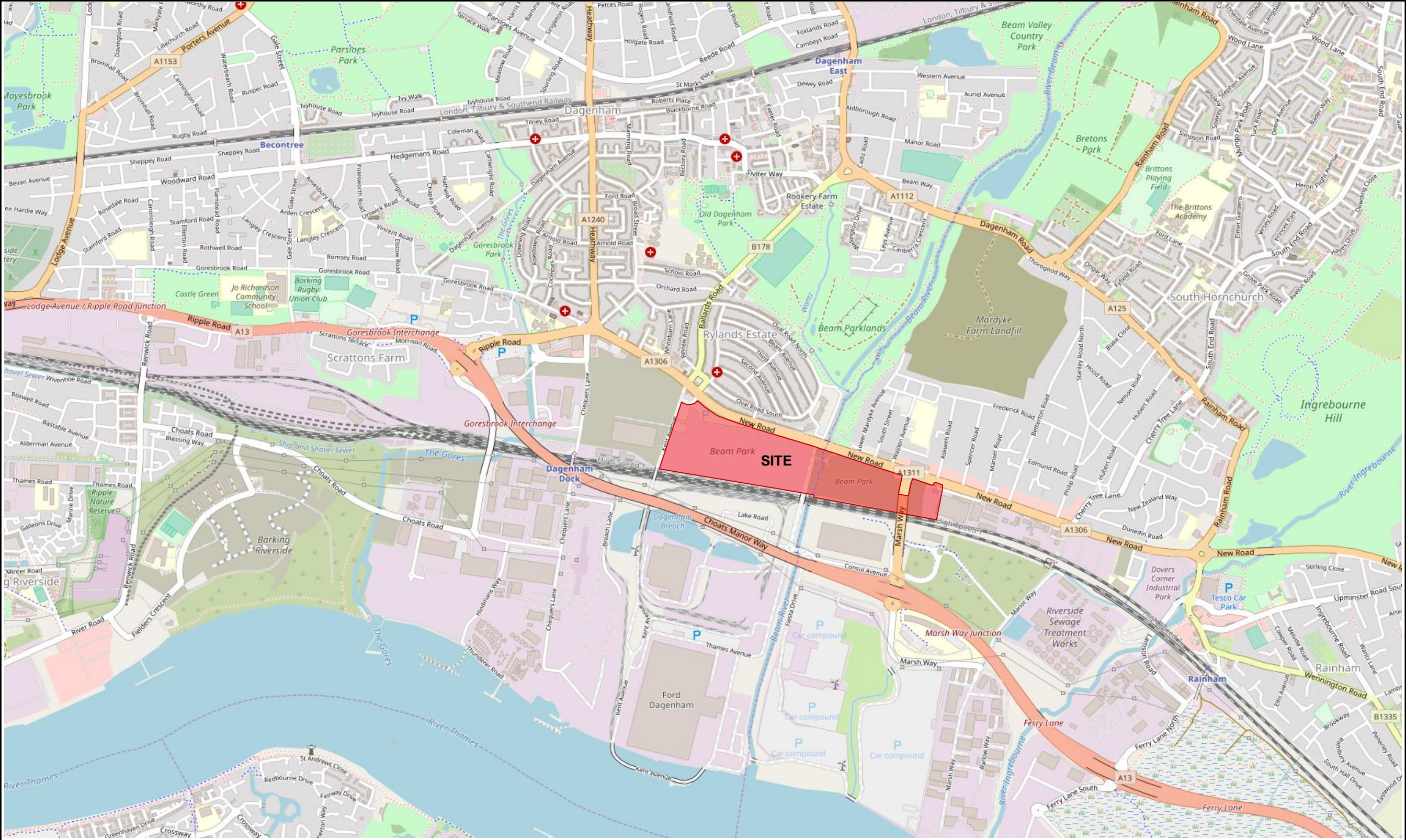
6.3 The contact details of the onsite management team will be provided to the LPA so that in the event of any issues that arise the authorities can arrange a meeting to discuss.


6.4 The contact details of the on-site management team will be provided to both TfL and the LPA so that in the event of any issues that arise the authorities can arrange a meeting to discuss.

