H O N D O

DESIGN & ACCESS STATEMENT
MARCH 2020

20-24a POPE'S ROAD

Adjaye Associates | on behalf of AG Hondo Pope's Road BV (Hondo)

This Design & Access Statement has been prepared to form part of the 20 - 24A Pope's Road planning application and should be read in conjunction with the remainder planning documentation.

All images and drawings are illustrative unless otherwise stated.

CONTENTS

1.0 INTRODUCTION 5

1.1 EXECUTIVE SUMMARY 6
1.2 CLIENT BRIEF 7
1.3 DESIGN TEAM 8
1.3.1 DEVELOPMENT MANAGER - AG HONDO POPE'S ROAD BV 8
1.3.2 ARCHITECT - ADJAYE ASSOCIATES 9

2.0 PROJECT CONTEXT 11

2.1 SITE ANALYSIS 12
2.2 URBAN CONTEXT 13
2.3 SITE LOCATION 14
2.4 HISTORIC CONTEXT 15
2.4.1 BRIXTON ORIGINS 15
2.4.2 RETAIL & ARCADE BEGINNINGS 16
2.4.3 TOWNCENTRE & INDUSTRIAL TRANSITION 17
2.4.4 BRIXTON VILLAGE & MARKET ROW 18
2.4.5 POPE'S ROAD 19
2.4.6 BRIXTON IDENTITY & PRESENT DAY 20
2.5 NEIGHBORHOOD POROSITY 21
2.5.1 THE BRIXTON MARKET 21
2.5.2 SHOP SIZES & URBAN GRAIN ANALYSIS 22
2.5.3 STREET MARKET 24
2.5.4 THE ARCADES 25
2.6 TRANSPORT LINKS 26
2.7 CONSERVATION AREAS 27
2.8 TOWNCENTRE & USES 28
2.8.1 TOWNCENTRE & USES DAY 28
2.8.2 TOWNCENTRE & USES NIGHT 29
2.9 PROTECTED VISTAS & PROTECTED AREAS 30
2.10 POLICY CONTEXT 31
2.10.1 POLICY Q26 31
2.10.2 SITE ALLOCATION 32
2.11 STREETSCAPE 33
2.11.1 STREETSCAPE 33
2.11.2 ELECTRIC AVENUE NORTH 34
2.11.3 ELECTRIC AVENUE WEST 35
2.11.4 ELECTRIC AVENUE SOUTH 36
2.11.5 ELECTRIC AVENUE EAST 37
2.11.6 POPE'S ROAD EAST - WEST 38
2.11.7 RAILWAY ARCHES 39
2.12 EXISTING BUILDING 40
2.12.1 EXISTING BUILDING OUTLINE 40
2.12.2 EXISTING BUILDING STREET VIEWS 41
2.12.3 EXISTING BUILDING ORGANISATION 44
2.12.4 EXISTING BUILDING ELEVATIONS 45

3.0 DESIGN CONSIDERATIONS 47

3.1 CONCEPT & DESIGN BRIEF 48
3.2 DESIGN OBJECTIVES 49
3.3 MASSING ANALYSIS 50
3.4 PUBLIC REALM INTERFACE 52
3.4.1 PUBLIC REALM EXISTING CONSTRAINTS 52
3.4.2 PUBLIC REALM INTERFACE - EXISTING 54
3.4.3 PUBLIC REALM OPPORTUNITIES 55
3.5 ARCHITECTURAL LANGUAGE - FAÇADE ARTICULATION 56
3.5.1 ARCHITECTURAL HERITAGE 56
3.5.2 INDUSTRIAL ORIGINS 57
3.5.3 TRIPARTITE STRUCTURE 58
3.5.4 TOWERS & FEATURE CORNERS 59
3.5.5 PEDIMENTS 60
3.5.6 ARCHES 61
3.5.7 WINDOW HEADS & LINTELS 62
3.6 MATERIALITY 64
3.6.1 MATERIALS AS MASS 64
3.6.2 CONCRETE 65
3.6.3 BRICK 66
3.6.4 DETAILING 67
3.6.5 STREETSCAPE MATERIALITY 68
3.7 DESIGN DEVELOPMENT 70
3.7.1 STATUTORY CONSULTATION 70
3.7.2 PUBLIC CONSULTATION 71
3.7.3 MASSING EVOLUTION 72
3.7.4 DESIGN EVOLUTION 73

4.0 DESIGN PROPOSALS 75

4.1 BUILDING ORGANISATION 76
4.2 BUILDING USES 77
4.2.1 BUILDING USES OVERVIEW 77
4.2.2 MARKET FLEXIBLE USES 78
4.2.3 WEST BLOCK USES 80
4.2.4 EAST BLOCK USES 81
4.3 FLOOR PLANS 82
4.4 PUBLIC REALM 94
4.4.1 GROUND FLOOR PERMEABILITY - MARKET 94
4.4.2 PUBLIC REALM COMPONENTS 96
4.4.3 PUBLIC REALM ON POPES ROAD 98
4.4.4 MARKET STREETS 106
4.4.5 PUBLICLY ACCESSIBLE OPEN SPACE 108
4.4.6 PUBLIC REALM BENEFITS 116
4.4.7 POTENTIAL FUTURE CONNECTION 117
4.5 FACADE LANGUAGE 118
4.5.1 CONTEXTUAL CONSIDERATIONS 118
4.5.2 SITE CONSIDERATIONS 119
4.6 FAÇADE CONSIDERATIONS 120
4.6.1 FAÇADE ASPECTS 120
4.6.2 EAST/WEST FACADE 121
4.6.3 SOUTHFAÇADE - WEST BLOCK 122
4.6.4 SOUTH FACADE - EAST BLOCK 123
4.6.5 NORTH FACADE - WEST BLOCK 124
4.6.6 NORTH FACADE - EAST BLOCK 125
4.7 EXTERNAL APPEARANCE 126
4.7.1 WEST FACADE - POPE'S ROAD 127
4.7.2 SOUTH FACADE 128
4.7.3 NORTH FAÇADE 129
4.7.4 EAST FACADE - VALENTIA PLACE 130
4.7.5 INTERNAL FACADES 131
4.8 TYPICAL FAÇADE MODULATION 132
4.8.1 WEST BLOCK - WEST FACADE 132
4.8.2 WEST BLOCK - SOUTH FACADE 136
4.8.3 WEST BLOCK - NORTH FAÇADE 140
4.8.4 EAST BLOCK - SOUTH FACADE 144
4.8.5 EAST BLOCK - NORTH FACADE 146
4.9 MATERIALITY 150
4.10 DETAILING 151
4.11 SIGNAGE 152
4.12 ENERGY AND SUSTAINABILITY 153
4.13 MAIN TOWNSCAPE VIEWS 154

5.0 ACCESS AND SERVICING 163

5.1 ACCESS TO THE BUILDING 164
5.2 ACCESS WITHIN THE BUILDING 165
5.2.1 ACCESS WITHIN THE BUILDING 165
5.2.2 MARKET ACCESS 166
5.2.3 OFFICE ACCESS 167
5.3 SERVICING AND DELIVERIES 168
5.3.1 MARKET DELIVERIES STRATEGY 168
5.3.2 OFFICE DELIVERIES STRATEGY 169
5.4 WASTE AND REFUSE STRATEGY 170
5.4.1 MARKET WASTE AND REFUSE STRATEGY 170
5.4.2 OFFICE WASTE AND REFUSE STRATEGY 171
5.5 BICYCLE PARKING 172
5.6 PUBLIC TOILET ACCESS 173
5.7 FACADE AND MAINTENANCE STRATEGY 174

6.0 CONCLUSION 177

INTRODUCTION

1

1.1 EXECUTIVE SUMMARY

This report supports the planning application for the redevelopment of Pope's Road, located adjacent to the Brixton Road railway infrastructure and the vibrant Brixton Village community. AG Hondo Pope's Road BV (Hondo) appointed Adjaye Associates to work on the aforementioned redevelopment.

Positioned on a constrained plot enclosed by the railway infrastructure, the proposed mixed-use scheme delivers a unique opportunity to preserve and expand on Brixton's existing rich heritage and marketplace. The design draws in two key areas of the town centre, opening it up with improved legibility to create a publicly accessible space that benefits existing traders and visitors. This also creates an opportunity to facilitate and improve permeability through and around the site.

Drawing on Brixton's cultural history and creative entrepreneurship, the proposed scheme further supports the local community and provides increased space for independent retailers, creatives and start-ups to thrive. The addition of a publicly accessible open market space with the capacity to hold various events driven by the community, enhances its character and establishes a key destination that contributes to the diversity of local business and provides an enhanced and prosperous environment for traders to continue to grow.

Flexible offices with affordable work space will be delivered to address an evident shortfall in modern offices in the area. Ultimately this allows Brixton to benefit from increased economic opportunities and diversify and support job creation, served by well-connected transport links and accessibility.

The design proposal being submitted has been subject to extensive pre-application discussions with the Council and key local stakeholders, as a result of which they have progressed positively in the design and land use terms. The design reflects location and respects surroundings views from Loughborough Park Conservation Area and the Brixton Conservation Area.

Fig. 1.1 Proposed View from Brixton Station Road

1.2 CLIENT BRIEF

The prime position of the site marks a clear opportunity to utilise and incorporate the two sides of the town centre and create a landmark destination that strengthens relationships with Brixton Village Market, Brixton Station, Market Row and creating a new commercial focal point for the whole Brixton Central.

An overview of the main development criteria / objectives are as follows:

Site and Local Context

- Utilises the site's position, facilitates the potential for improved connectivity and increased permeability
- Considers the opportunities created by the emerging SPG being prepared by the London Borough of Lambeth
- Adopts a holistic approach to sustainability, integrating social and economic aspects through stage of scheme evolution

Land Use

Market – c25,000sqft extension to the existing Brixton Village

- Permeable extension of the existing markets
- Flexible mix of uses that can evolve over time
- Offers a rooftop restaurant
- Is a covered but unconditioned space
- Creation of an atmosphere in keeping with the existing Brixton Village

Commercial Office – c.200,000sqft

- Includes a office entrance integrated through the market
- Provides commercial offer that creates a critical mass for wider commercial eco-system for Brixton Central

Public Realm

- Considers the Market to be publicly accessible open space
- Includes a flexible central space that can facilitate different activities throughout the life cycle of the week
- Develops options for the ability to create a significant focal point at the Brixton Station end of the site

Fig. 1.2 View of the existing building on site from Pope's Road looking south

1.3 DESIGN TEAM

1.3.1 AG HONDO POPE'S ROAD BV

Formed in 2013 by Taylor McWilliams, Hondo Enterprises is a property investment, development and asset management company focusing on projects in central London. It purchased Brixton Village and Market Row at Brixton Market in 2018.

From the outset they have worked with all stakeholders to preserve, invest and promote the unique character of the Markets to ensure that a vibrant future for tenants, visitors and the local community can be secured for the long term.

From the traditional fishmongers and greengrocers to the new restaurants and retailers, Hondo are committed to ensuring there is a balanced mix within the market. For the first time in the market's history they have introduced a transparent and independently verified service charge system.

Traders will also benefit from advisory boards that will help with marketing and accelerating their business models. In addition Hondo have looked to find new talent from the local community offering two free six month residencies through our Brixton Kitchen initiative.

Hondo has actively built relationships with community groups in Brixton, like Sleepless Brixton and has discussed providing space for community activities and events.

Hondo have also partnered with Impact Brixton, who have now relocated from Pop Brixton into the Markets. It is the intention that Hondo and Impact Brixton develop this relationship further with a view to delivering an affordable workspace facility, that is fully integrated with training and education focused on the local community,

With a diverse portfolio, projects include the ground up development of the 120 bedroom Curtain Hotel, the purchase of the 21 story mixed use Relay Building in Aldgate, and the recent acquisition of the NCP development portfolio.

1.3.1.1 External view of The Curtain, Shoreditch, London

1.3.1.2 External view of the Relay Building, Aldgate, London

1.3.1.3 Internal view of the Brixton Village, Brixton, London

1.3.2 ARCHITECT - ADJAYE ASSOCIATES

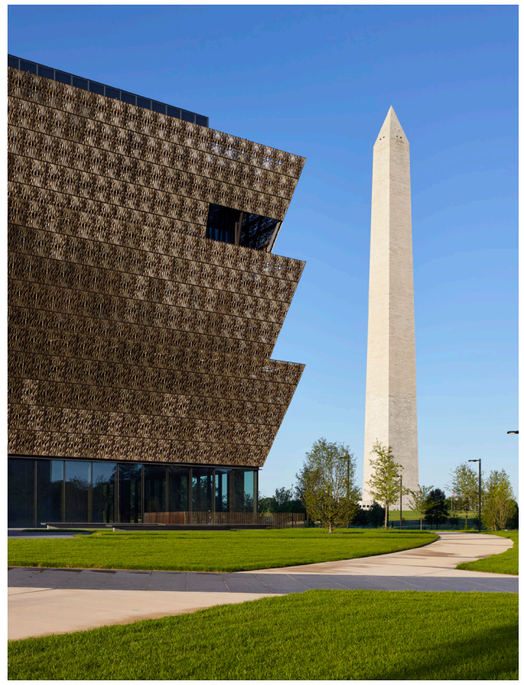
Adjaye Associates was established in June 2000 by founder and principal architect, Sir David Adjaye OBE. Based in the UK, US and Ghana, the firm has completed work across Europe, North America, the Middle East, Asia and Africa. One of the largest commissions to date is the design of the Smithsonian Institution's National Museum of African American History and Culture on the National Mall in Washington D.C.

The firm believes that architecture presents opportunities for transformation – materially, conceptually and sociologically. Our team has a proven track record in creating spaces that are layered with meaning, strengthened by rigorous historical research, and expressed through bold form, materiality and light.

The approach to urban development is driven by the human-scaled complexities of urban living in conjunction with topography, geography and climate. Driven by the desire to enrich and improve daily life, the practice's buildings are designed to meet the diverse needs of our clients. Inspiration is drawn from many influences around the world and the work clearly articulates this enthusiasm for issues of place and identity.

The practice is currently working on a number of projects including The Abrahamic Family House in Abu Dhabi commissioned by The Higher Committee for Human Fraternity; The Studio Museum in Harlem, New York and the UK Holocaust Memorial, London.

Other Completed works include: two neighbourhood libraries in Washington DC (2012); the Moscow School of Management SKOLKOVO in Moscow (2010); The Nobel Peace Centre in Oslo (2005); The Bernie Grant Arts Centre in London (2007); the Museum of Contemporary Art in Denver (2007); Rivington Place in London (2007); and the Idea Stores on Chrisp Street (2004) and Whitechapel (2005) – two pioneering new libraries in the London Borough of Tower Hamlets.



1.3.2.1 Smithsonian National Museum of African American History and Culture (NMAAHC), Washington DC, USA, 2016

1.3.2.2 UK National Holocaust Memorial and Learning Centre, Westminster, London, UK, Current

1.3.2.3 Idea Store, Whitechapel Road, London, UK, 2005

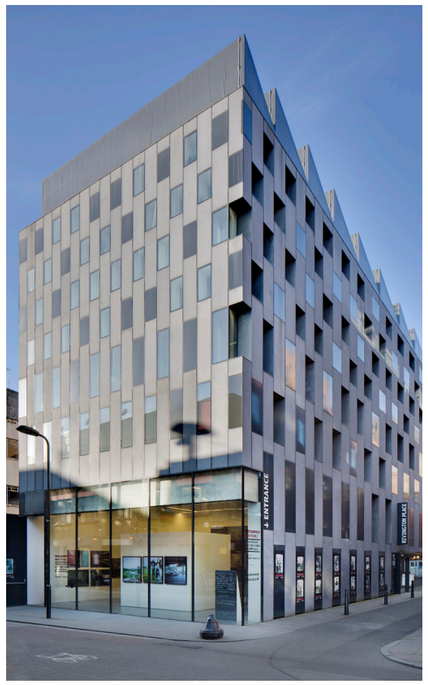


Figure 1.3.2.4 Rivington Place, Hackney, London, UK, 2007

1.3.2.5 Moscow School of Management, SKOLKOVO, Moscow, Russia, 2010

1.3.2.6 130 William Street, Manhattan, New York, USA, Current

PROJECT CONTEXT

2

2.1 SITE ANALYSIS

Pope's Road is centrally located within the heart of Brixton's town centre and benefits from well-served transport links, including proximity to Brixton Railway Station and Underground as well as numerous bus routes.

The current unused and elongated site is wedged between two elevated railway viaducts which historically served as a coal depot from the late 1800s. Facing the north-east across Pope's Road, there are currently unused viaducts concealed by large hoardings.

The bricked viaduct arches are semi-circular, rising from pavement level to a crown height of approximately 4.5m on the north and 9.5m on the south. The southern viaduct is approximately 12m high, passing over a lower elevated railway line.

The wide range of multicultural food and goods are prevalent throughout and around the site, with open and outdoor vendors and covered markets in arcades home to hundreds of independent shops.

The narrow thoroughfare of Pope's Road is lined on each side with demountable market stalls selling fresh produce and clothing. This extends beyond the railway line underpass where more market stalls occupy the whole length of Electric Avenue.

Surrounding retail units and shops of various sizes cover the urban grain from micro to small units on Reliance Road, Market Row and Brixton Village and larger units on Brixton and Pope's Road. Brixton Station Road also has a daily street market, with pitches located in front of the arches. The street is also pedestrianised with increased traction and footfall and during market trading periods.

Key listed buildings surround the site including the vibrant and multicultural Brixton Village Market and Market Row to the south. Loughborough Park Conservation Area also occupies the Eastern edge and portion of plot. Pope's Road is also short distance from the Grade II listed Brixton Recreation Centre to the Northern edge.

Fig. 2.1.1 Aerial view of Pope's Road looking north east

Fig. 2.1.2 Aerial night view of Pope's Road looking north east

2.2 URBAN CONTEXT

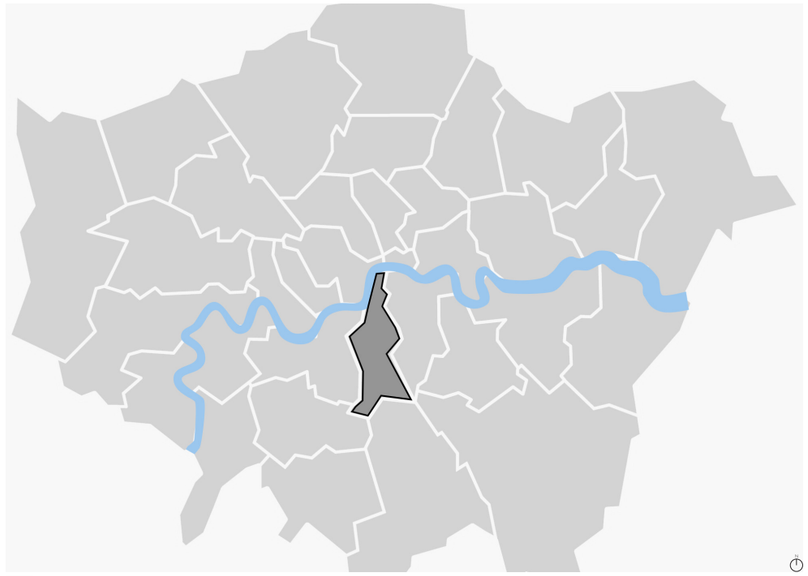
The site is located in Brixton, part of the London Borough of Lambeth and Inner South London. It borders with neighbouring districts including Stockwell, Clapham, Streatham, Camberwell, Tulse Hill, Balham and Herne Hill. The plot itself is positioned between two railway viaducts that face Pope's Road to the West and Valentia Place to the East.

Brixton town centre is an established area with rich heritage buildings and a strong urban streetscape defined by the character and culture of the community. The thriving and prominent street markets, retail units and arcades are central to the area and have long been rooted in its urban history and tradition. It is also home to several major and longstanding venues and performance spaces that contribute to the area as a popular and celebrated cultural destination.

Brixton is characterized by a rhythmic uniformity of architectural periods and styles such as rows of Victorian and Georgian terraced housing made from London Stock brick with distinctive façade details and features.

As a previously middle-class Victorian suburb due to strong transport links, the historic marketplace was formed on Atlantic Road in 1870 and rapidly dispersed to Brixton Road and its wide footways. The later adjacent and covered markets were built in the 1920s and 1930s and provided alternative vendor accommodation due to the widening of roads and rising costs of pitches on the high street.

The urban fabric of Brixton has continued to change along with gradual developments. It is encapsulated by narrow and straight streets that diverge into intimate urban corridors. The streetscape is served by Brixton Road that connects to adjoining minor roads and thoroughfares. It stretches along to the Grade II listed Lambeth Town Hall at the southern apex and forks off at the Junction of Brixton Hill and Effra Road.



London Borough of Lambeth

Fig. 2.2.1 London Borough Map

London Borough of Lambeth

Application Site

Fig. 2.2.2 London Borough of Lambeth Map

Application Site

Fig. 2.2.3 Site Location Plan

2.3 SITE LOCATION

The proposed Pope's Road site is located and bound by two railway viaducts. It is positioned between both Brixton Station Road and Atlantic Road, the two main artery roads that run from the East to West towards Brixton Road.

To the North on Brixton Station Road, the Pop Brixton and Grade II listed Recreational Centre are both within the site's locality. The Grade II listed Brixton Market village is also accessible via Atlantic Road. The junction from North to South Pope's Road meets with the notable Electric Avenue lined with markets.

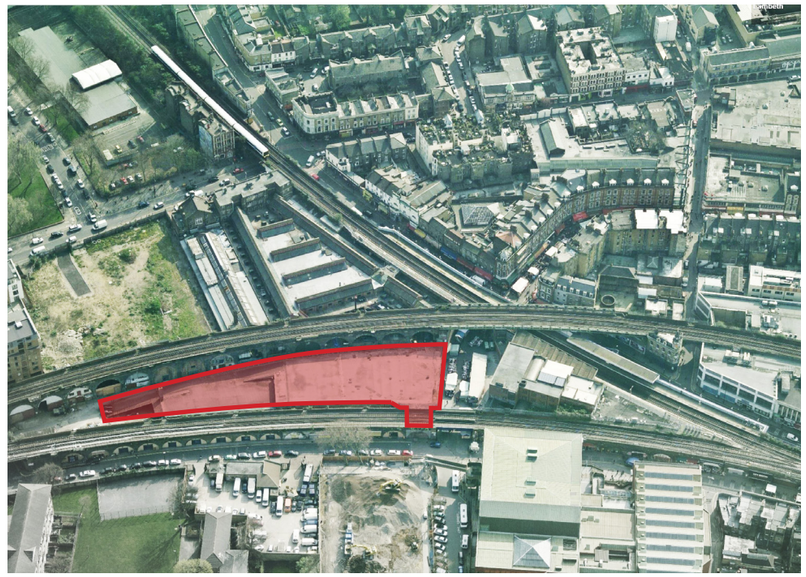
On the west side the site is also adjacent to the Brixton Network Railway Station and the portion of Pope's Road that faces the site is contained by two railway underpass.

There is a varied mix of terraced house configurations with activated independent shop frontages and associated commercial units and market stalls. A great proportion of the site and the surrounding area is allocated to various independent trade and street vendors.

A single-story wood clad side building is located next to the site and houses poorly maintained public toilet facilities.

Application Site

Fig. 2.3.1 Aerial View of the Applecation Site looking east



Application Site

Fig. 2.3.2 Aerial View of the Application Site looking south

Application Site

Fig. 2.3.3 Location Plan

2.4 HISTORIC CONTEXT

2.4.1 BRIXTON ORIGINS

Brixton has a rich and diverse urban history that can be traced back to the Roman era when Brixton Road was part of the route from London to Brighton. The area remained largely undeveloped and mainly agricultural until the Industrial Revolution and the advent of the railway, as well as the construction of the bridges across the Thames.

The creation of Vauxhall Bridge, at the beginning of the 19th century, provided a direct connection between Brixton and the centre of London. Brixton soon developed around Brixton Road and Acre Lane. Two of its oldest buildings have survived from this period: St Matthews Church, built in 1812, and Trinity Almshouses, built in 1824.

Fig. 2.4.1.1 Vauxhall Bridge, 1817

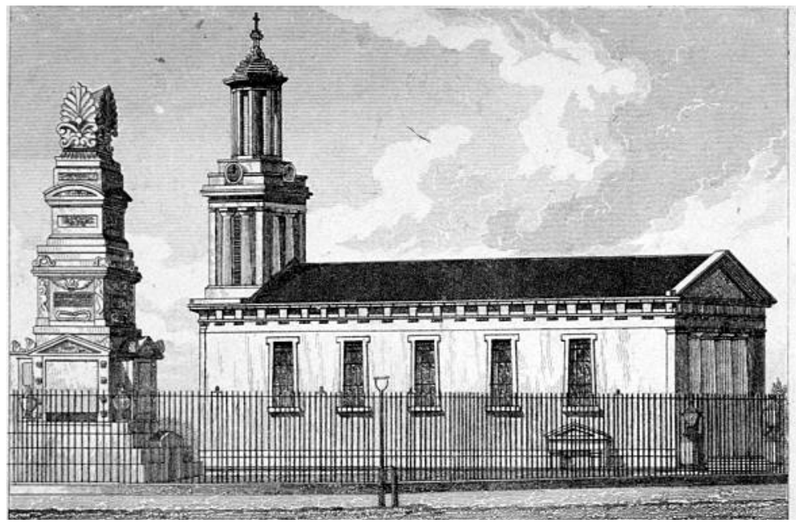


Fig. 2.4.1.2 St Matthew's Church, 1825

Fig. 2.4.1.3 Map of Brixton area, 1745

Fig. 2.4.1.4 Trinity Almshouse, 1898

2.4 HISTORIC CONTEXT

2.4.2 RETAIL & ARCADE BEGINNINGS

By 1879, Brixton was well served by railways, with no less than three stations close to Coldharbour Lane. The railway played a major role in the transformation of Brixton into a middle class suburb between the 1860s and 1890s.

In 1880, Electric Avenue was given its name after it became the first street in London to be lit by electricity. During this time, large residences were constructed along the main roads of Brixton, which were converted into flats and boarding houses at the start of the 20th century as subsequently the middle classes were replaced by an influx of the working classes.

Note that the shops on Brixton Road were still set back a generous distance from the main road - the building line would be brought forward to its current position in 1935.

Fig. 2.4.2.1 Coloured Electric Avenue postcard, c. 1904

Fig. 2.4.2.2 Original Marks and Spencer located under Brixton Railway Viaduct, 1920

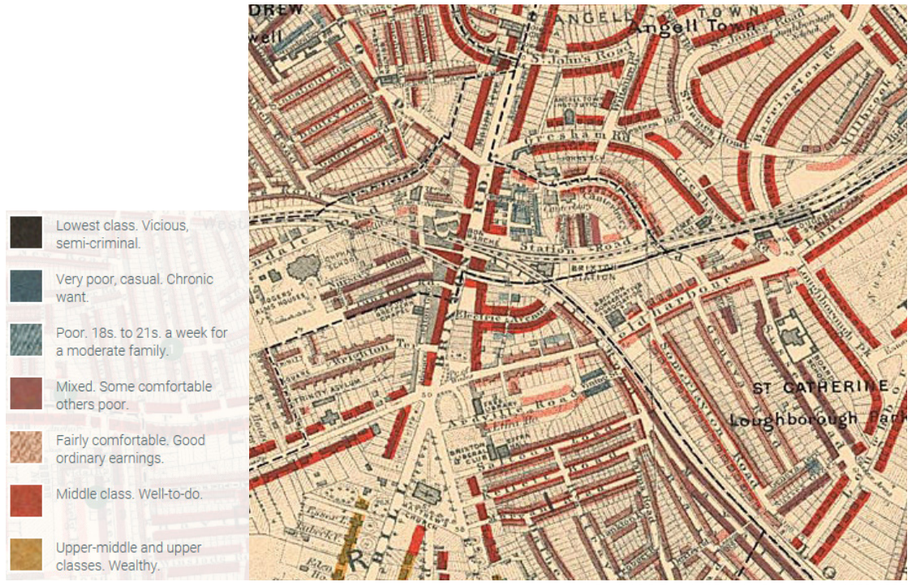


Fig. 2.4.2.3 Detail of Booth's Poverty Map 1898-1899

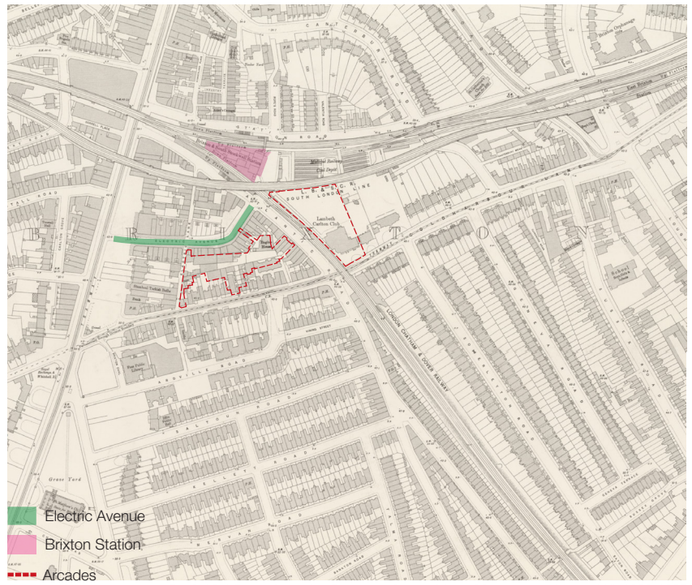


Fig. 2.4.2.4 Detail from the Ordnance Survey Map of Brixton and Herne Hill, 1893-1895

2.4.3 TOWN CENTRE & INDUSTRIAL TRANSITION

Towards the end of the 19th century, Brixton had developed into a mostly affluent, middle-class suburb. When industrialist Charles Booth produced his famous maps of London in 1899, coloured street by street to indicate levels of poverty and wealth, Brixton represented favourably with low levels of poverty in the area and wealthy villas on Brixton Hill.

In 1880, Electric Avenue, designed with elegant Victorian canopies, became the first market street to be lit by electricity. Whilst St Matthew's Church, in 1883, was the first place of worship to have electric lighting in Britain.

By the start of the 20th century, Brixton underwent an enormous social upheaval as the middle-classes were replaced by a large working-class population. Larger properties were subsequently subdivided into flats or boarding houses. This proved popular accommodation with workers in West End theatres, which apparently initiated Brixton's celebrated association with artistic culture.

From 1910-1915, at least nine cinemas opened in Brixton, some of which were housed in the railway arches. The Ritzy Picturehouse, a Brixton landmark, was built at this time.

Brixton Railway Station was opened in 1862. The formation of the railway transformed Brixton, and introduced the industrial revolution to the area. The new rail infrastructure led to the creation of a number of warehouses and factories.

Fig. 2.4.3.1 Brixton Road, c.1910

Fig. 2.4.3.2 Pope's Road street market, c. 1950

Fig. 2.4.3.3 Pope's Road with coal merchants, 1921

2.4 HISTORIC CONTEXT

2.4.4 BRIXTON VILLAGE & MARKET ROW

Granville Arcade (present day Brixton Village) is situated on a trapezoidal plot between the railway viaducts to the north, Atlantic Road to the west, Coldharbour Lane to the south, and a 1904 steam laundry to the west. Granville Arcade was built in 1935-1938 to the east of Market Row, to the design of Alfred and Vincent Burr and is named after the builder/developer, P Granville Grossman.

The twin main entrances facing the south at Granville Arcade form part of a 4 storey residential elevation, with ground floor shops. From the entrances run a pair of diverging arcades which are joined laterally by 4 more arcades.

The arcades have pitched glazed roofs supported by curved steel trusses. There is another entrance on the west of the Arcade under the viaduct, and another smaller entrance to the north-west. The interiors consist of two stories with retail shops and restaurants on the ground floor, and office space above.

Market Row is a post - WWI shopping arcade that occupies an infill site between properties fronting Atlantic Road, Coldharbour Lane, Electric Avenue and Electric Lane. The market was built c. 1928 to the design of RS Andrews and J Peascod.

The market has three external entrances which are the only, street-facing elevations of the building, and visible to the public realm. The entrances are two-storey, with a pedestrian entrance between the retail units on either side, and windows to the first floor.

The construction of Market Row is a steel frame and masonry infill with rendered, painted finish. The entrance wall-head, above the window, openings forms a parapet concealing a flat roof.

The avenues that define the market's form, are illuminated by pitched glazed roofs, supported by reinforced concrete open arch trusses with roundels, aligned to each shopping bay. The interior consists of a number of shallow units that line both sides of the T-shaped avenues. The units are of two storeys, plus attics divided by a concrete pilaster frame.

In addition to the Granville Arcade and Market Row, in the early 1920's another relevant arcade has been built in the vicinity, Reliance Arcade is the smallest of the three arcades and connects Electric Lane to Brixton Road.

Fig. 2.4.4.1 Interior of Granville Arcade, 1968

Fig. 2.4.4.2 Interior of Granville Arcade, 1985

Fig. 2.4.4.3 Market Row Electric Lane west entrance, 1952

Fig. 2.4.4.4 Market Row Interior at east entrance towards Atlantic Road, c 1970

Fig. 2.4.4.5 Market Row east entrance from Atlantic Road, 1964

2.4.5 POPE'S ROAD

Pope's Road has historically served as a linkage between Brixton Village and Brixton Station Road and has been a thriving and busy street market over one hundred years.

A prominent historic feature which has since been withdrawn, is the rows of coal staithes and various merchant offices and frontages that served local business in the area.

The coal staithes played a significant role for delivering large quantities of coal which would be transported and brought in via Brixton's train lines and distributed by horse drawn carts.

Coal was also transported to merchants populating Pope's Road by means of an elevated set of two railway sidings where wagons would drop and store coal into the spaces below. The coal staithes were traditionally made from wood and supported by a wooden platform.

Up until the 1950s Pope's Road remained a thriving market street and in this period the coal depots that existed were gradually cut out and removed with the elevated sidings swept away. In a postwar upheavel phase several large scale buildings were built around the area and the most significant one for its scale, social moves and pioneering qualities was the Brixton Recreation Centre.

Fig. 2.4.5.1 Brixton Station Road looking east, c. 1940

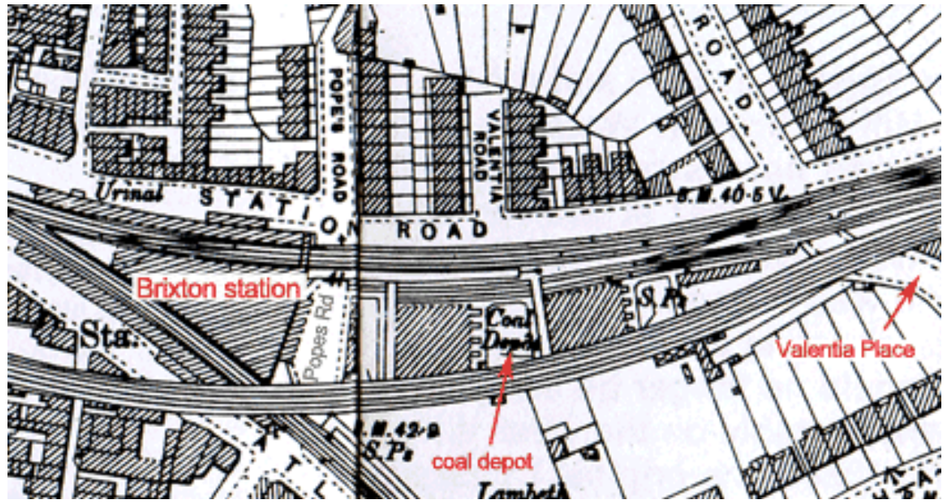


Fig. 2.4.5.2 Ordnance Survey Map with coal depot located on current application site

Fig. 2.4.5.3 Pope's Road looking north with Tesco on current application site, street market and multi-storey car park, 1974

Fig. 2.4.5.4 Pope's Road looking north with street market, 1956

2.4 HISTORIC CONTEXT

2.4.6 BRIXTON IDENTITY & PRESENT DAY

Brixton has remained a historically rich community that has developed and shifted over recent decades. During the post-war period, the cultural and social landscape of the area shifted as it become a place of settlement welcoming an extensive Afro-Caribbean community and wave of multinational immigration. June 22nd, 2018 marked the 70th anniversary of the arrival of the SS Empire Windrush and a new era of Caribbean settlement in post-war Britain.

It continues to retain a reputation that embodies a diverse and culturally rich livelihood through music, festivals, entertainment, art and food. The significance of the traditional marketplace reflects the multicultural diversity of the area's residents which can also be found and portrayed in surviving murals.

After the 1981 riots, the council helped to fund a number of iconic murals throughout Brixton, including Stephen Pusey's Brixton Academy Mural painted in 1982 depicting children of different backgrounds at play, highlighting the symbolic representation of racial harmony between them.

Founded in 1981 by historian and activist Len Garrison, the celebrated Black Cultural Archives located in Windrush Square, is a significant platform and national repository that recognises the importance of Black Culture and history in the UK.

Launched in 2009 and designed to support local business and trade, The Brixton Pound featured Len Garrison, as well as community social justice campaigner Olive Elaine Morris.

At a time when Brixton's reputation was anguished by rioting, a significant memory in the area's history was Nelson Mandela's visit to the Brixton Recreation centre on 12 July 1996. His recognition and support towards the Brixton community as a central place in the UK black culture and social movement remains commemorative to the community today.

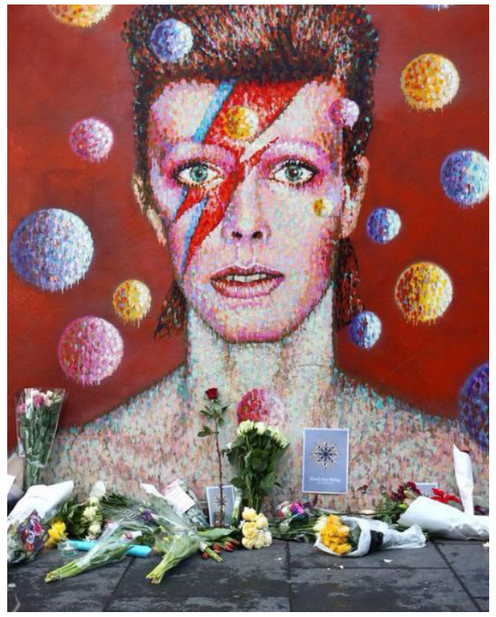


Fig. 2.4.6.1 Street Art dedicated to David Bowie

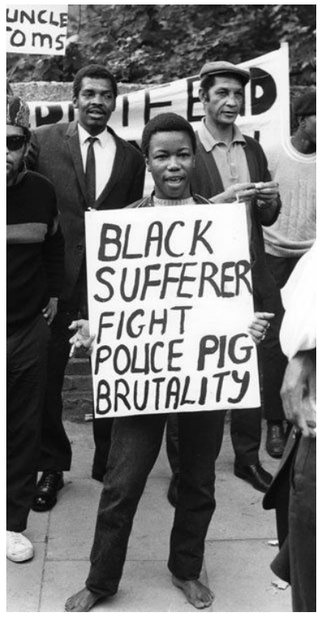


Fig. 2.4.6.2 Olive Elaine Morris (1952-1979)

Fig. 2.4.6.3 Brixton Village, 1961 (British Pathe)

Fig. 2.4.6.4 Black Thrive

Fig. 2.4.6.5 Windrush Generation 1948

Fig. 2.4.6.6 Trader in the Brixton Village

2.5 NEIGHBORHOOD POROSITY

2.5.1 THE BRIXTON MARKETS

Run by the Brixton Market Traders' Federation Community Interest Company, Brixton Market collectively comprises a street market in the centre of Brixton that included the adjacent covered market areas in nearby arcades such as Reliance Arcade, Market Row and Granville Arcade (recently rebranded as 'Brixton Village').

The market sells a wide range of foods and goods but is best known for its African and Caribbean produce, which reflect the diverse community of Brixton and surrounding areas of Lambeth.

The Street Market is managed by the London Borough of Lambeth. The covered arcades have always been in private ownership, although substantial public funding was provided for their refurbishment under the Brixton Challenge grant scheme.

Electric Avenue which is now part of the street market was built in the 1880s and was one of the first streets to have electric light. Glazed iron canopies covered the footpath, but these were significantly damaged by WW2 bombs, and finally removed in the 1980s.

In 2016, Electric Avenue was refurbished with funding from the Mayor of London's High Street Fund, Lambeth Council, Transport for London and the Heritage Lottery fund to include an illuminated sign celebrating the area's history.

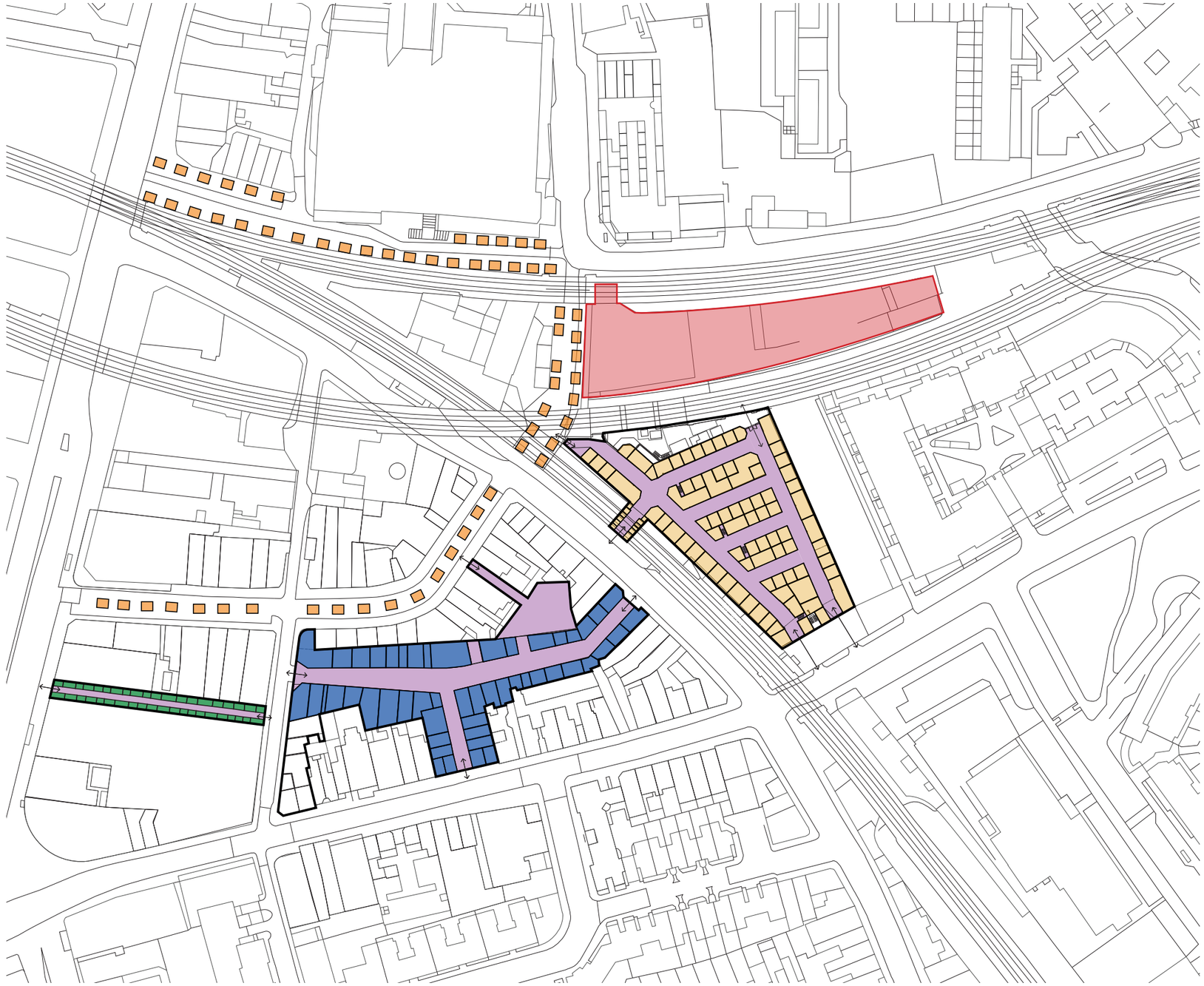


Fig. 2.5.1 Brixton Markets plan

2.5 NEIGHBORHOOD POROSITY

2.5.2 SHOP SIZES & URBAN GRAIN ANALYSIS

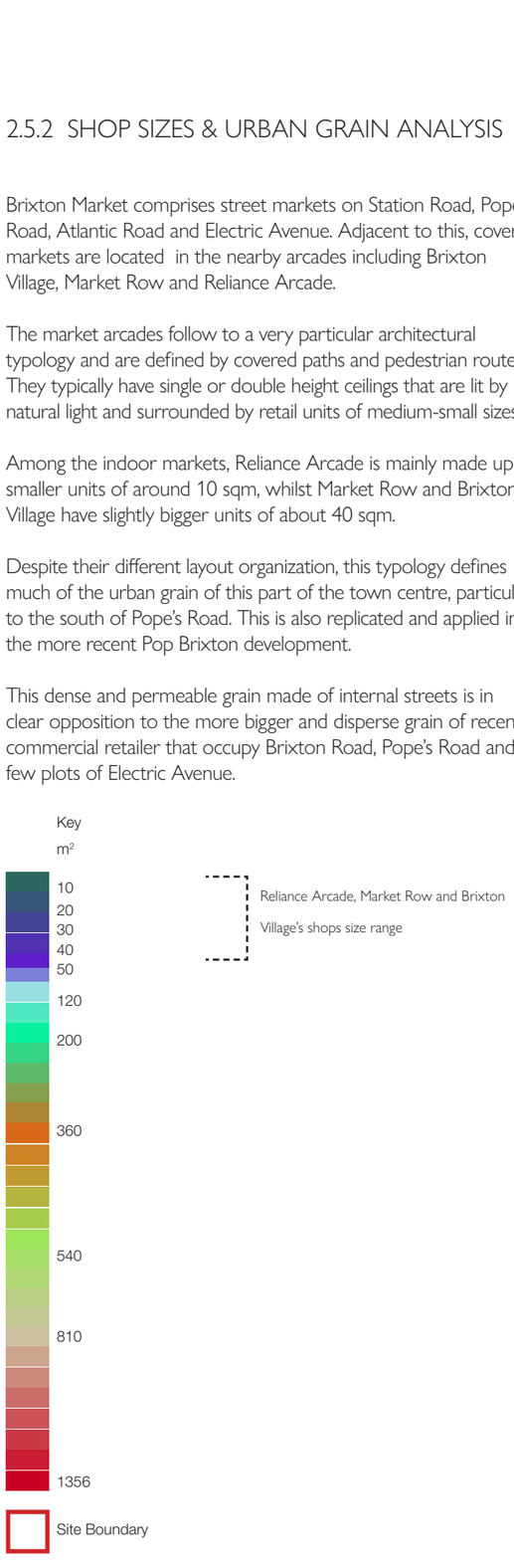
Brixton Market comprises street markets on Station Road, Pope's Road, Atlantic Road and Electric Avenue. Adjacent to this, covered markets are located in the nearby arcades including Brixton Village, Market Row and Reliance Arcade.