7 SUMMARY

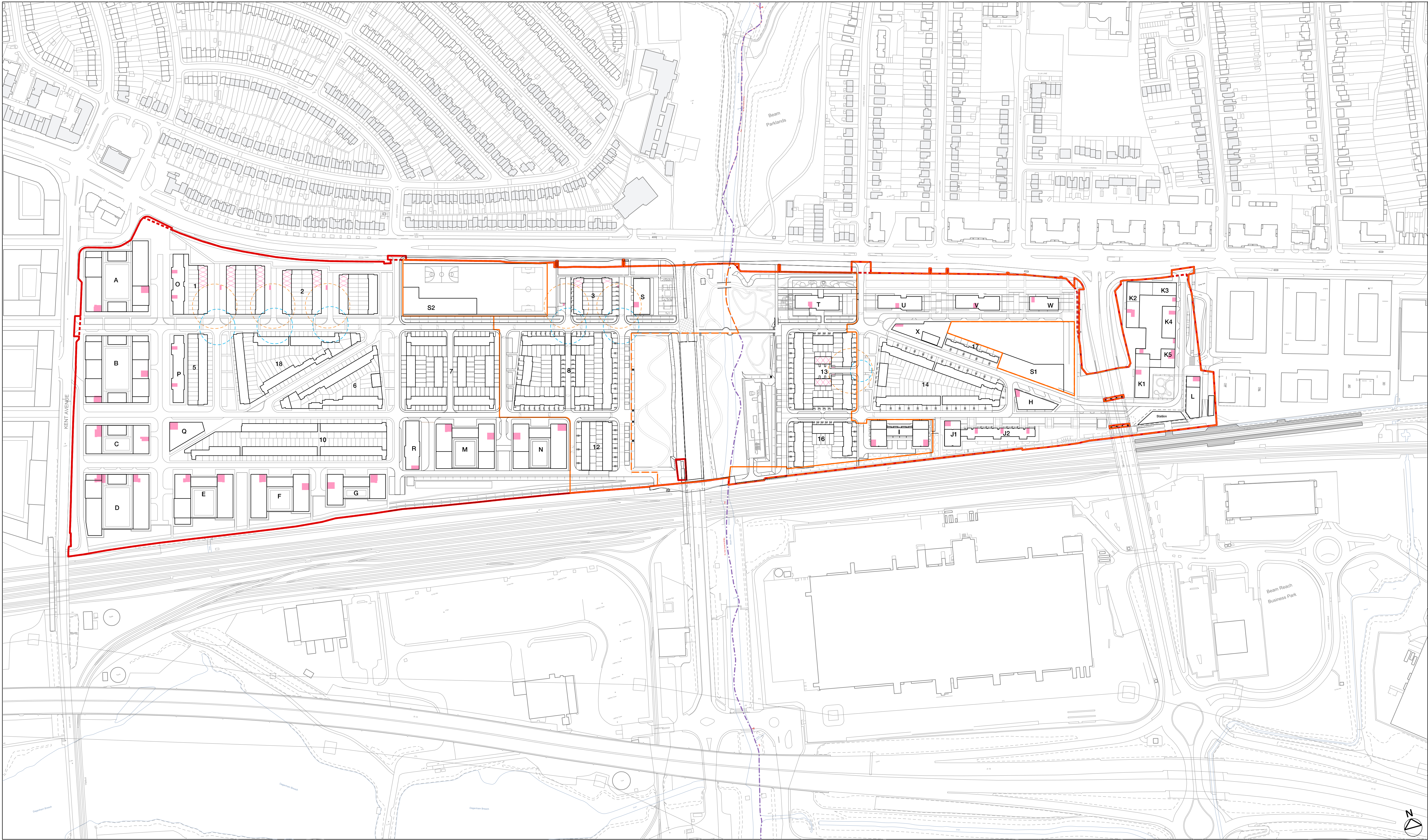
- 7.1 The purpose of this DSMP is to manage and control deliveries and servicing movements so as to minimise service vehicle and car conflict and to safeguard the safety and amenity of pedestrians.
- 7.2 As per the consented scheme, all deliveries and servicing will be undertaken within the site, including refuse collection.
- 7.3 For refuse collection, Buildings 13, T and I will be provided with a dedicated bin store at ground floor level, whereby refuse operators will collect waste directly from these locations. The refuse strategy will ensure that pulling distances for refuse vehicle operators are no more than 25 metres.
- 7.4 A dedicated servicing bay will be located via the road between Buildings I and J, and south of Building I. This area will be used to undertake deliveries to the residential units.
- 7.5 The DSMP will ensure the successful and efficient operation of servicing / delivery activity on a day to day basis.

FIGURES



<div>Key:</div> <div><div></div><div>Masterplan Site Location</div></div>	Beam Park, Dagenham				Countryside Properties PLC and Quadrant Housing Trust (L&Q)	
	Strategic Masterplan Site Location				<div></div> <div>Network Building, 97 Tottenham Court Road, London W1T 4TP Tel: 020 7580 7373 Email: london@vectos.co.uk www.vectos.co.uk</div>	
	DRAWN: EG	CHECKED: AP	DATE: 15/03/2019	SCALES: NTS		DRAWING REFERENCE: Figure 1

APPENDIX A



General Notes

DO NOT SCALE. All dimensions must be checked on site, errors are to be reported.

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Contractors must ensure that cross referenced drawings and specifications noted on these drawings are checked on a regular basis to ensure that the latest revisions are used.

Key

Consented planning application boundary

Borough boundary

Phase 2 boundary (2a LBH area, 2b LBBD area)

Phase 2A GLA Reserved Matters Application boundary

Key

Refuse areas

Houses served by refuse area

Refuse vehicle reverse distance (20m LBBD, 12m LBH)

25m Refuse collection distance

Scale 1:2000

0 20m 50m 100m 150m

Client

Countryside House
The Drive, Great Warley
Brentwood CM13 3AT
+44(0)1277 260000

Client

L&Q
10 Grove Crescent Road
Stratford, London E15 1BJ
+44(0)300 456 9998

Stakeholder

Greater London Authority
City Hall, More London
Riverside, London SE1 2AA
+44(0)20 7963 4000

Architect

Patel Taylor
48 Rawstorne Street
London EC1V 7ND
+44(0)20 7278 2323

Title

Refuse locations
GLA Phase 2A
Reserved Matters Application
(All built development within LBH)

Project

Beam Park

Scale

1:2000 @ A1 1:4000 @ A3

Issue Record

PL4 For Information

By

EP SR 26.07.2019

Drawing Number

448-PT-MP-PL-1116

Revision

PL4

Status

For Information

Patel Taylor

48 Rawstorne Street
London EC1V 7ND
T +44 (0)20 7278 2323
www.pateltaylor.co.uk