The market arcades follow to a very particular architectural typology and are defined by covered paths and pedestrian routes. They typically have single or double height ceilings that are lit by natural light and surrounded by retail units of medium-small sizes.

Among the indoor markets, Reliance Arcade is mainly made up of smaller units of around 10 sqm, whilst Market Row and Brixton Village have slightly bigger units of about 40 sqm.

Despite their different layout organization, this typology defines much of the urban grain of this part of the town centre, particularly to the south of Pope's Road. This is also replicated and applied in the more recent Pop Brixton development.

This dense and permeable grain made of internal streets is in clear opposition to the more bigger and disperse grain of recent commercial retailer that occupy Brixton Road, Pope's Road and few plots of Electric Avenue.



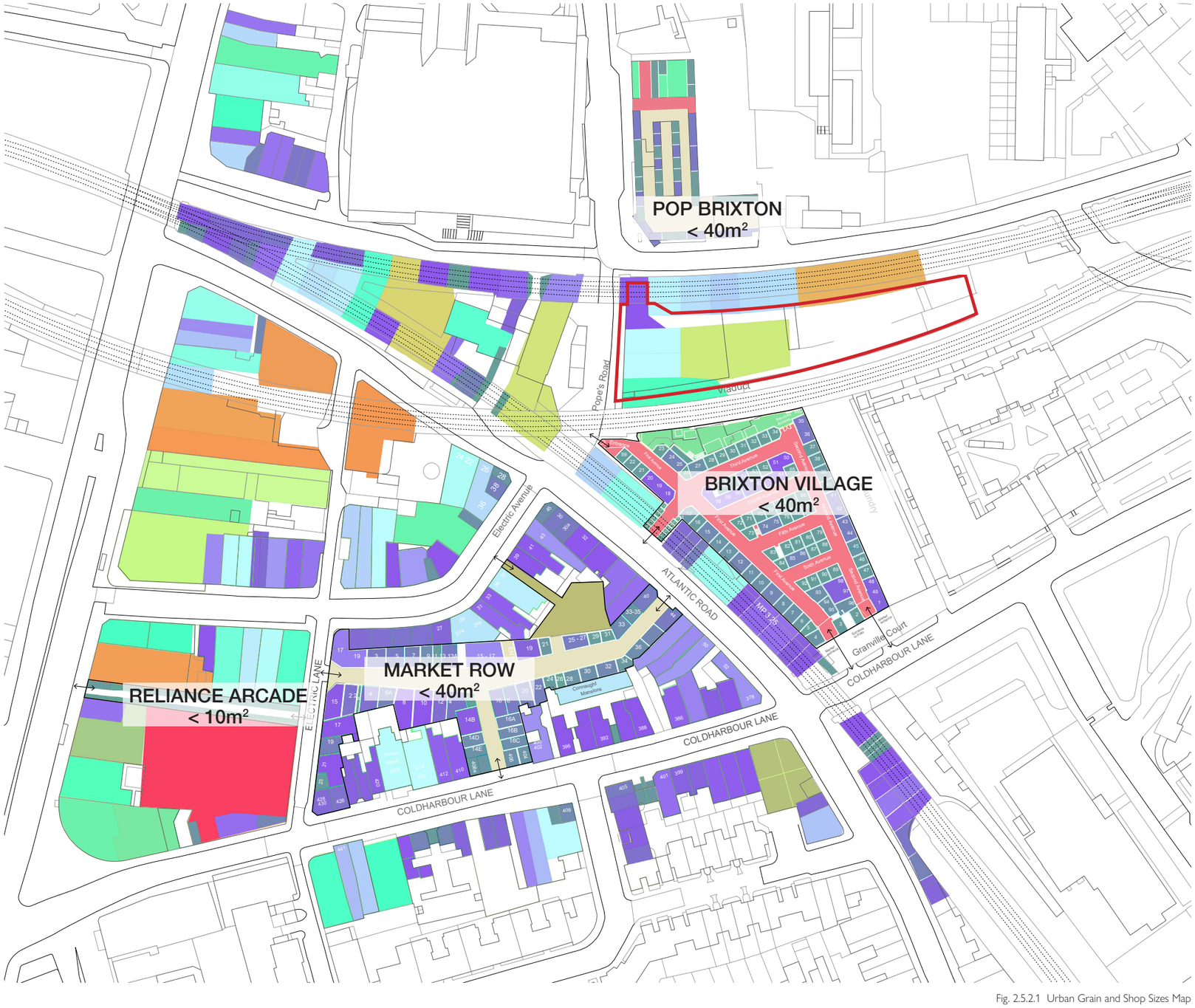


Fig. 2.5.2.1 Urban Grain and Shop Sizes Map

Within the two covered Reliance Arcade and Brixton village Markets, smaller units are occupied by various independent entities offering enhanced day and night economies.

Serving as a popular visitor destination, the smaller units support and house a range of independent restaurants and cafés serving local food that span Europe, India, East Asia, Africa, South America and the Caribbean as well as more known mini-chains.

Larger retail units are not as realised to their full potential and are occupied by various larger supermarkets and high street store names.

MICRO UNITS
RELIANCE ARCADE
< 10m²

Fig. 2.5.2.2 Reliance Arcade

SMALL UNITS
MARKET ROW - BRIXTON VILLAGE
< 40m²

Fig. 2.5.2.3 Brixton Village

LARGE UNITS
POPE'S ROAD - BRIXTON ROAD
< 200m²

Fig. 2.5.2.4 Iceland

Fig. 2.5.2.5 Reliance Arcade

Fig. 2.5.2.6 Brixton Village

Fig. 2.5.2.7 Sports Direct

2.5 NEIGHBORHOOD POROSITY

2.5.3 STREET MARKET

Pope's Road

Pope's Road formed a key part of the railway infrastructure and history of Brixton. After the closure of the coal depot and removal of coal staithes, from the 1950s it continued to serve as a bustling road market lined with a succession of merchant offices.

Little has changed and the road still acts as a street market today where short-term stall holders sell various inexpensive goods and a row of small retail shops occupy the pre-existing merchant offices.

Atlantic Road

Brixton's street market was founded on Atlantic Road with the advantage of inexpensive trains bringing in customers from wider areas. It has played a pivotal role for independent businesses who have operated in the railway viaduct arches since the early 1900s.

David Greig's regional supermarket chain established in 1881, was one of the first shops to bring prominence to the street. Following the success of its first butchery store on 54-58 Atlantic Road, by 1944 it expanded and dominated much of the street including 5, 7, 9, 30a, 54, 65 and 58 Atlantic Road as well as Electric Avenue and Station Road. The original signature thistle motif and tiling are still present on the exterior today.

The street has kept much of its market operations with the archway units doubled in size and the railway over-head platform now extended outward with minimal sheltered protection.

Electric Avenue

Built in 1888 as the first market street to be lit by electricity in London, it was previously built with an iron and glass canopy as protection for shoppers.

As a now open and pedestrianised street, in the centre of Brixton's shopping district, it's active and vibrant street market with stalls, sell every sort of goods in conjunction with retail units located at the ground floor of the buildings enclosing the streets constitute one of the most famous and characteristic street markets in London.

Brixton Station Road

The main road that runs towards the centre of Brixton, provides direct access to Brixton Road Station. Since the 1920s, it has also hosted a busy street market with shops tucked into the railway arches and it continues to do so today.

It a previously housed a number of small shops towards the Brixton Road the up until the 1970s. Through ambitious replanning, the area made way for redevelopment and the building of the Grade II listed Brixton Recreation Centre. Led by designer George Finch with Ove Arup from 1974-1985, the addition of the public sports and leisure centre was built with the aim of bringing human scale at street level combined with the introduction of bold and pronounced form.

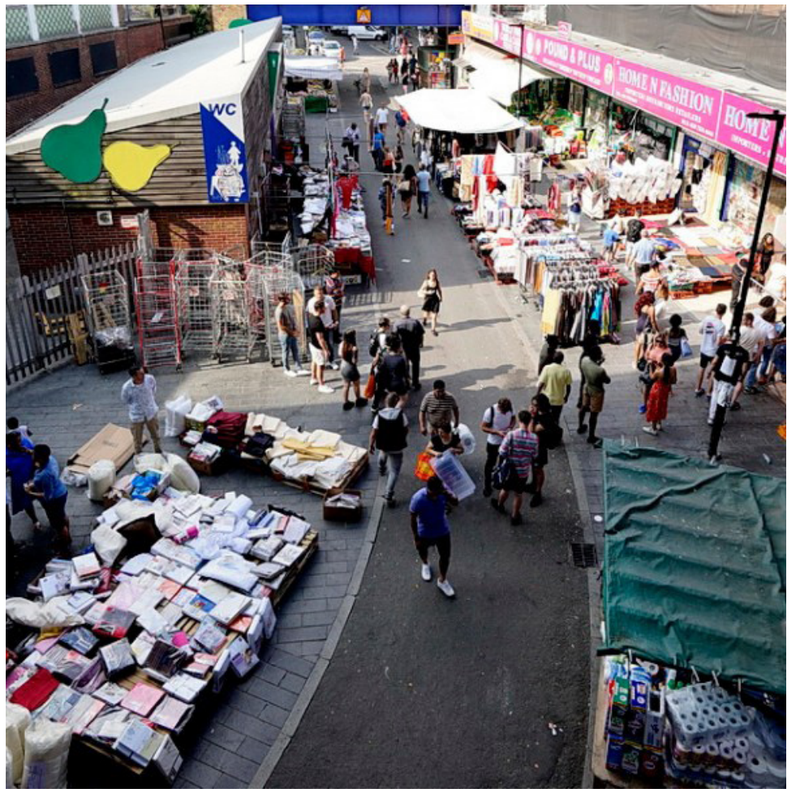


Fig. 2.5.3.1 Pope's Road

Fig. 2.5.3.2 Electric Avenue

Fig. 2.5.3.3 Brixton Station Road

Fig. 2.5.3.4 Atlantic Road

2.5.4 THE ARCADES

Reliance Arcade

455 Brixton Road (c1924) provides a narrow straight pedestrian route from Brixton Road to Electric Lane. It incorporates the original Georgian house and has a beautiful Neo Egyptian façade to Electric Lane; which was later extended forward by Ernest J Thomas in 1931.

Today the inside consists of micro shops no larger than typical market stalls selling a scenario of mostly disparate products. Dense and colorful frontages with hanging goods create a very active atmosphere.

Market Row

40 - 44 Atlantic Road was designed by Andrews and Peascod in 1928. It was built in the back yards of existing premises and links Atlantic Road, Coldharbour Lane and Electric Lane.

In plan it configures as a sinewy lane whilst the interior space is double-height and lit from above via continuous roof skylights. Bigger shop units exceed their boundary and populate the internal streets with goods animating the atmosphere.

Brixton Village

Was built as Granville Arcade in 1937 to designs of Alfred and Vincent Burr; the developer was Mr Granville-Grossman. It was opened by actor Carl Brisson on 6 May 1937.

Based on a geometrical grid with one tilted axis, the interior of covered streets called 'Avenues' have double-height ceilings, similar to Market Row. There are over 100 shops which offer a wide variety of goods. All the frontages are painted with braight colors that animate the space whilst the natural light allowed in by the roof skylight makes the space look airy and generous. It links Coldharbour Lane, Atlantic Road and Pope's Road.

The three market arcades in close proximity, forming an extensive network of stalls, are rare survivals and their special character is what marks out Brixton as distinctive from other suburban shopping centres: a mixture of history, interesting architecture, the variety of goods on sale and the cultural mix of Brixton, known as the symbolic 'soul of black Britain'.

Since 2011, the shops in Brixton Village and, more recently, Market Row and Reliance Arcade have increasingly converted into cafes and restaurants, serving a wide range of different cuisines. As a result, they are now open 8am–11.30pm every day except Monday, when they shut at 6pm.

Fig. 2.5.4.1 Brixton Village

Fig. 2.5.4.2 Market Row

Fig. 2.5.4.3 Reliance Arcade

2.6 TRANSPORT LINKS

Brixton has been long established and well served by a number of existing transport links located within the vicinity of the site and provides good access for commuters as well as cyclists and pedestrians.

Brixton Tube station forms the southern terminus of the Victoria line and provides regular key routes and connectivity to Greater London areas including major London terminals such as Victoria, Euston and Kings Cross St Pancreas and International Station.

Brixton Railway on Atlantic Road managed by London & South Eastern Railway Limited, is on the 3-mile Chatham main line route which operates every 15 minutes, running to and from London Victoria to Dover directions.

The area is also well served by a high number of 24-hour bus routes that run in multiple directions, stopping directly outside the tube station, overground as well as Atlantic Road and Electric Avenue.

A cycle path runs directly through Brixton Road with the closest bike storage areas located from at St John's Crescent, to Ferndale Road and Saltoun Road.

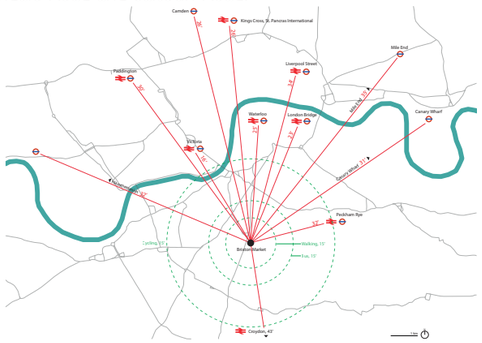


Fig. 2.6.1 Travelling Conncetions and Time from Brixton to Central London

Key

- Underground / Victoria Line
- Overground
- Chatham Main Line
- South Eastern Main Line
- Bike Storage
- Bike Path
- Bus Stop
- Site Boundary

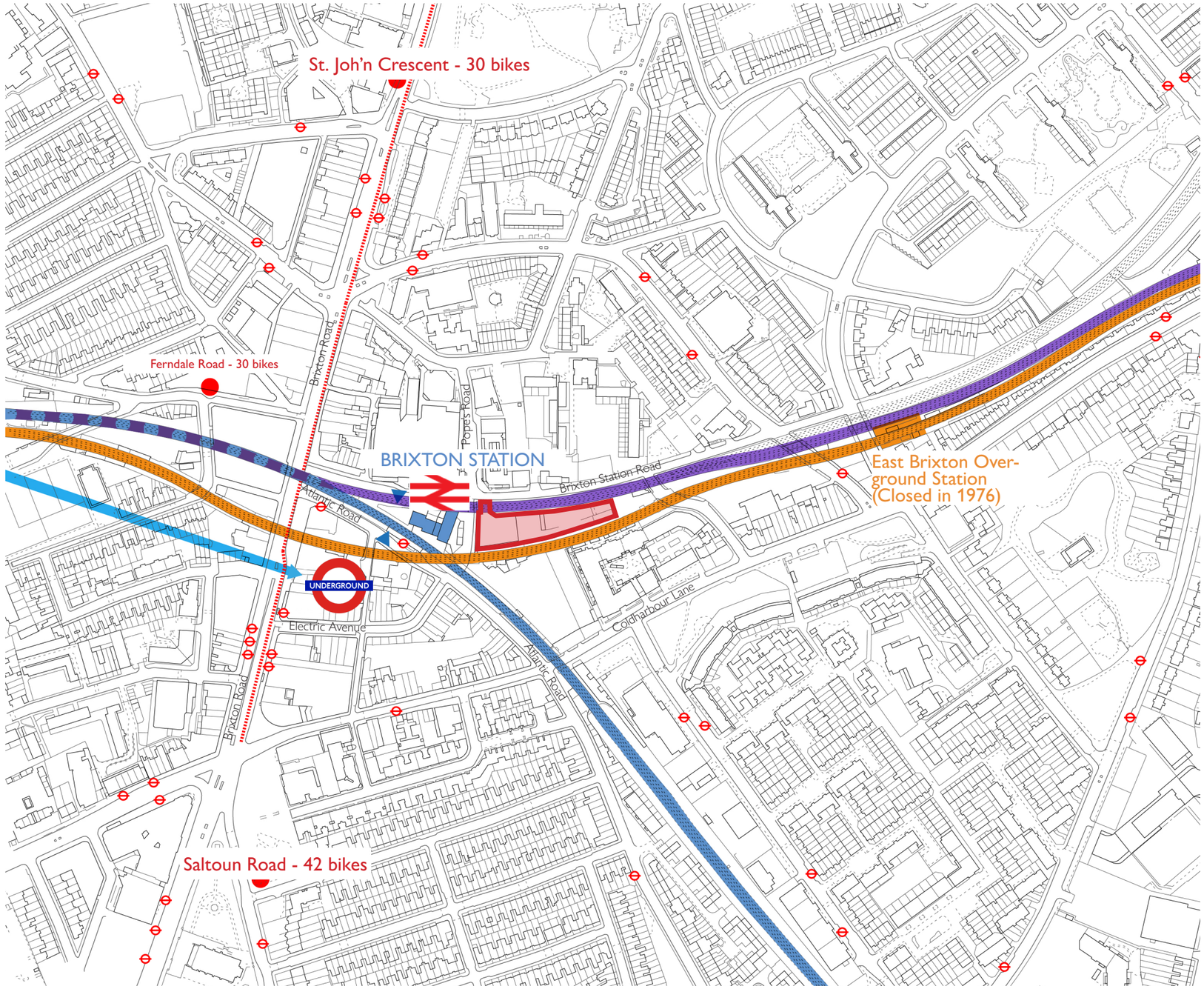


Fig. 2.6.2 Transport Links Map

2.7 CONSERVATION AREAS

The Pope's Road site and wider Brixton Area is characterised by a wealth of 19th Century developments and 20th century purpose built arcades. It is closely surrounded by a number of distinctive Grade II listed buildings including the Brixton Recreation centre to the North and Brixton Village and Market Row to the South.

The Loughborough Park Conservation Area is located to the east of the site with Brixton Conservation Areas spread throughout the centre from Wyck Gardens on Brixton Road leading up to the Grade II listed Church of St Matthew.

Key

1. 44-46 Stockwell Road
2. O2 Academy Brixton
3. 201-209 Stockwell Road
4. 337-339 Brixton Road
5. 341 Brixton Road
6. 363-365 Brixton Road
7. St John Gate
8. St John Building
9. Brixton Recreation Centre
10. Platform Piece
11. Brixton Lodge
12. Reliance Arcade
13. Market Row
14. Brixton Village
15. Brixton Fire Station
16. 297-299 Coldharbour Lane
17. 289-291 Coldharbour Lane
18. 20-22-24-26-28 Loughborough Park
19. 63-73 Loughborough Park
20. 5-23 Moorland Road
21. Ritzy Cinema
22. Trinity homes
23. 24-26 Acre Lane
24. Lambeth Town Hall
25. Bust of Sir Henry Tate
26. Mausoleum of Richard Budd
27. Effra House
28. Churchyard Gate Piers
29. Church of St Matthew
30. Pillar Box

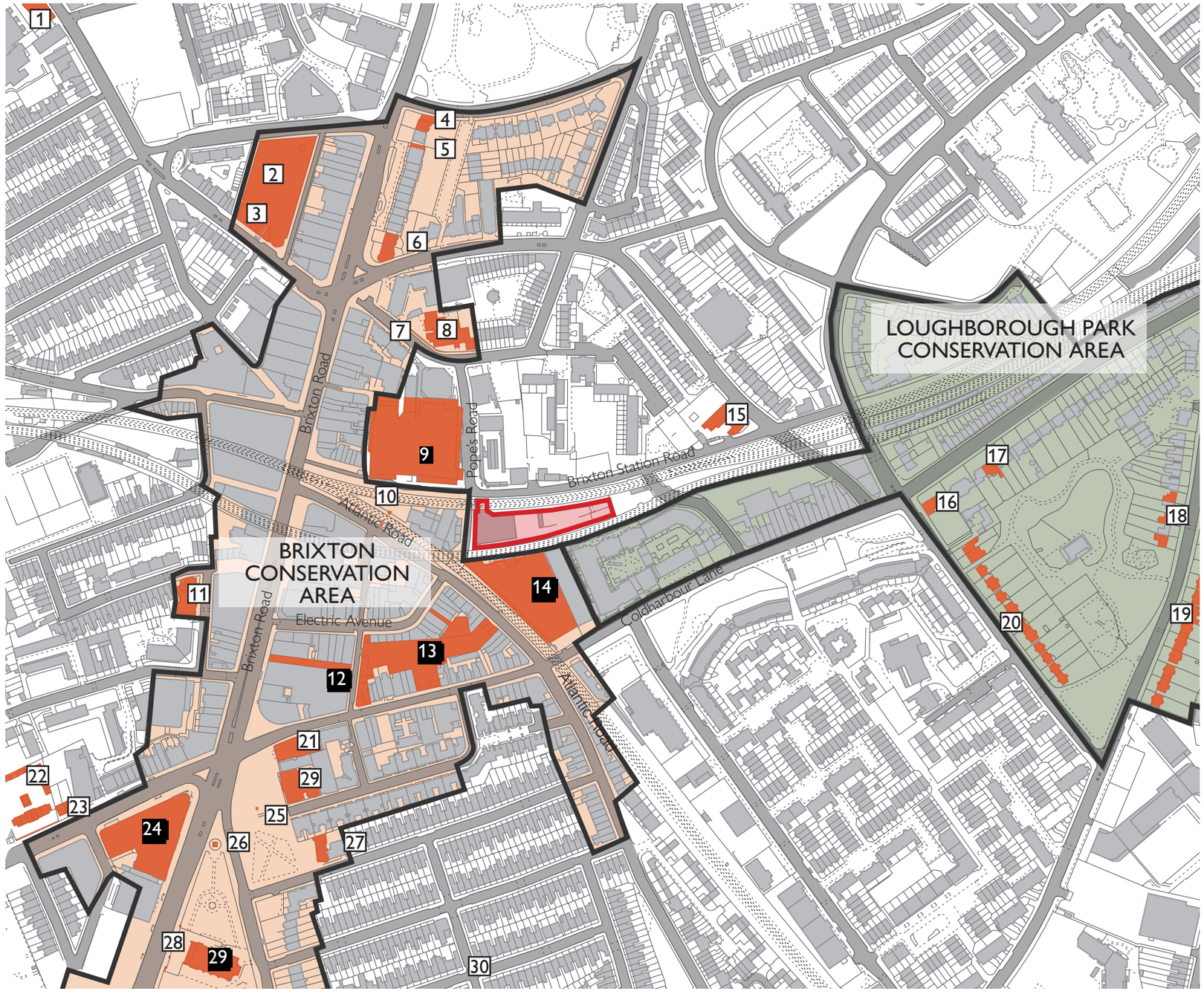


Fig. 2.7 Conservation Areas & Listed Buildings Plan

2.8 TOWNCENTRE & USES

2.8.1 TOWNCENTRE AND USE - DAY

The site between Pope's Road, Brixton Station and Valentia Place comprises residential and commercial mixed-use, including street markets, independent café, bars and entertainment venues. The narrow thoroughfare of Pope's Road is lined on each side with demountable market stalls selling fresh produce and clothing.

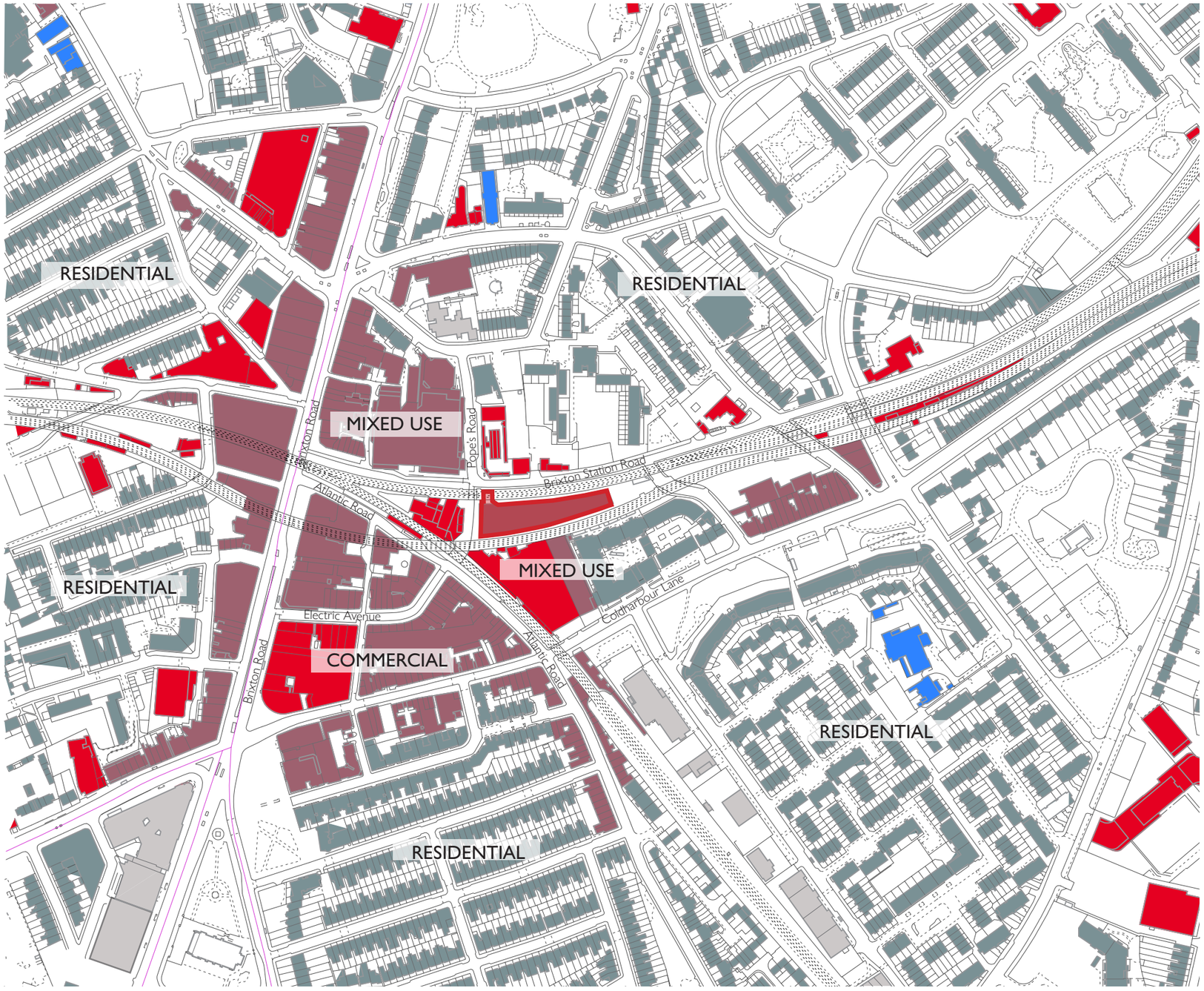
Brixton Station Road has a daily street market, with pitches located in front of the railway arches. The street is pedestrianised during market trading periods: Monday - Saturday from 10am-4pm except Wednesday from 10am-2.am.

This current distribution of uses set the base for the creation of a potential commercial centre revolving around Pope's Road and the markets as opposed to a more civic centre located around Windrush Square.

Brixton currently benefits from a number of advantages such as high transport connectivity and access, as well as a strong and local workforce. The Brixton Economic Action plan outlines current opportunities and challenges that face the area in regards to local businesses, residents and investors.

The necessity for increased affordable office and work space that ensures economy retention alongside growth and development is yet to be realised to its full potential. Our proposal offers a flexible scheme that accommodates an improved working and public environment as well as space for growth. This ultimately allows Brixton to secure economic opportunities as well as diversify and support future job creation.

The delivery of enhanced amenity and additional retail and leisure space will help capture both new and emerging populations and consolidate and strengthen Brixton's daytime economy and position. This will also be crucial in attracting new commercial operators into the area whilst allowing current businesses to operate with secured longevity.



Key

- Commercial
- Mixed Use
- Residential
- Educational
- Civic
- Site Boundary

Fig. 2.8.1 Towncentre & Uses Day Plan

2.8.2 TOWNCENTRE AND USE - NIGHT

Brixton's night and evening economy is well known and attracts numerous visitors both in and outside the local community.

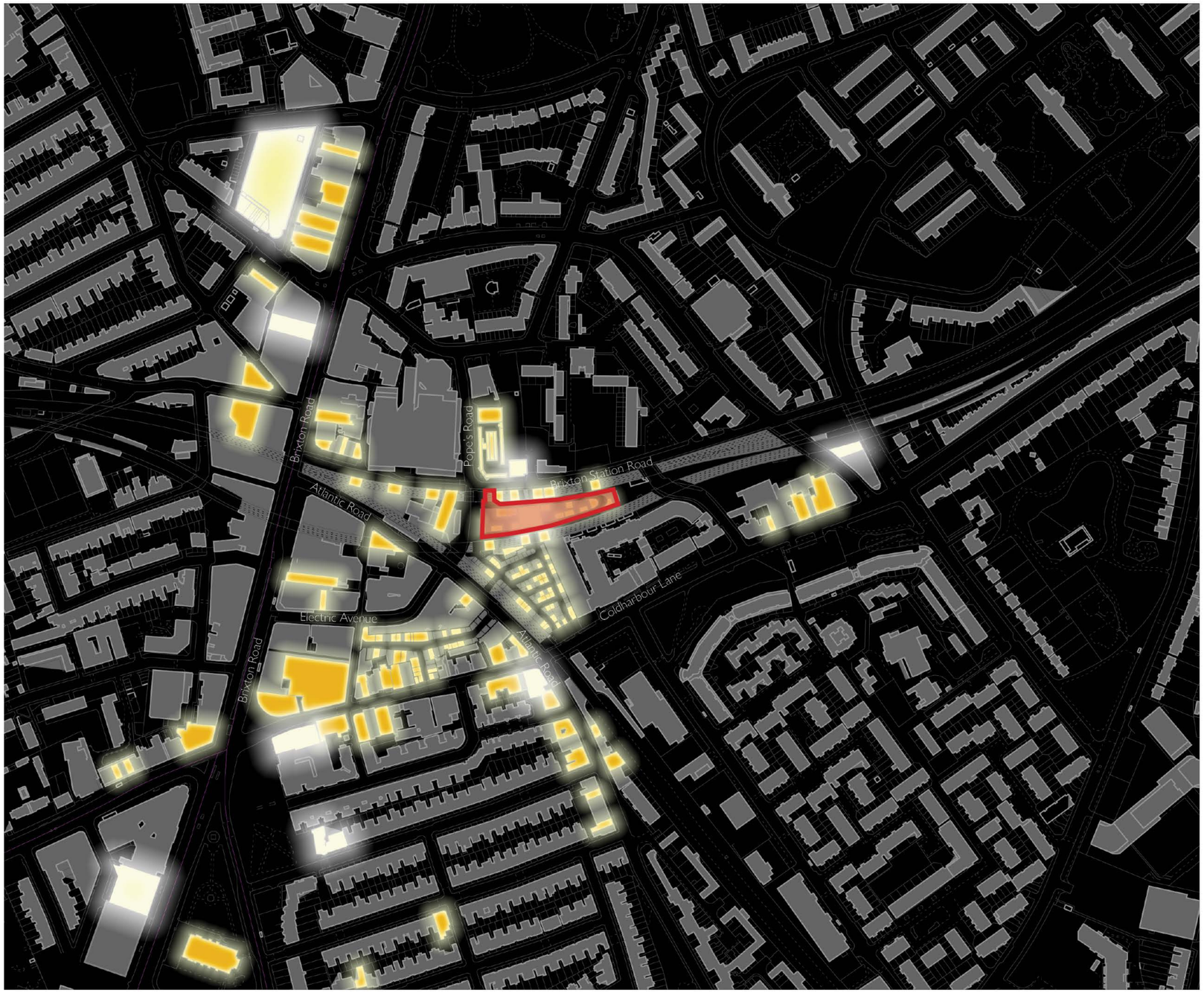
Medium sized cultural and entertainment venues such as Brixton Academy, Brixton Windmill, The Ritzy Cinema Café and bar alongside the introduction of Night Tube services on the Victoria Line, have marked the area as a leading visitor destination.

This is balanced by the food and drink offer in Brixton Market Village, Pop Brixton and beyond that further adds to the inviting community creating a vibrant nightlife atmosphere.

Building on the existing conviviality of Brixton and in response to its regional growth, our design aims to support the evening increase in visitor footfall. By activating the underused and neglected site in Pope's Road our proposal creates safer neighbourhoods through natural surveillance.

The introduction of an improved pedestrianised network route that opens up the front of Brixton Station and Pope's Road towards the East, will help to reduce the impact of congested areas during peak times.

In response to deterioration of certain parts of the Brixton and the lack of public toilet provisions, our proposal will also replace and increase the number of public toilets, making them free and maintained.



Key

- Eating & Drinking spaces
- Cultural Venues (incl. Art & Music venues)
- Site Boundary

Fig. 2.8.2 Towncentre & Uses Night Plan

2.9 PROTECTED VISTAS & AREAS

During the early design stages and after an extensive analysis of the Brixton and Loughborough Park Conservation Areas and surroundings, all the relevant views are considered in the THVIA report.

The site is located outside of the viewing corridors defined on the map but would nonetheless be visible in views from Brockwell Park and Electric Avenue and other points mentioned in the Brixton Tall Building Study, and these views have informed the design development.

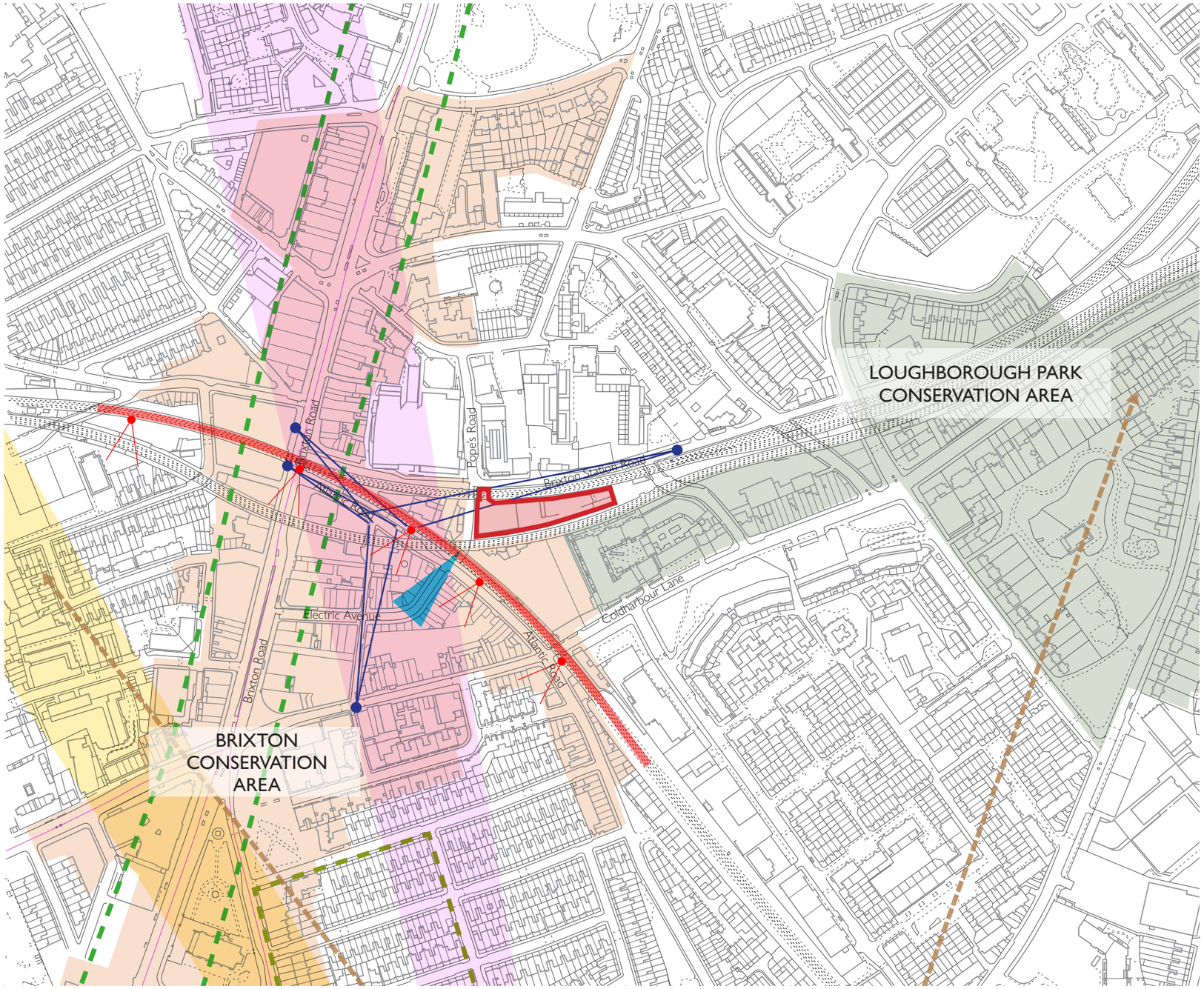


Fig. 2.9 Protected Vistas & Areas Plan. Source -Brixton Tall Building Study 2014

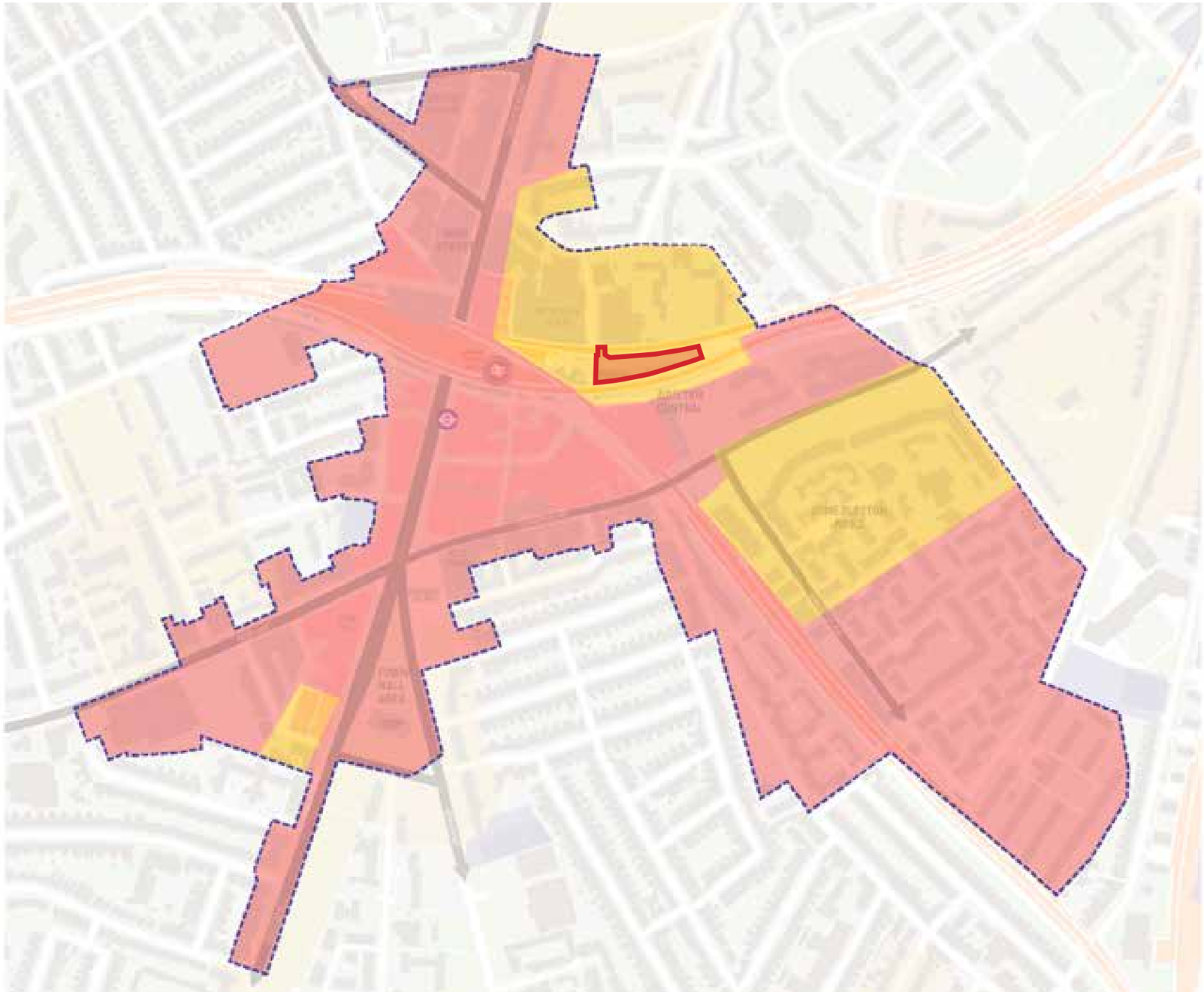
2.10 POLICY CONTEXT

2.10.1 POLICY Q26

This policy is from the adopted Local Plan. Policy Q26 outlines:

Proposals for tall buildings will be supported where:

- They are not located within areas identified as inappropriate for tall buildings in Annex 11;
- The site is not located within an area deemed "Inappropriate to Tall Buildings" but within an area identified as "Sensitive to Tall Buidlings"



Area outside dotted line has not been assessed

Fig. 2.10.1 Brixton SPD Area, Lambeth local plan. Source: Lambeth Tall Buildings Study, 2014

2.10 POLICY CONTEXT

2.10.2 SITE ALLOCATION

Lambeth Local Plan Semptember 2015 identifies this site as Site 16-Brixton Central between the viaducts SW9.

The preferred use highlighted in the document outlines

Improvements to Brixton Station to include a new station entrance and pedestrian links. Mixed-use development including retail, new workspace, food and drink, community, educational, leisure and recreation uses, possible market extension and associated uses. Development to include revitalised railway arches with options to provide links through to improve north-south routes.

Large site bounded by railway viaducts with considerable development potential subject to improving access and permeability through the site.

Key design considerations outlined in the document

(i) provides opportunities to improve the station entrance and station facilities generally, including lift access and cycle parking;

(ii) provides public realm improvements to Brixton Station Road to include links to both the mainline and underground stations, the opening up of arches to provide links to north-south routes east of Popes Road and potential links to Brixton Village;

(iii) enhances the arches to provide active uses and routes through;

(iv) opens up Popes Road to provide a wider public space with the potential to provide improved and/or additional market spaces;

(v) includes market facilities;

(vi) includes environmental improvements to the viaduct arches serving the Orpington Line;

(vii) integrates and complements development on the Popes Road site (Site 15);

(viii) includes the reprovision of the redundant 1950s building;

(ix) avoids creating a canyon on either side of the railway viaducts;

(x) proposes low buildings to protect the amenity of new residential development on Coldharbour Lane adjoining the site.

Key

- Site 16 Boundary
- Listed Buildings
- Opportunities to open up arches-Pedestrian linkage
- Extending Commercial Activities
- Opportunity to Reinstate Station Entrance
- London Underground Station
- Rail Station
- Conservation Area
- Application Site Boundary

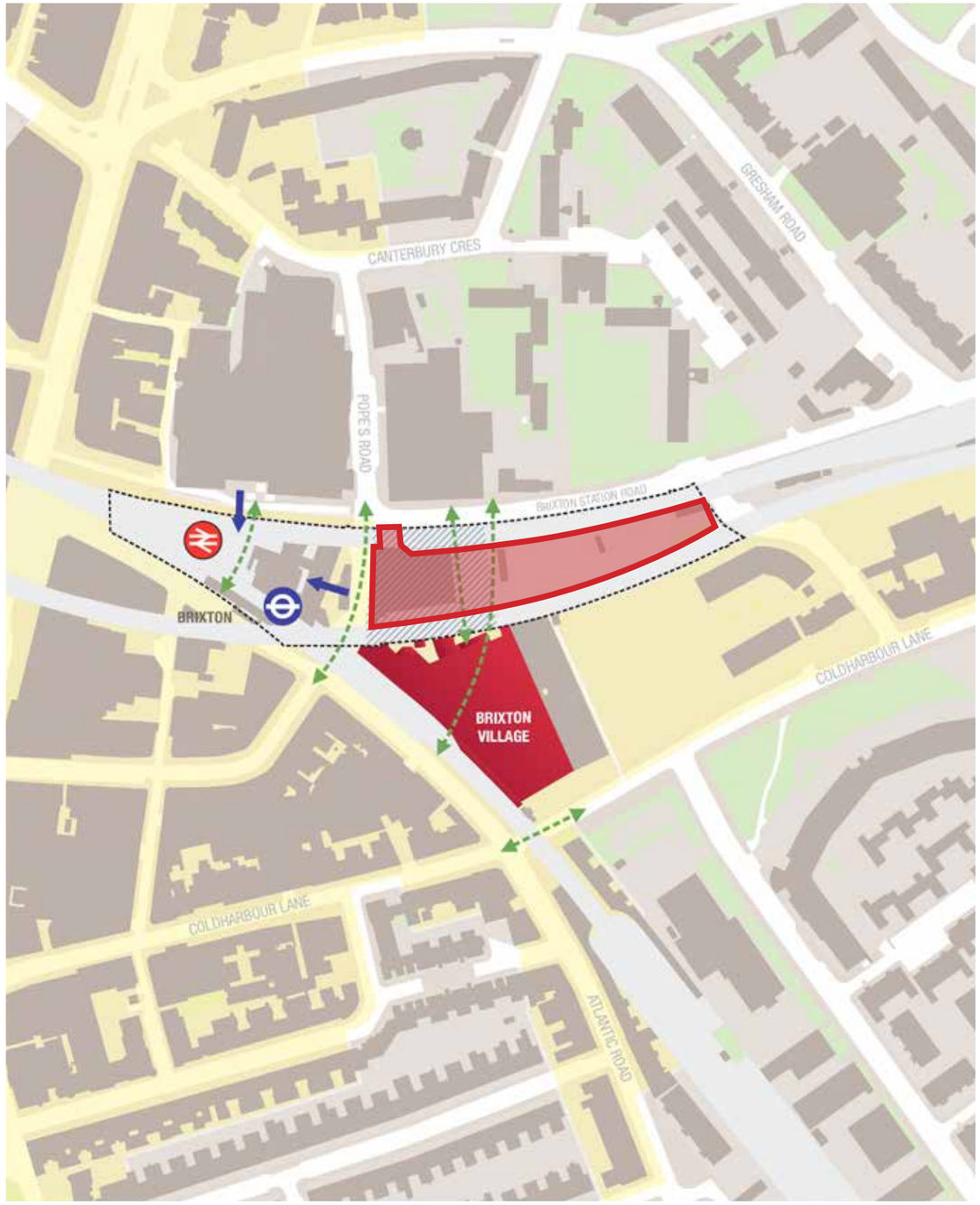


Fig. 2.10.2 Site 16-Brixton Central_between viaducts_SW9. Source: Lambeth Local Plan, 2015

2.11 STREETSCAPE

2.11.1 STREETSCAPE

The site is enclosed on the North and South by railway viaducts which effectively constitute the main internal street elevation of the site. However on Brixton Station Road and Atlantic Road, a consistent number of the viaduct arches are populated with retail units that create an active street frontage along with the street market.

The only street facing façade of the site is the west which overlooks Pope's Road where the only building present is the derelict one storey timber clad volume occupied by a public toilet.

The west end of the site however represents on the south the visual termination of Electric Avenue with its high quality architectural heritage. The building on Electric Avenue show a clear tripartite structure of the façade characterized by and active ground floor a rigorous middle and a top part.
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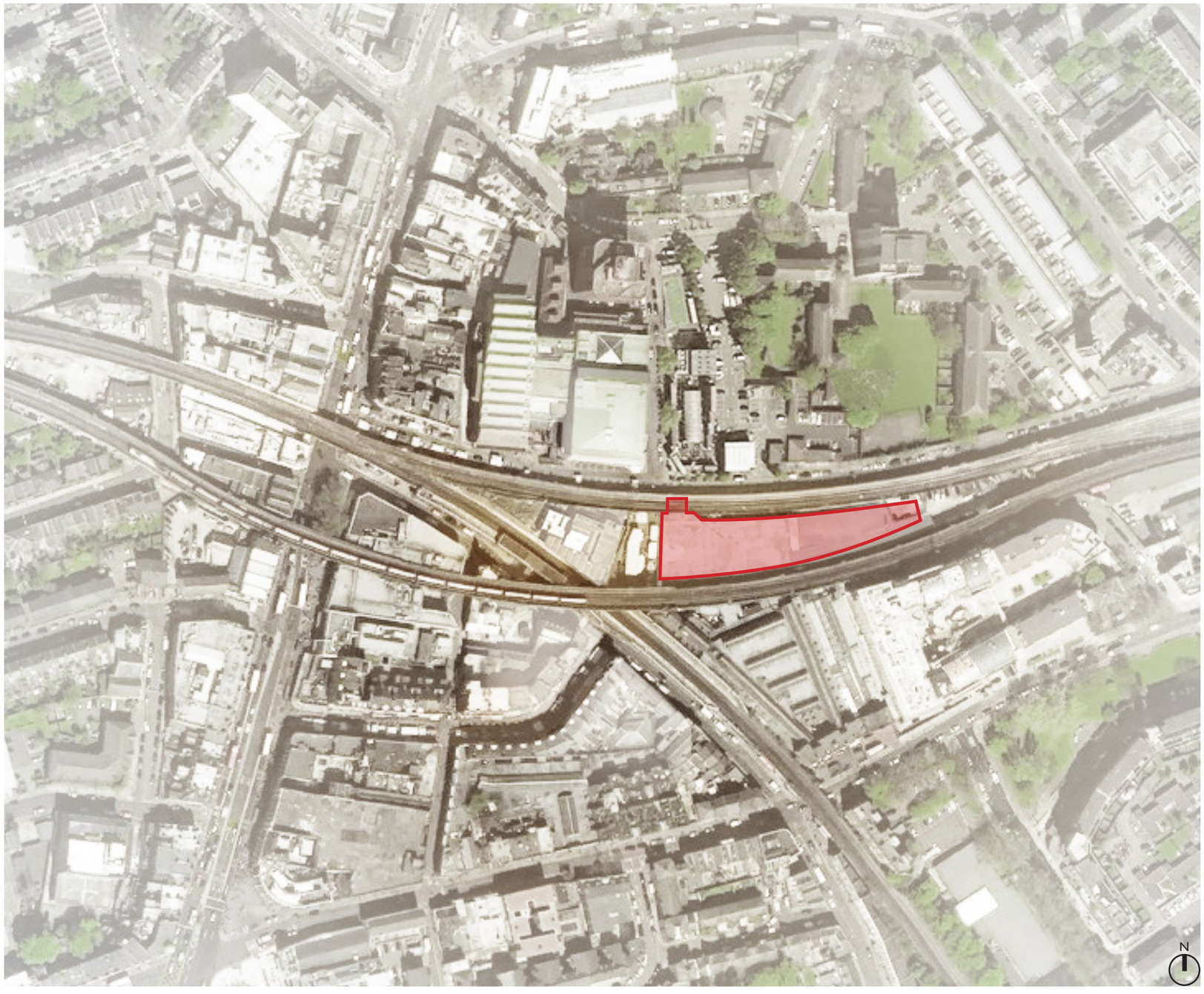


Site Boundary

Fig. 2.11.1 Map of relevant streetscape surveyed

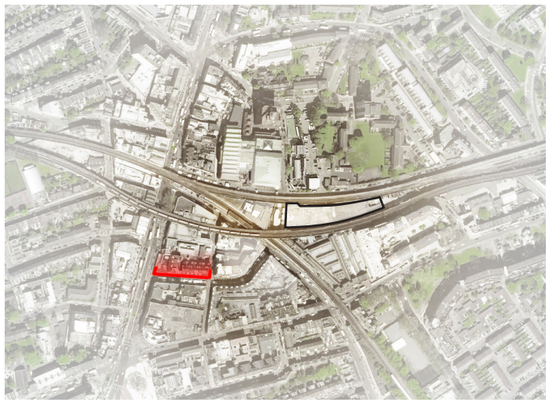
2.11 STREETSCAPE

2.11.2 ELECTRIC AVENUE NORTH

The north side of Electric Avenue is characterized by mostly Victorian buildings with more modern addition on the west end of the street. Brick is the main material used in a orange yellow tone.

The buildings along Electric Avenue are generally defined by two main parts: the base of the building, public realm interface, and a second part which corresponds to the main volume of the building and its upper levels.

The top of the buildings have created a third part which, following a tripartite stance, often corresponds to the roof, mansard or set back storey.



20-20A Pope's Road

Key:

C	Top of the building, often a roof, mansard or set back storey(s)
B	Main volume of the building
A	Base of the building, interface with the public realm

2.11.3 ELECTRIC AVENUE WEST

The west side of Electric Avenue is characterized by hybrid Victorian and Georgian buildings which shows a similar organization as the one in the north side. This part of the street is characterized by a red brow tonality.

In all the buildings present on this street the base are always very porous and active and in a way the market stalls of the street market almost blend in with the base of the buildings.

20-20A Pope's Road

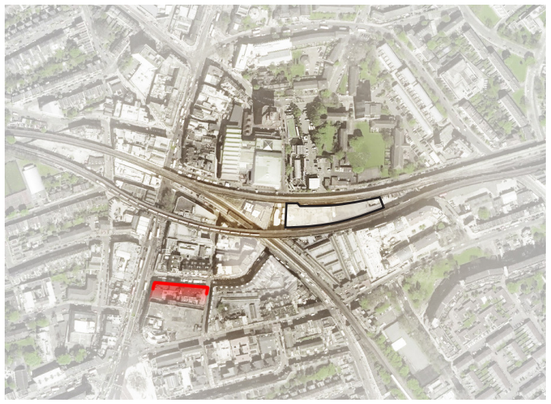
Key:

C	Top of the building, often a roof, mansard or set back storey(s)
B	Main volume of the building
A	Base of the building, interface with the public realm

2.11 STREETSCAPE

2.11.4 ELECTRIC AVENUE SOUTH

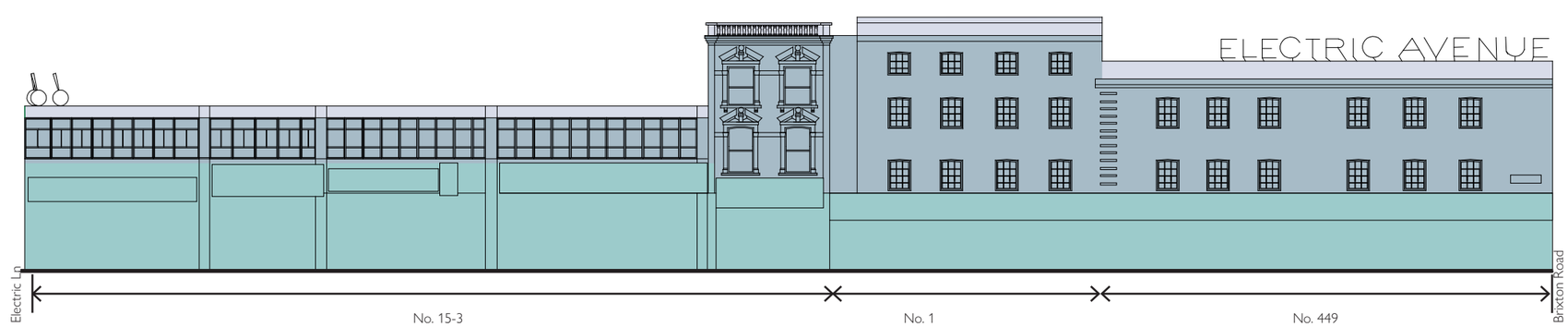
On the south side of Electric Avenue All the buildings retain a very active and porous ground floor.
Despite the stark differences in style all buildings have a more or less marked tripartite organization.



20-20A Pope's Road

Key:

C	Top of the building, often a roof, mansard or set back storey(s)
B	Main volume of the building
A	Base of the building, interface with the public realm



2.11.5 ELECTRIC AVENUE EAST

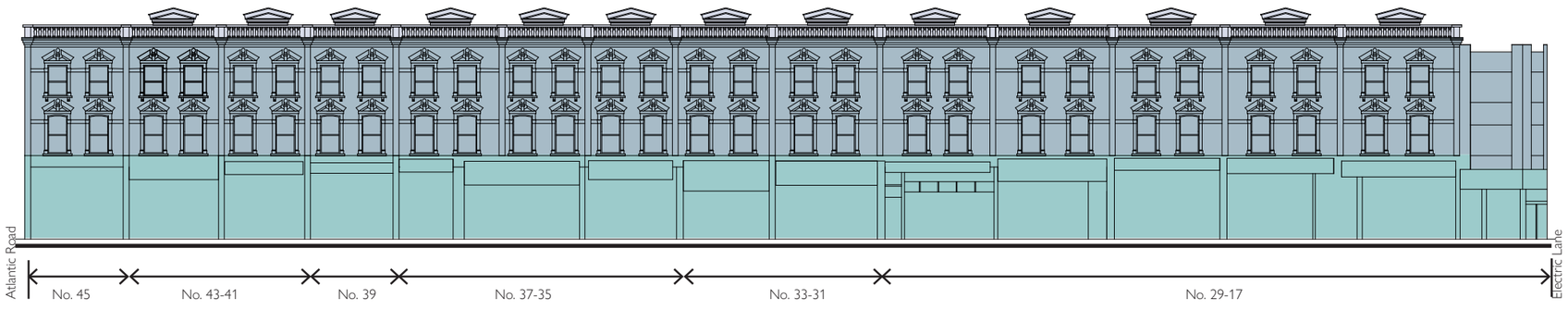
The east side of Electric Avenue presents a more typical set of Victorian buildings which dispalys tripartite façades with porous base.

The tonality of these façades is defined by the orange brown load bearing brick used. A more modern addition can be found on the intersection with Electric Lane.

☐ 20-20A Pope's Road

Key:

C	Top of the building, often a roof, mansard or set back storey(s)
B	Main volume of the building
A	Base of the building, interface with the public realm

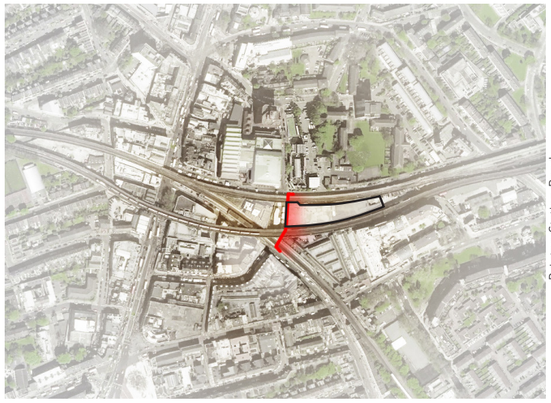


2.11 STREETSCAPE

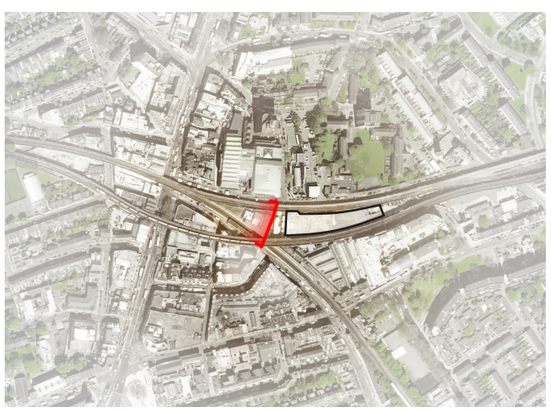
2.11.6 POPE'S ROAD EAST - WEST

Popes Road represents the only street facing side of the site. In the current condition on the west side where the site is located, the street façade is occupied by a single storey building containing two large retailer which have glazed front and big signage on an horizontal spandrel.

On the east side the foreground is occupied by a semi derelict building containing the public toilet and a metal fence which blocks the access to the station building in the background. On the sides below the viaducts on both east and west market stalls effectively constitute the street elevation.



☐ 20-20A Pope's Road

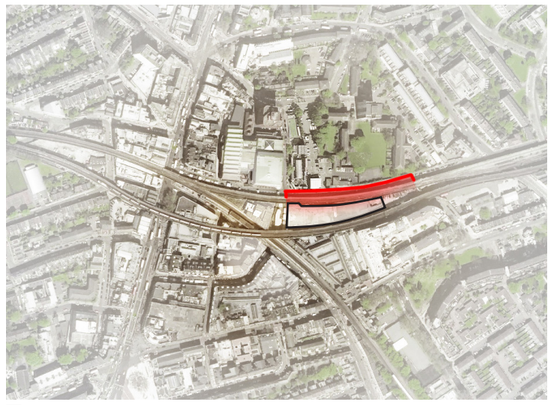


☐ 20-20A Pope's Road

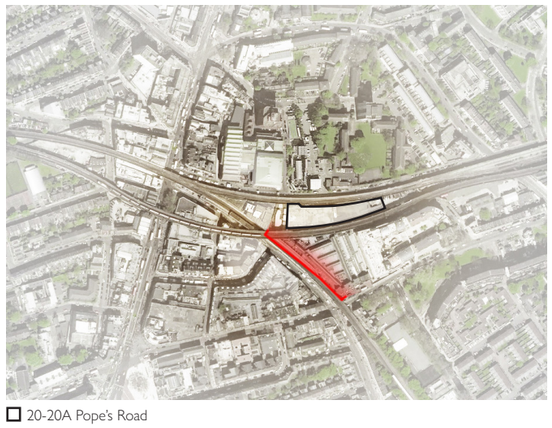
2.11.7 RAILWAY ARCHES

The north and south sides of the site are bound by railway arches which have a clear street presence on Brixton Station Road. Parallel to this, the more concealed by signage and supplementary structure on Atlantic Road.

The rhythmic structure of the arches is interrupted by different retail units which are tucked into them which present frontages characterized by different signage, colours and styles



20-20A Pope's Road



20-20A Pope's Road

2.12 EXISTING BUILDING

2.12.1 EXISTING BUILDING OUTLINE

The existing building at 20-24a Pope's Road is a one storey building containing two mid-size retail units that are accessible directly from Pope's Road.

The existing building currently occupies most of the site and the frontage on Pope's Road spans from viaduct to viaduct blocking the east west circulation towards Valentia Place.

The building has a stepped roof which creates two flat platforms that can be accessed by vehicles from Valentia Place on the eastern end of the site.

Existing Building outline

Fig. 2.12.1.1 Plan View

Existing Building outline

Fig. 2.12.1.2 Aerial View looking east

Existing Building outline

Fig. 2.12.1.3 Aerial View looking west

Existing Building outline

Fig. 2.12.1.4 View looking north-east from railway platform

2.12.2 EXISTING BUILDING STREET VIEWS

Due to the shape and location of the site bound in between two railway lines the existing building has effectively only one street facing façade on the west on Pope's Road.

On the east elevation there is a service yard whilst the north and south street elevation are represented by the railway arches and partially non-accessible to the public.

Existing Building outline

Fig. 2.12.2.1 Views keyplan

2.12 EXISTING BUILDING

2.12.2 EXISTING BUILDING STREET VIEWS

Facing Pope's Road, the building is presented by a small cantilever capped by a thick horizontal spandrel that contains the signage that marks the entrances of various retailers. The main frontage is glazed and set back below the cantilever.

On the east side, Valentia Place is currently used as a service yard and characterized mainly by a series of gates which grant access to the site, the roof of the building and to the side streets that run east west and are parallel to the railway lines.

The side street on the north is currently not accessible to the public whilst only a portion of the street on the south can be accessed. When accessible the existing building elevations on these streets are mostly composed of an unremarkable brick wall and capped by a metal fence.

1 Fig. 2.12.2.2 View from Pope's Road looking north

2 Fig. 2.12.2.3 View from Pope's Road looking south

3 Fig. 2.12.2.4 View from Valentia place looking west

4 Fig. 2.12.2.5 View from side street looking west

5 Fig. 2.12.2.6 View from street looking west

2.12.2 EXISTING BUILDING STREET VIEWS

During the night, the retail frontages on Pope's Road are fairly well lit with the addition of street lights.

However the east, north and south part of the site are overlooked and there is poor illumination making the public circulation around the site difficult and unsafe.

This also contributes to low levels of natural surveillance in the public area.

1 Fig. 2.12.2.7 View from Pope's Road looking north

2 Fig. 2.12.2.8 View from Pope's Road looking south

3 Fig. 2.12.2.9 View from Valentia place looking west

4 Fig. 2.12.2.10 View from side street looking west

5 Fig. 2.12.2.11 View from street looking west

2.12 EXISTING BUILDING

2.12.3 EXISTING BUILDING ORGANISATION

The existing building is organized by an east-west elongated and linear set of spaces with two main retail spaces set in the west part of the site.

A wider storage area on the east part is directly accessible from the service yard located on Valentia Place.

The rear of the building presents a gated ramp which provides access to two vehicular accessible roofs located on two different levels.

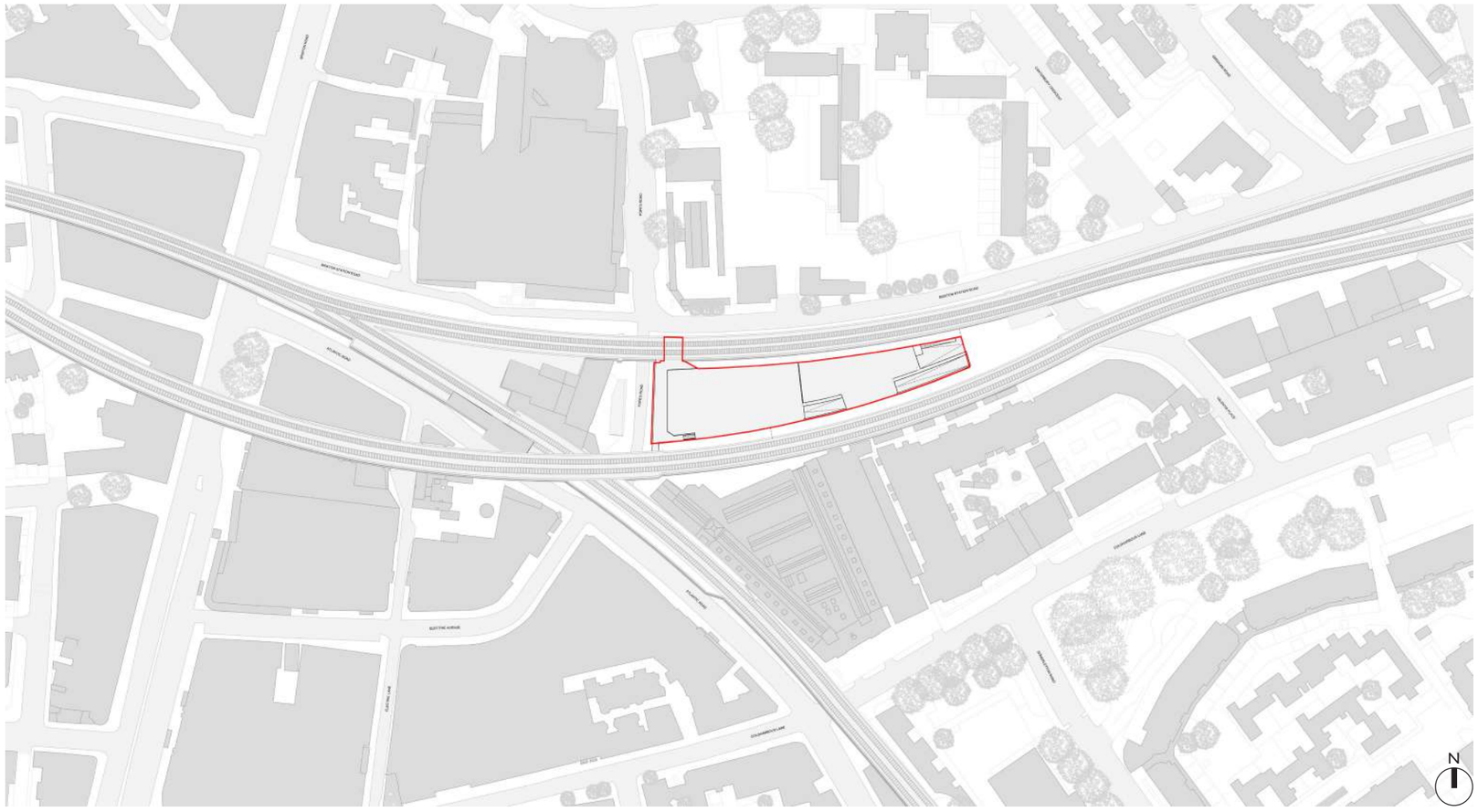


Fig. 2.12.3.1 Existing Building Location Plan

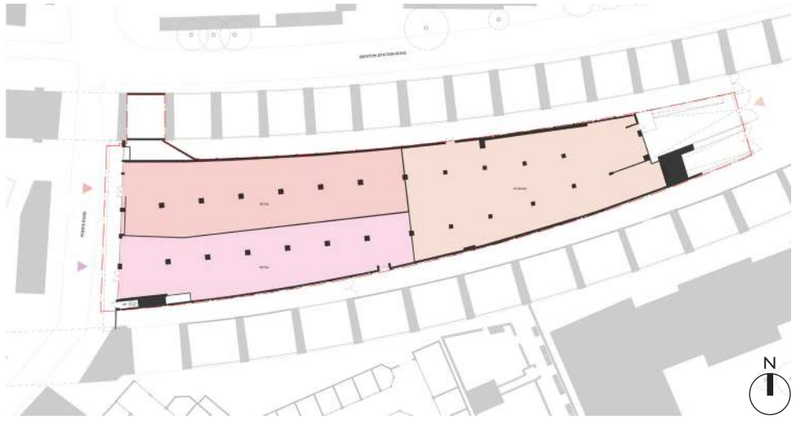


Fig. 2.12.3.2 Existing Building Ground Floor Plan - Uses

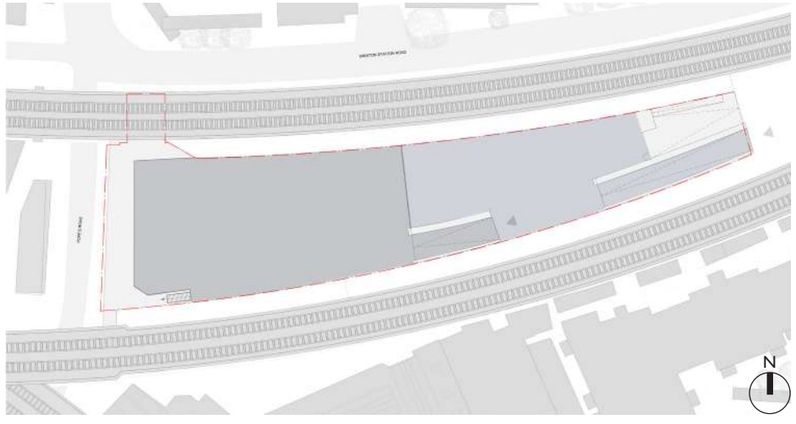


Fig. 2.12.3.3 Existing Building Roof Plan - Uses

Key

- Existing Retail Unit 1
- Existing Retail Unit 2
- Existing Storage
- Elevated Parking 1
- Elevated Parking 2
- Site Boundary

2.12.4 EXISTING BUILDING ELEVATIONS

The main and only elevation of the existing building is the west facing Pope's Road whilst the other three are a result of the functional and organisational layout of the building

Fig. 2.12.4.1 West Elevation

Fig. 2.12.4.2 East Eelevation

Fig. 2.12.4.3 South Elevation

Fig. 2.12.4.4 North Elevation

DESIGN CONSIDERATIONS

3

3.1 CONCEPT & DESIGN BRIEF

The brief required a design that focused on the opportunity to celebrate and generate future growth within Brixton, through redevelopment of the Pope's Road area. In understanding and analysing the existing context and fabric, our response aims to enhance and reconfigure Pope's Road, by making it the new commercial focal point for Brixton Central.

The design introduces a new flexible building that meets the office space demand, realising the ambition for more businesses and creative industries to start up and thrive. This is complemented by maximising the need for greater public and open space for improved circulation, accessibility and connectivity.

This also identifies the potential to integrate and extend the existing Brixton market by opening it up to allow for increased local investment. The design of an additional and extensive two floor flexible market space, is utilised with a communal area in the centre for social gathering and events. This market extension further enhances the style, culture and character that has made the area of Brixton so popular in recent years.

With this motive in mind, our approach throughout the design process has been executed through the analysis of the context, understanding its origins, character, materiality and other architectural elements that comprise the Pope's Road area. We have also responded to the local communities' needs through public consultations and engagement

The result is a design that draws inspiration from the context, seeking to create a mixed-use building of contemporary high quality to the heart of Brixton.

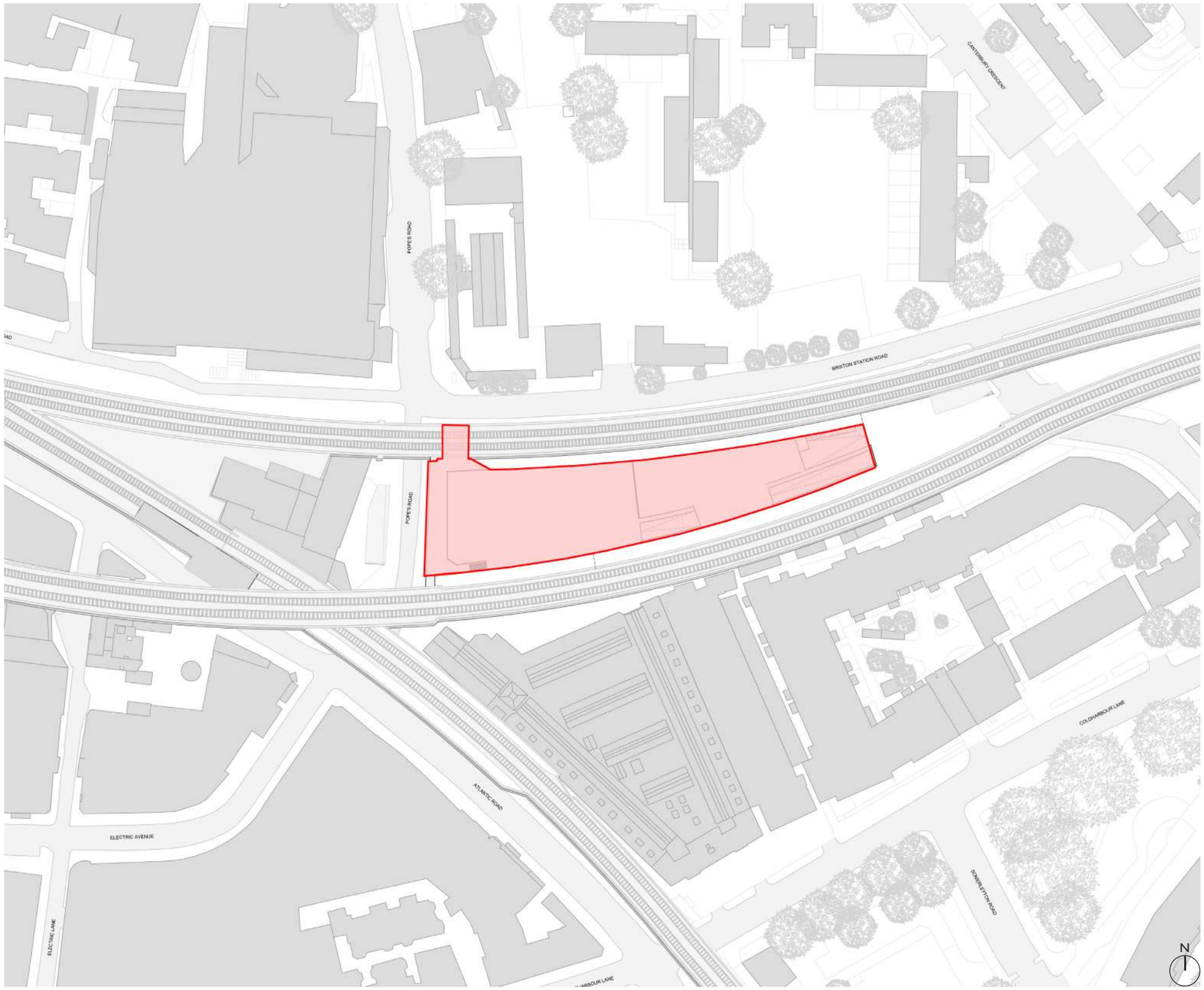
3.2 DESIGN OBJECTIVES

The project's site location is key to the success of Brixton Village. Its redevelopment constitutes a unique opportunity for the creation of a building that complements the character of the area and further enhances the distinctive Brixton Market and wider area.

The demolition of the existing building creates a further opportunity to introduce a holistic new development of office spaces, as well as public realm offer and improvements.

Our proposal will seek to:

- Create a distinctive development of high architectural quality that delivers flexible retail and office space.
- Provide a new design that draws inspiration from the historic context of the site and its surrounding buildings.
- Provide 18 floors of highly efficient office space that help meets current local demands
- Provide 2 floors of publicly accessible market space in line with the character of the existing Brixton Market, in turn increasing permeability and benefitting local traders.
- Provide a restaurant that will offer views over the City of London
- Improve public realm areas with greater permeability and facilitate potential future connections to local transport link



Application Site Boundary

Fig. 3.2 Location Plan

3.3 MASSING ANALYSIS

The site is bound by two railway structures corresponding to the south-eastern rail and the TFL Overground network in the north and south respectively.

Due to these site parameters, there is currently only one active façade to the site and limited access to the north/south and east sides of the building.

Accordingly, the proposed massing is designed in response to several constraints and opportunities imposed by the site conditions, these include:

- Site Boundary
- Permeability Potential
- Volume Articulation and central open accessible space
- Rights of light of surrounding buildings

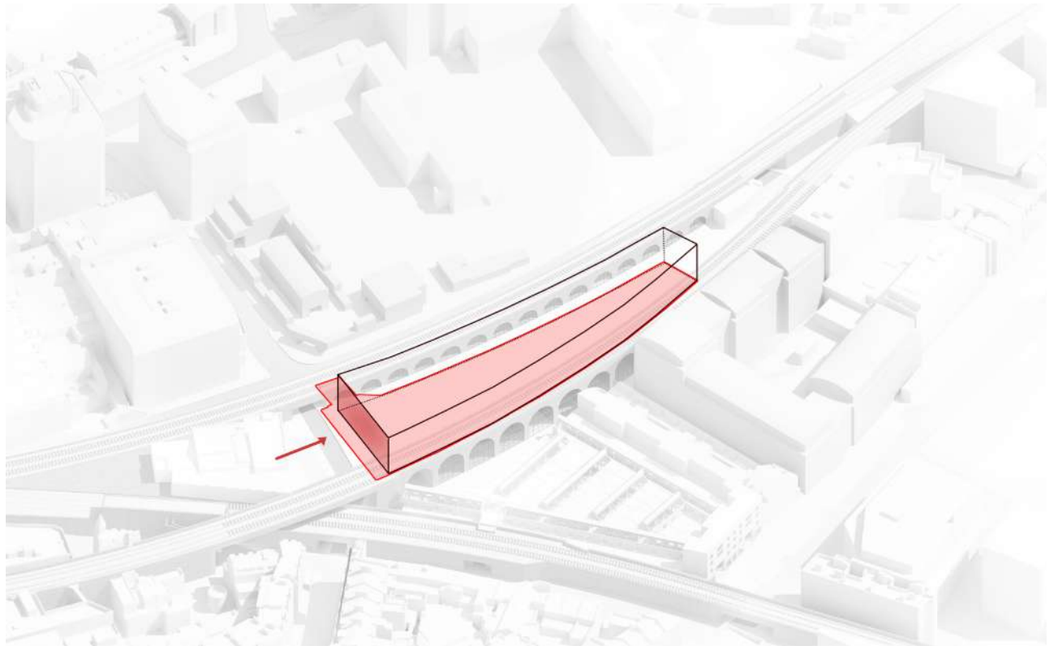


Fig. 3.3.1 Site Boundary diagram

SITE BOUNDARY

The project's site is unconventional, tapering from west to east. Due to the boundary's particular shape – which is inherently inefficient –the massing has been positioned directly up against the boundary line. An exception has made for the west boundary on Pope's Road, where a set back has been provided.

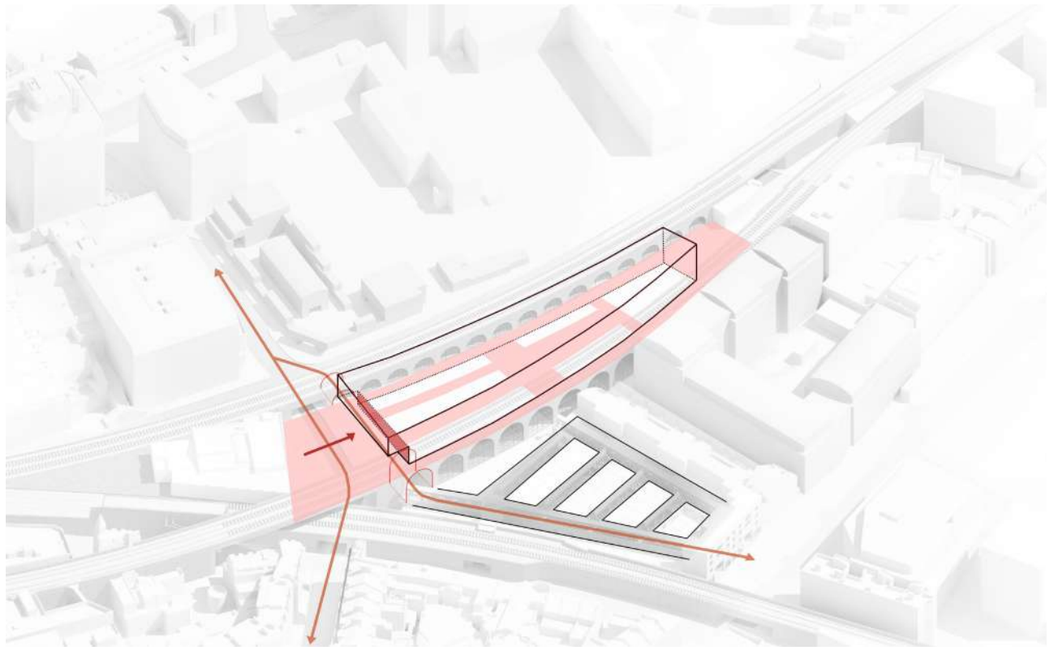


Fig. 3.3.2 Permeability Potential diagram

PERMEABILITY POTENTIAL

Our scheme takes into account the opportunity for facilitating permeability is taken in a north-south capacity on Pope's Road. It also aims to facilitate connection with Brixton Village by maintaining through access between the first two arches via north and south. Finally, the scheme facilitates an east-west connection within the ground floor, and opens up side streets for servicing and Network Rail maintenance.

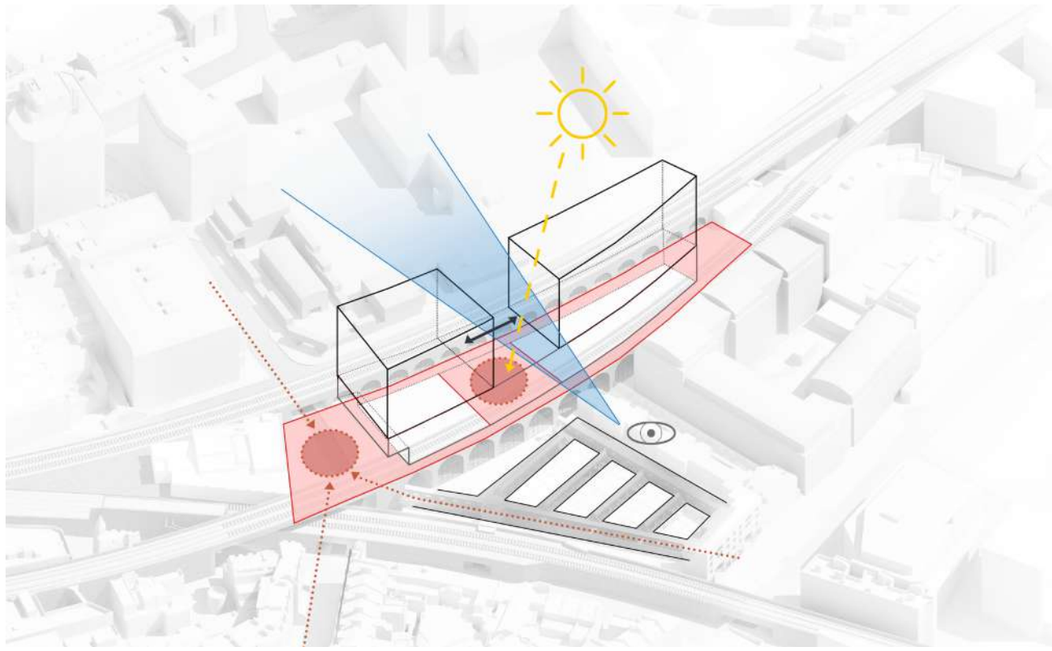


Fig. 3.3.3 Volume Articulation & Natural Light diagram

VOLUME ARTICULATION & NATURAL LIGHT

In order to mitigate the site's unconventional, linear shape – avoiding the creation of a wall-like effect – the volume of the scheme has been is split into two major parts. This creates two focal points: one on the west on Pope's Road; and another in the centre of the site. The split allows natural light to permeate in through the site's centre and also works to allow visual permeability through the void.

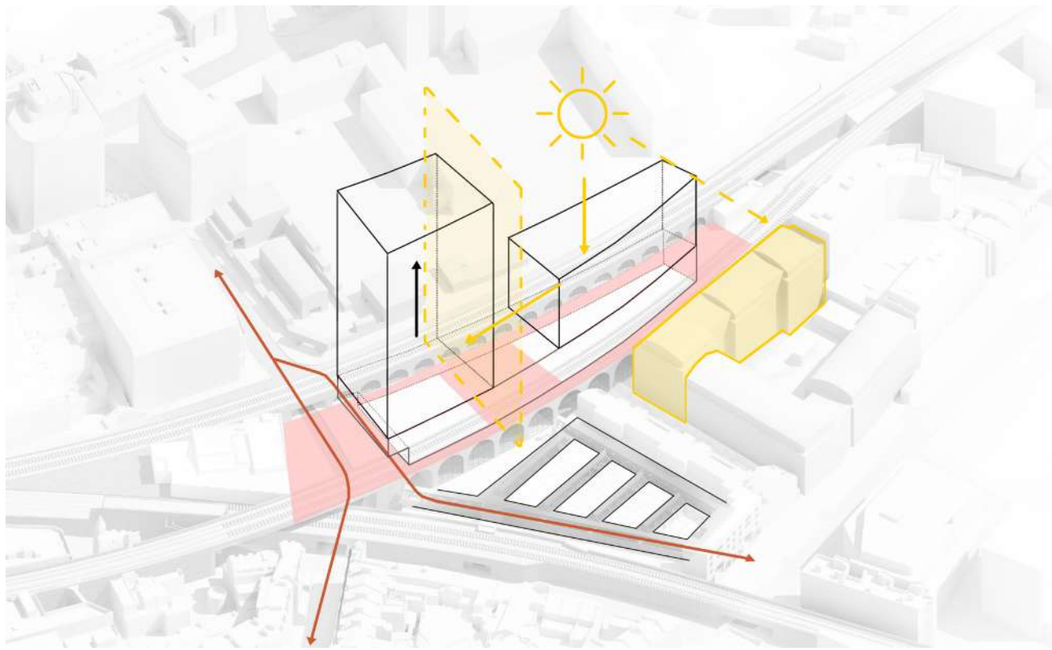


Fig. 3.3.4 Daylight and Sunlight diagram

DAYLIGHT & SUNLIGHT

The proposed scheme actively addresses daylight / sunlight rights for the neighbouring properties to the south, and accordingly, the height of the east block has been limited. The size of the west block has also contained on the west in order to avoid infringing on neighbouring daylight sunlight rights. More height has been proposed on the west block, which is the only and primary street face of the building that faces Pope's Road. This increased height aims to create a recognisable and sensitive landmark for the town centre of Brixton.

3.4 PUBLIC REALM INTERFACE

3.4.1 PUBLIC REALM EXISTING CONSTRAINTS

Following analysis and frequent site visits, the following constraints affecting the development have been identified:

- The recently refurbished structure now housing two retail stores is of poor architectural quality, with metal bollards running the full length of the site, creating a barrier between site and pedestrians.
- Public realm is limited to Pope's Road, and its activity is restricted to the stores' business hours, encouraging decreased security and antisocial behaviour at night time.
- The structure housing the area's public toilets is extensively deteriorated and its condition as a standalone building limits the public realm extent to its frontage.
- The position of the public toilet within the public realm creates unused spaces beside and behind the structure, where rubbish and other nuisances accumulate.
- There is no connection between Pope's Road and the corridors between site and railways arches.
- The side streets access is currently limited to servicing and Network Rail maintenance.

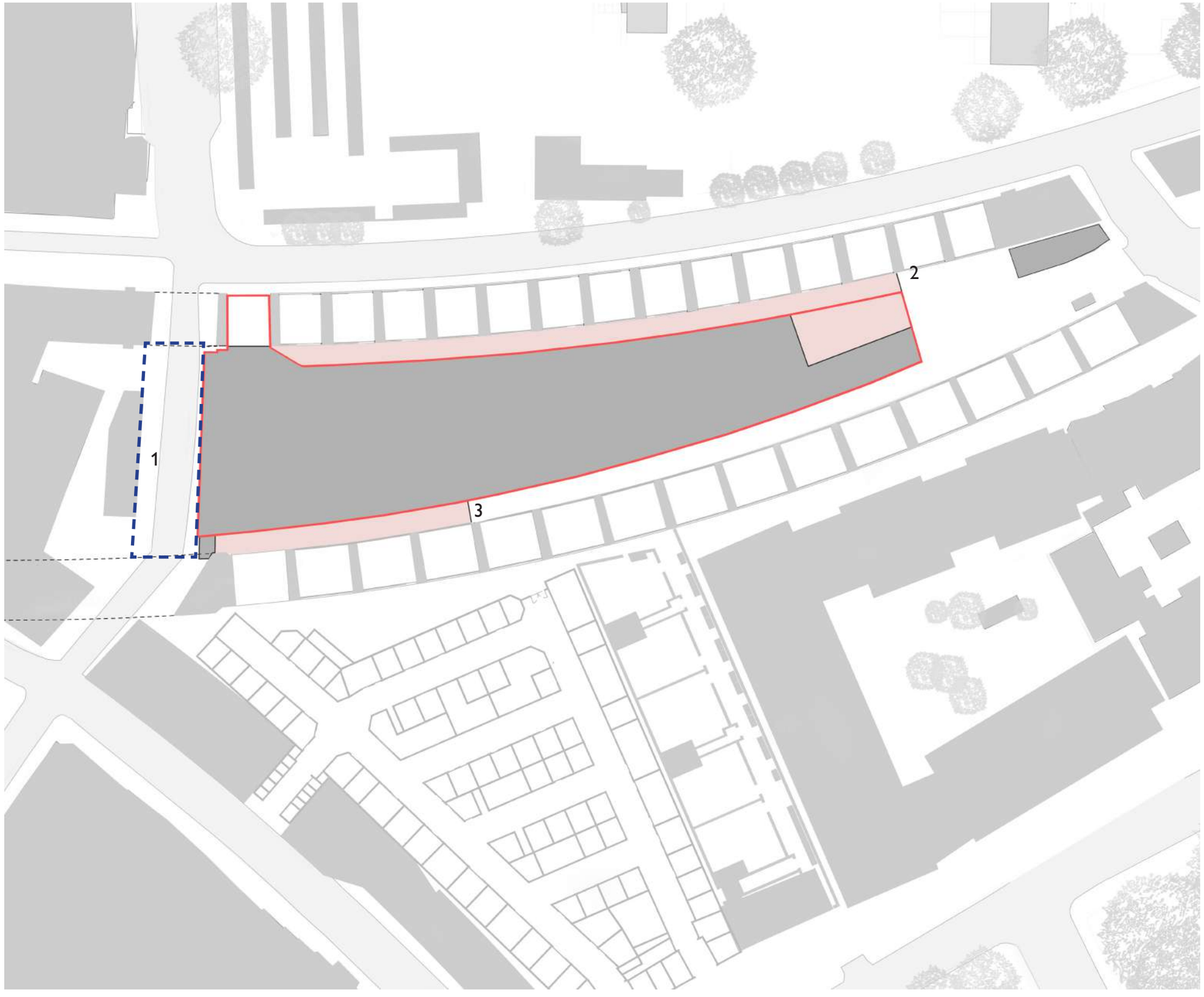


Fig. 3.4.1.1 Public realm existing contraints plan

1 Fig. 3.4.1.2 Compressed Public Realm on Pope's Road

1 Fig. 3.4.1.3 Poor quality building on Pope's Road

2 Fig. 3.4.1.4 Gates limit the east-west circulation around the site

3 Fig. 3.4.1.5 Poor quality of the Public Realm

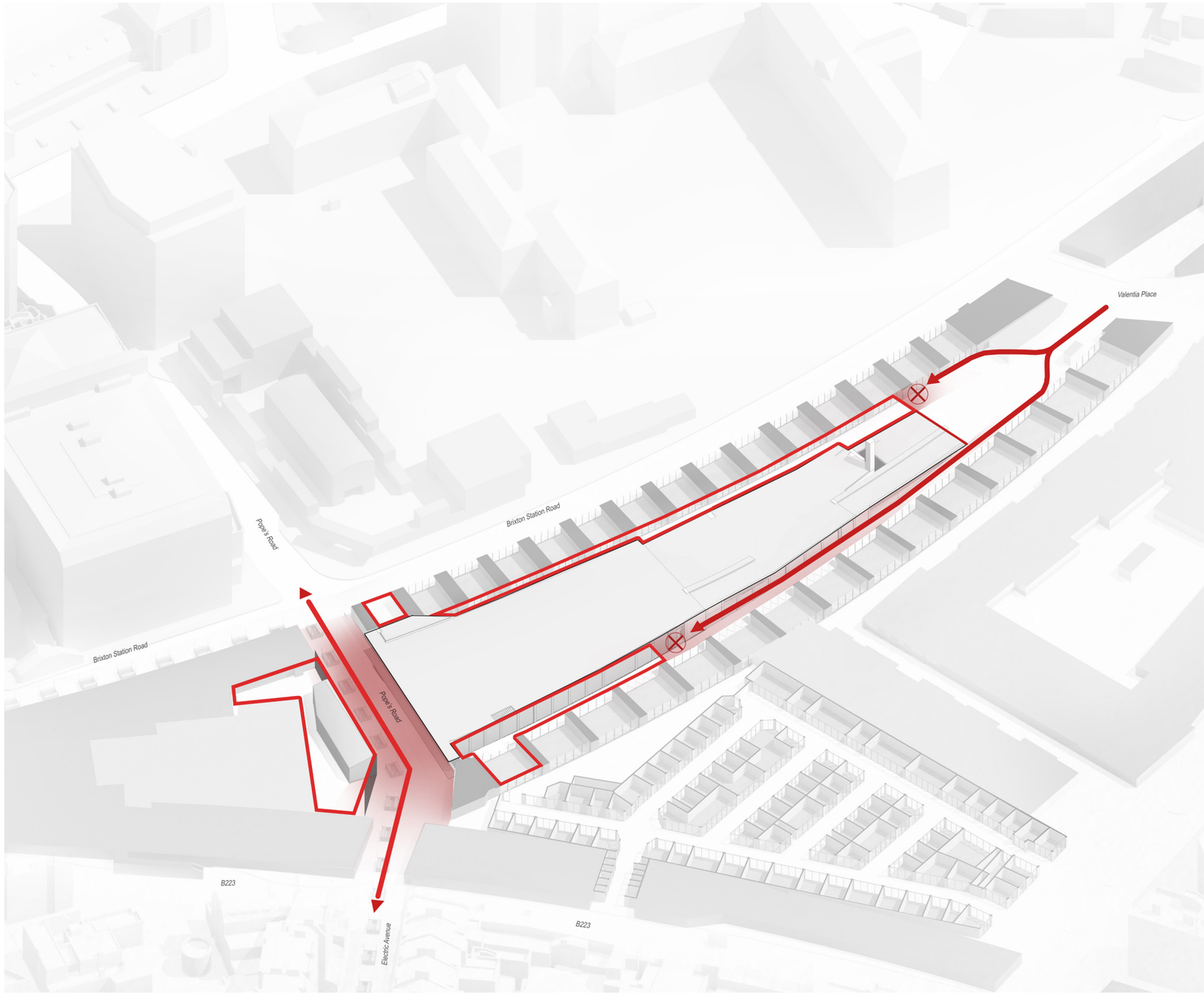
3 Fig. 3.4.1.6 Gates limit the east-west circulation

3.4 PUBLIC REALM INTERFACE

3.4.2 PUBLIC REALM INTERFACE - EXISTING

The existing condition of the public realm within and around the site poses a number of issues that can be summarized as follow:

- The public realm on Pope's Road whilst limited in space is able to deal with the peak pedestrian flows during the weekend when the street market is operational, however it is compressed particularly due to the presence of the derelict building which accommodates the public toilet and its metal fence
- There is a lack of connectivity North South particularly between the Brixton Village and Pope's Road



Key

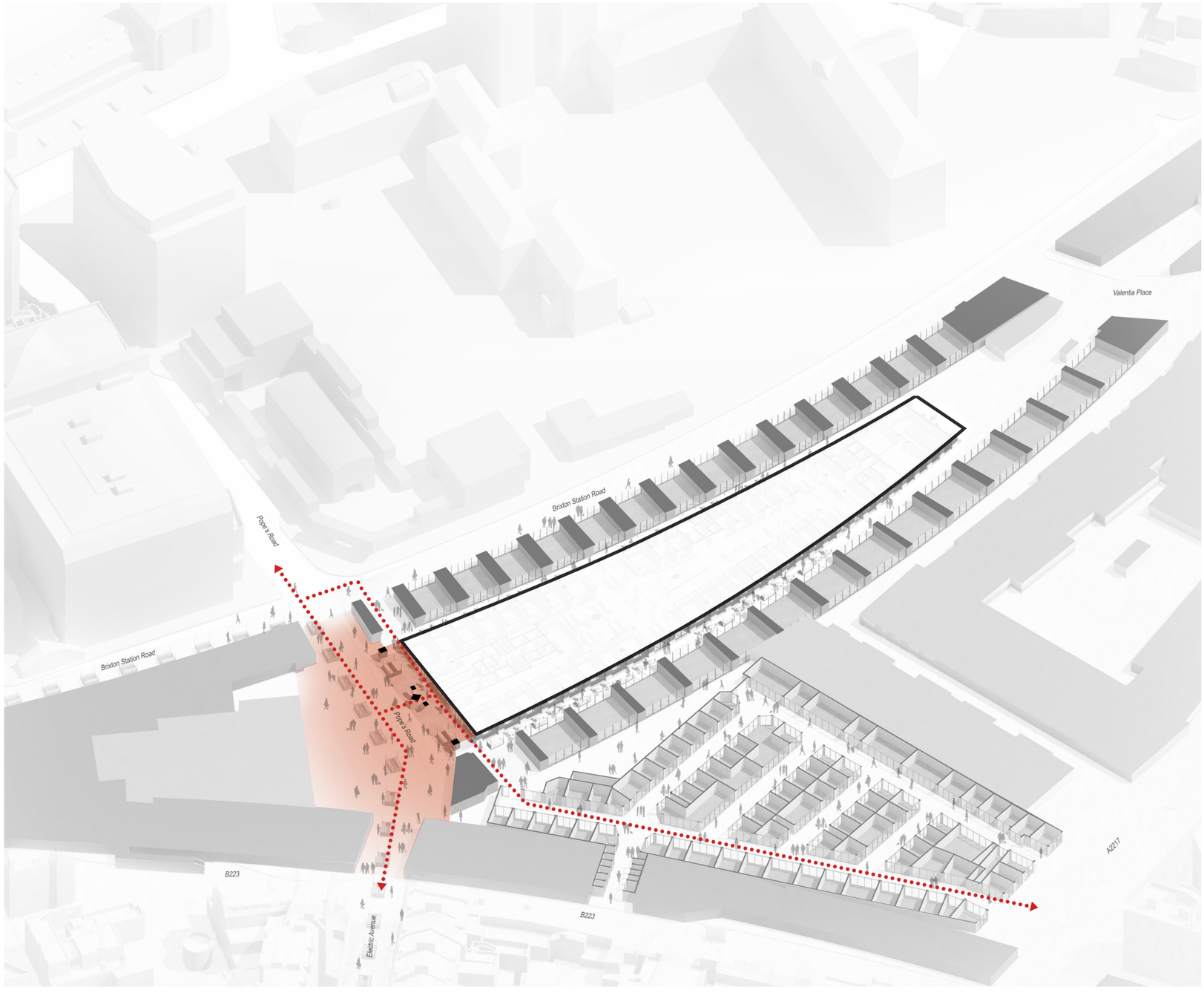
- Restricted access areas
- Poor quality public realm & inactive frontages
- Pedestrian paths
- Existing Gates

Fig. 3.4.2 Existing public realm interface axonometric diagram

3.4.3 PUBLIC REALM OPPORTUNITIES

The proposal seeks to facilitate optimal permeability whilst improving the overall quality of the public realm. The various public realm opportunities that have been identified as follows:

- Activating the frontage of Pope's Road by creating a more generous and active public realm creating a soft threshold from the public space into the proposed market. The addition of heightened night activation will encourage residents to use the streets more in the evening and increase natural surveillance
- Facilitating the future North South permeability, particularly into the Brixton Village, by creating a permeable market with publicly accessible market streets inspired by the arcades and rows found in the existing Brixton market
- Introducing a generous covered publicly accessible open space in the center of the site that can host various community activities and potentially facilitate the future connectivity between the Brixton Village and the Pop Brixton site



Key

Potential Public Realm opportunity on Pope's Road

Potential improved paths

Fig. 3.4.3 Public realm opportunities axonometric diagram

3.5 ARCHITECTURAL LANGUAGE - FAÇADE ARTICULATION

3.5.1 ARCHITECTURAL HERITAGE

Brixton has a rich and diverse architectural heritage that includes a plethora of styles, movements and time periods.

Our research-led team has undertaken rigorous study into the architectural heritage of Brixton, allowing us to better understand the area's context and inform our proposed design.

In doing so, we have recorded examples of Victorian and Georgian buildings existing within the site's neighbourhood, particularly the buildings on Electric Avenue, which are of significant architectural merit.

In Brixton's town centre, there exist several listed buildings such as the Grade II-listed Lambeth Town Hall and the Grade II-listed Brixton Recreation Centre, amongst others.

1 Fig. 3.5.1.2 Brixton Tate Library

2 Fig. 3.5.1.3 Victorian buildings on Electric Avenue

Fig. 3.5.1.1 Buildings Location Map

3 Fig. 3.5.1.4 Victorian buildings on Atlantic Road

4 Fig. 3.5.1.5 Victorian facade of the Trinity Homes

3.5.2 INDUSTRIAL ORIGINS

The history of Brixton, and particularly Pope's Road, was shaped by the advent of the nearby railway. The significance of the railway is still very much visible in the town centre, particularly in the various viaducts and their defining arched structures.

Within the rich architectural heritage of Brixton, classical examples of Victorian and Georgian buildings integrate with modern and industrial buildings.

The Brixton Recreation Centre, a Grade II-listed building, and the International House, represent the most striking examples. The market stalls are also housed in more modernist structures, offering significant stylistic and visual variety throughout the borough's urban core.

1 Fig. 3.5.2.2 Railway viaduct on Atlantic Road

2 Fig. 3.5.2.3 Brixton Recreation Centre

Fig. 3.5.2.1 Buildings Location Map

3 Fig. 3.5.2.4 Market Row entrance on Electric Lane

4 Fig. 3.5.2.5 Prince of Wales building, facade detail

3.5 ARCHITECTURAL LANGUAGE - FAÇADE ARTICULATION

3.5.3 TRIPARTITE STRUCTURE

One of the most common architectural feature within the surrounding built context is the organisation of facades in tripartite format.

This often includes a very active and porous ground floor, usually occupied by retail; a very repetitive, structured and modular central space, usually emphasised by articulated windows openings; and an upper horizontal spandrel that caps middle volumes within the background various forms via chimneys, skylights and roofs.

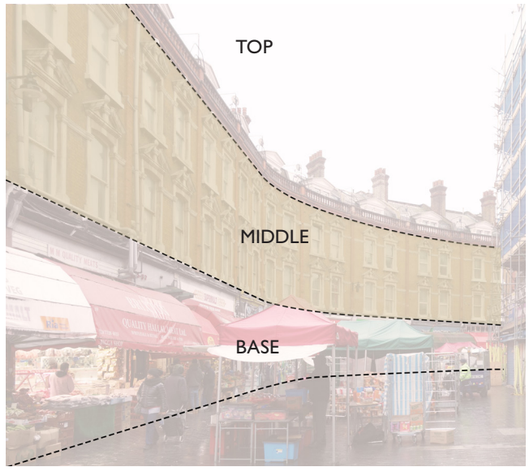
The materiality of the three distinct parts is usually distinct, with a more porous lower zone, a solid central, and varied upper zones.

1 Fig. 3.5.3.2 Buildings on Electric Avenue

2 Fig. 3.5.3.3 Building on Atlantic Road

3 Fig. 3.5.3.4 Historic picture of Bon Marche building on Brixton Road

Fig. 3.5.3.1 Buildings Location Map



3.5.4 TOWERS & FEATURE CORNERS

Towers and feature corners are prevalent in many buildings in the surrounding built context of our proposal's site. As outlined in the Tall Building Study published by Lambeth Council, these should serve as reference for any tall building proposed in Brixton.

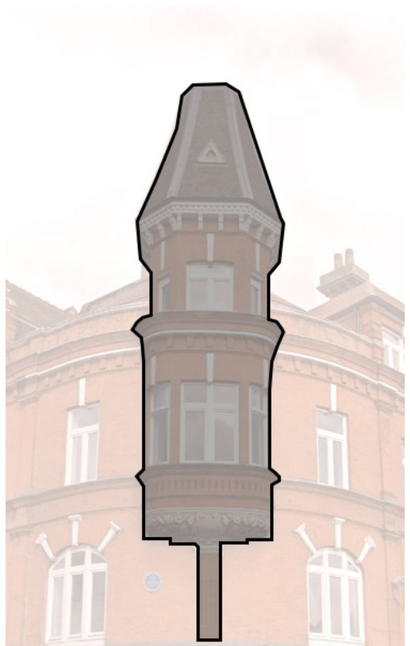
These elements typically demonstrate a variation in width that develops vertically, creating elegant and slender silhouette. They constitute articulated profiles that stand out against the skyline, lending a distinctive architectural landscape to the town centre of Brixton.

1 Fig. 3.5.4.2 Building on Brixton Station Road

2 Fig. 3.5.4.3 Lambeth Town Hall on Brixton Hill

3 Fig. 3.5.4.4 Budd family mausuleum on Brixton Oval

Fig. 3.5.4.1 Buildings Location Map



3.5 ARCHITECTURAL LANGUAGE - FAÇADE ARTICULATION

3.5.5 PEDIMENTS

Many of Brixton's historical buildings include pediments as a key feature of their facades – most notably St Matthew's Church and the Trinity Homes. These pediments are often triangular in form and can also be seen in smaller scale in a number of window heads present in many facades of the surrounding buildings.

1 Fig. 3.5.5.2 Historic picture of Brixton Synagogue on Effra Road

2 Fig. 3.5.5.3 St Matthew's Church on Brixton Hill

3 Fig. 3.5.5.4 Trinity Homes building on Acre Lane

Fig. 3.5.5.1 Buildings Location Map

3.5.6 ARCHES

Arches are a predominant visual feature throughout Brixton's neighbourhoods, present in many different elements and formats.

Arches are evidently visible in the distinctive masonry structure of the railway viaducts, and as part of different facades elements including window heads and arched openings. Arches also exist in the metal roof structure of the existing markets, where they create a continuous skylight that allows natural lights to infiltrate into the market realm.

Arched openings and facade features are also visible in the railway structure, facade elements, or on the structure of the existing markets' streets.

1 Fig. 3.5.6.2 Building on Atlantic Road

2 Fig. 3.5.6.3 Internal Arcade of the Brixton Village

3 Fig. 3.5.6.4 Railway viaduct arches on Brixton Station Road

Fig. 3.5.6.1 Buildings Location Map

3.5 ARCHITECTURAL LANGUAGE - FAÇADE ARTICULATION

3.5.7 WINDOW HEADS AND LINTELS

Throughout Brixton's built context, window heads play an important role in the character of the local townscape. Often appearing in a variety of forms – especially triangles and arches – window heads bring richness and vitality to the facades, their bold repetition providing structure and rhythm to building facade.

Window heads are usually joined together by continuous horizontal lintels that provide pronounced horizontal articulation to the facades. Both elements usually present in materials that are unique to their facade materials.

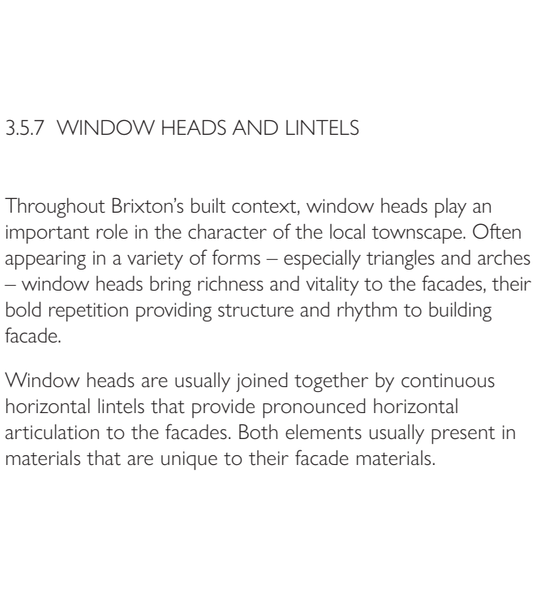


Fig. 3.5.7.1 Buildings Location Map

1 Fig. 3.5.7.2 Buildings facades on Electric Avenue

2 Fig. 3.5.7.3 Building facade on Atlantic Road

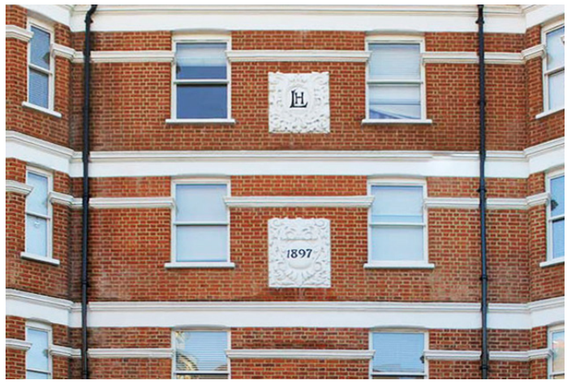
3 Fig. 3.5.7.4 Building facade on Brixton Road

4 Fig. 3.5.7.5 Building facade on Coldharbour Lane

As mentioned previously, the marked horizontality of the lintels often provides moment of variation within their modular organisation. Sometimes these horizontal lintels are interrupted by moments of verticality that introduce variation and activate facades.

Throughout Brixton's town centre, openings, window heads and lintels play a key role in the organisation and appearance of the varying building facades.

Fig. 3.5.7.6 Buildings Location Map

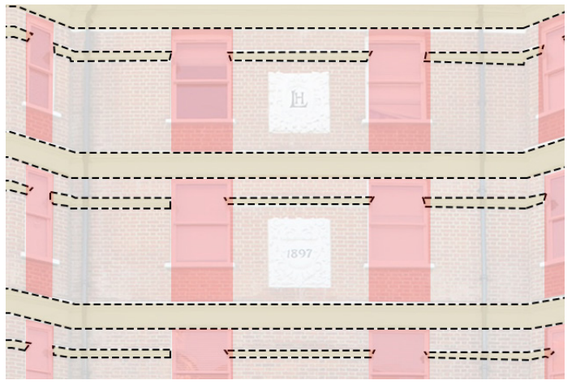


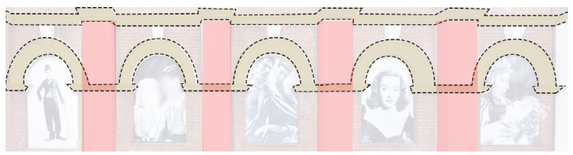
1 Fig. 3.5.7.7 Building facade Lancaster House

2 Fig. 3.5.7.8 Building facade on Electric Avenue

3 Fig. 3.5.7.9 Building facade Ritzy Cinema

4 Fig. 3.5.7.10 Building facade on Rushcroft Road





3.6 MATERIALITY

3.6.1 MATERIALS AS MASS

An in-depth materiality study was conducted by our team to gauge and record the most prevalent materials present in Brixton's town centre.

Our research indicated the predominance of solidity in the general appearance of Brixton's architecture, with buildings appearing to be built mostly in concrete or masonry, or combination of the two.

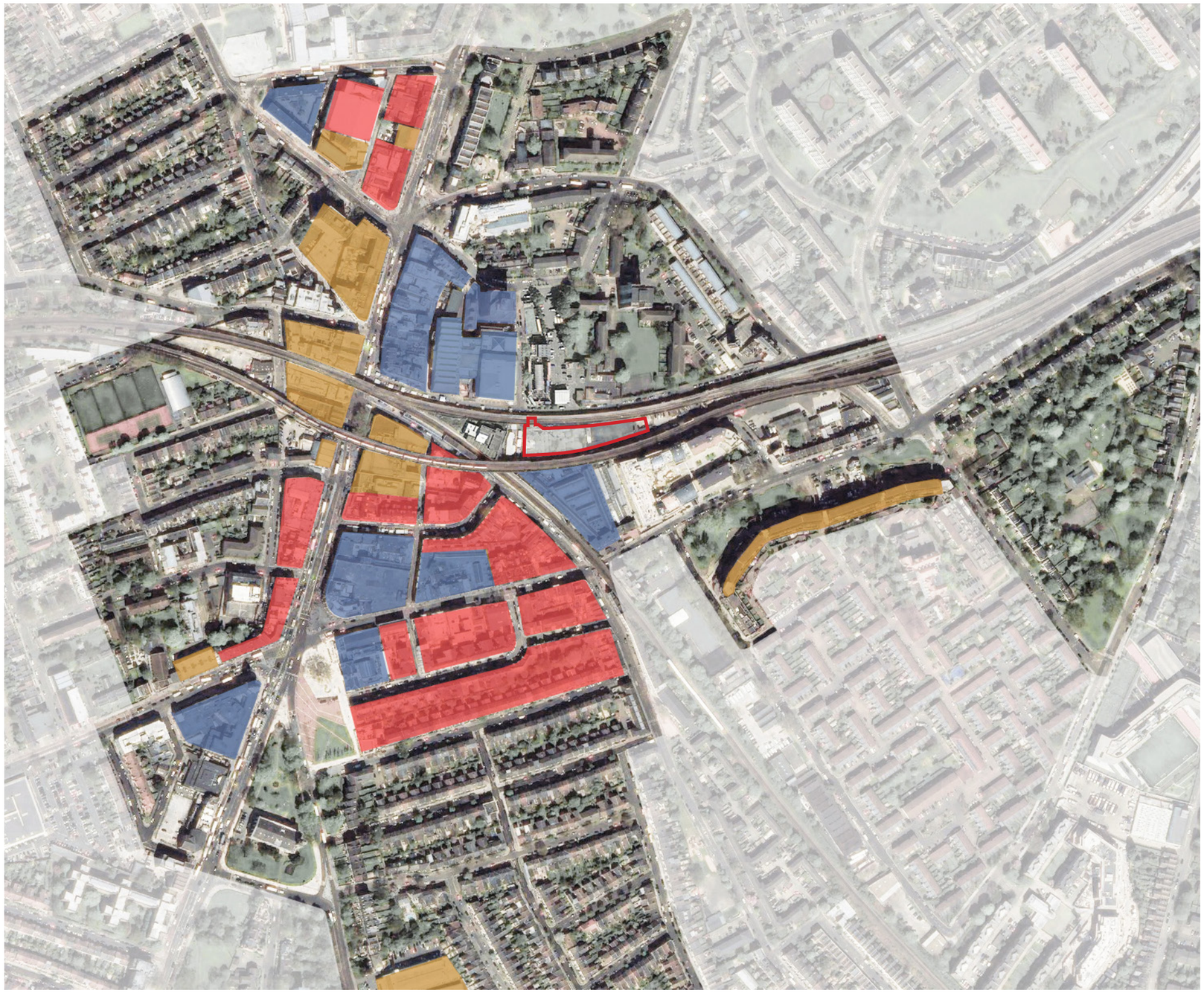


Fig. 3.6.1 Buildings Materiality Map

3.6.2 CONCRETE

Concrete can be found in some of the biggest structures present in Brixton's built environment, for example, in buildings including such as the Recreation Centre, Southwyck House and Market Row entrance.

It is used in a number of buildings throughout the area in combination with other materials, such as stone or brick. It presents in a variety of textures and colours, ranging from rough to smooth, and from gray to beige across the neighbourhood's different areas.

Fig. 3.6.2.1 Brixton Recreation Centre

Fig. 3.6.2.2 Market Row entrance

Fig. 3.6.2.3 Southwyck House

3.6 MATERIALITY

3.6.3 BRICK

Brick is the most dominant material existing in the town centre of Brixton. It presents in a variety of colors, textures and tonalities, with the three most prevalent tonalities being gray, red and yellow. Brick is used in both Victorian and Georgian buildings, as well as in more recently constructed development appearing throughout Lambeth.

Fig. 3.6.3.1 Building facade on Ferndale Road

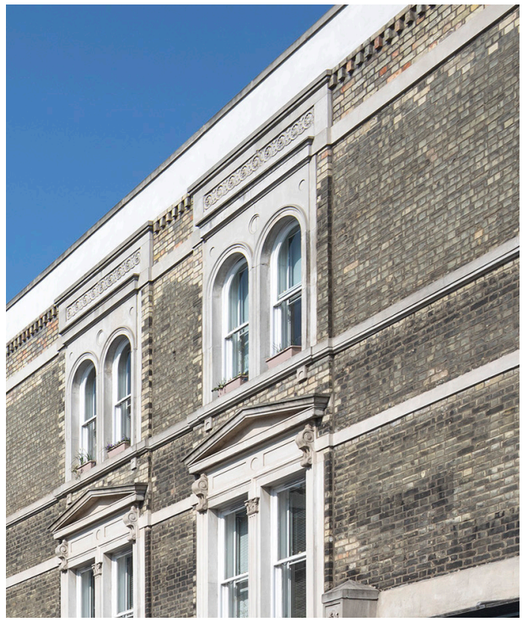


Fig. 3.6.3.2 Building facade on Atlantic Road

Fig. 3.6.3.3 Building facade of Electric Avenue

Fig. 3.6.3.4 Building facade on Electric Avenue

Fig. 3.6.3.5 Building facade on Brixton Station Road

3.6.4 DETAILING

Intricate brickwork paired with different coloured brick is used to enrich facades of buildings that make up Brixton's built context. Interestingly, detailing throughout the built context often incorporates a combination of different materials, as opposed to just one singular material palette.

Fig. 3.6.4.1 Brickwork detailing of railway viaduct

Fig. 3.6.4.2 Brickwork detailing of building facade on Brixton Road

Fig. 3.6.4.3 Brick & Concrete detailing of building facade on Coldharbour Lane

Fig. 3.6.4.4 Brick & Concrete detailing of building facade on Coldharbour Lane

Fig. 3.6.4.5 Brick & Concrete detailing of building facade on Coldharbour Lane

3.6 MATERIALITY

3.6.5 STREETSCAPES MATERIALITY

Fig. 3.6.5.1 Building facade on Electric Avenue north east

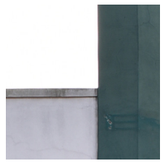
Metal signage

Brickwork detail

Brick tonality #1

Brick & concrete detail

Glazing detail #1



Exposed concrete

Glazing detail #2

Fig. 3.6.5.2 Building facades on Electric Avenue south west

Fig. 3.6.5.3 Building facades on Electric Avenue south east

Brick tonality #2

Brick tonality #4

Brick tonality #5

Brick tonality #6

Brick tonality #7

Glazing detail #3

Glazing detail #4

Fig. 3.6.5.4 Building facades on Electric Avenue north west

3.7 DESIGN DEVELOPMENT

3.7.1 STATUTORY CONSULTATION

The design for the building was informed by an extensive pre-application consultation process which evolved over 12 months.

During this time all aspects of the design were thoroughly discussed with the relevant bodies, stakeholders and the general public. The comments received were given thoughtful consideration and informed the proposal.

Lambeth City Council, the Greater London Authority and Network Rail were consulted during this process in their role as decision makers and statutory consultees.

In addition to this, Hondo promoted an active engagement strategy with stakeholders and immediate neighbours, as well as residents and local businesses.

A separate 'Statement of Community Involvement' is submitted as part of this application and will explain the local and stakeholder consultation, and how the scheme has responded to their feedback.

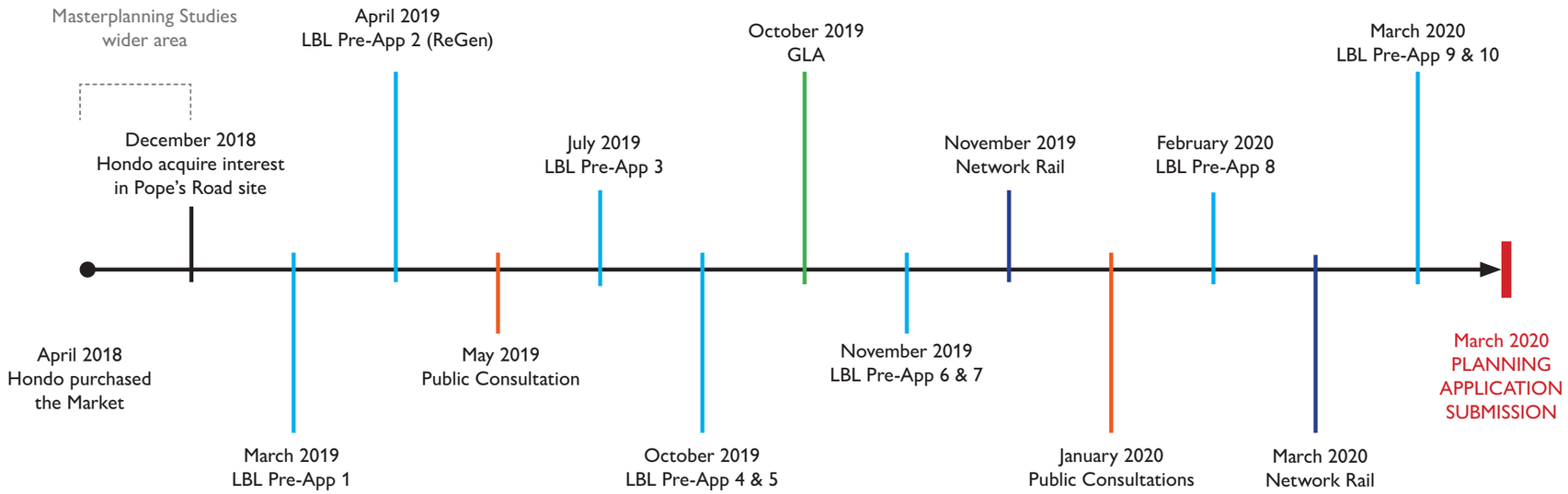


Fig. 3.7.1. Statutory Consultation Timeline diagram

3.7.2 PUBLIC CONSULTATION

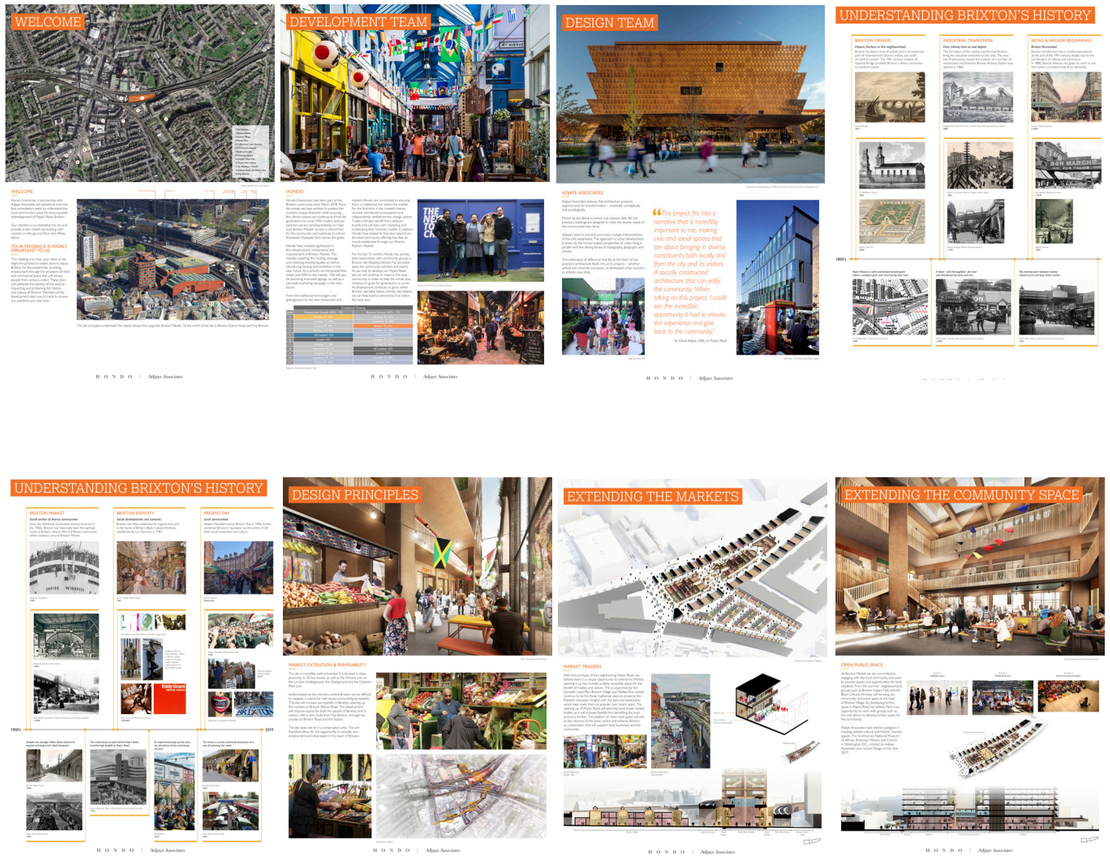


Fig. 3.7.2.1 Consultation boards - May 2019

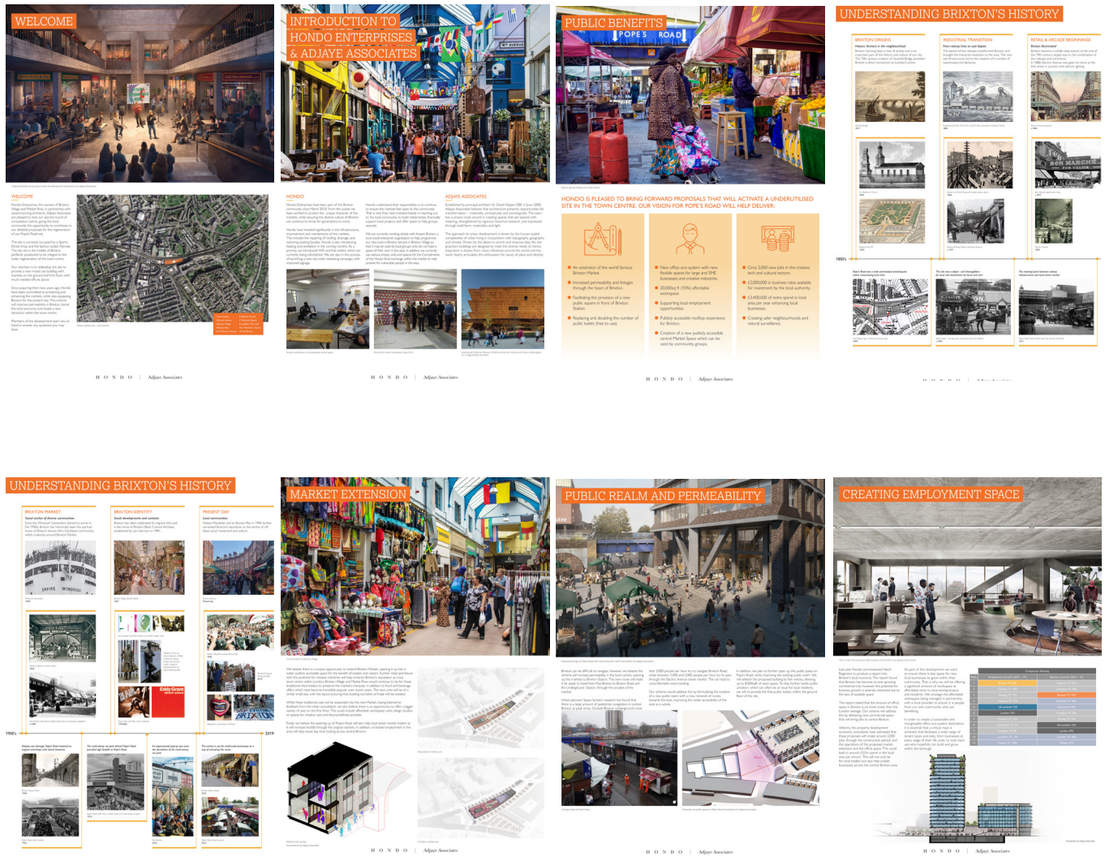


Fig. 3.7.2.2 Consultation boards - January 2020

3.7 DESIGN DEVELOPMENT

3.7.3 MASSING EVOLUTION

Fig. 3.7.3.1 July 2019, Massing axonometric diagram

JULY 2019

- No. of Floors:
  - GF + 21 west block, GF + 8 Floors east block
- Uses:
  - Flexible Retail on Ground and First Floor
  - Office on all the other floors

Fig. 3.7.3.2 October 2019, Massing axonometric diagram

OCTOBER 2019

- No. of Floors:
  - GF + 21 west block, GF + 8 Floors east block
- Uses:
  - Flexible Retail on Ground and First Floor
  - Office on all the other floors of the West Block
  - Hotel on the East Block with restaurant on the 8th floor

Fig. 3.7.3.3 January 2020, Massing axonometric diagram

JANUARY 2020

- No. of Floors:
  - GF + 21 west block, GF + 8 Floors east block
- Uses:
  - Flexible Retail on Ground and First Floor
  - Office on all the other floors of the West and East Block
  - Restaurant on the 8th floor of the East Block

Fig. 3.7.3.4 March 2020, Massing axonometric diagram

MARCH 2020

- No. of Floors:
  - GF + 19 west block, GF + 8 Floors east block with r evised floor to ceiling height
- Uses:
  - Flexible Retail on Ground and First Floor
  - Office on all the other floors of the West and East Block
  - Restaurant on the 8th floor of the East Block

3.7.4 DESIGN EVOLUTION

Fig. 3.7.4.1 July 2019, View from Electric Avenue

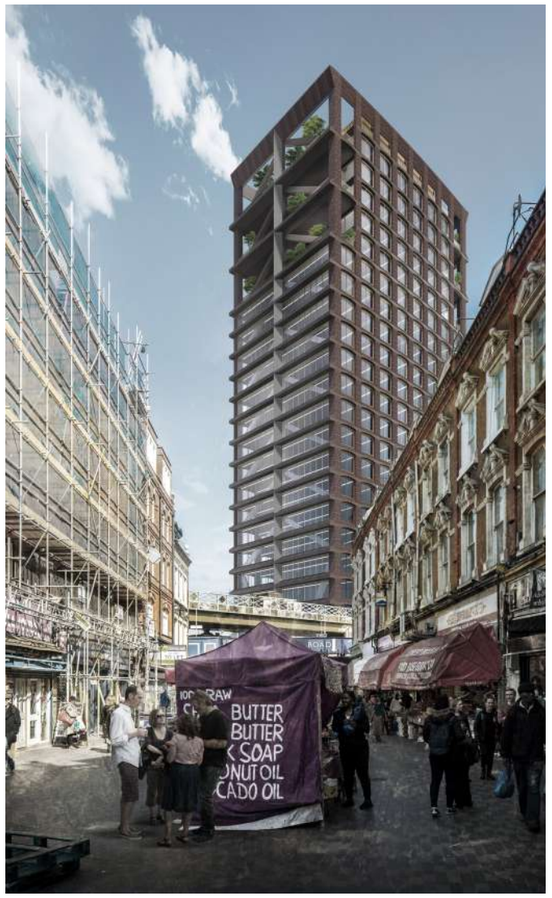


Fig. 3.7.4.2 October 2019, View from Electric Avenue

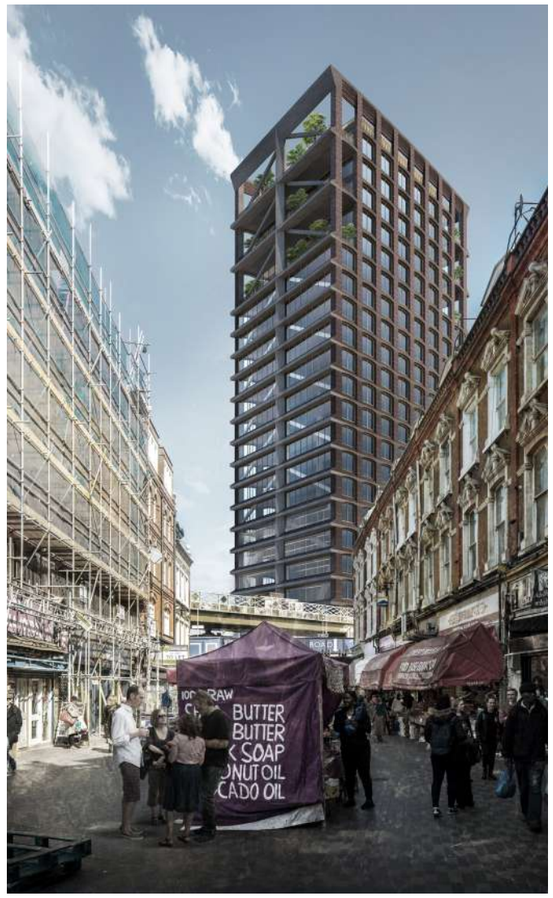


Fig. 3.7.4.3 January 2020, View from Electric Avenue

Fig. 3.7.4.4 March 2020, View from Electric Avenue

JULY 2019

The initial design proposed ground plus 21 storeys on the tallest block with a combination of Retail and Office uses.

The initial proposed materiality was pigmented concrete with timber elements in the form of balustrades and window apron.

The side elevations where incorporating modular elements with arches, whilst the west elevation had a more austere and repetitive appearance with linear elements.

Terraces where proposed in the internal east and west elevations.

OCTOBER 2019

A revised proposal was presented to LBL with a significant change in materiality and in the articulation of the building. The building uses were also revised with the inclusion of an hotel in the East block.

The same number of storeys on both blocks was maintained. The revised proposal articulate the façade in a tripartite structure, proposing brick as the main cladding element with concrete insertion in the form of lintels and structural elements. A thorough environmental impacts analysis influenced the facade design evolution. Terraces where distributed in the top of the building to articulate it and give a sense of lightness to the volume.

JANUARY 2020

A further change in the program was incorporated and presented to LBL.

The east block was reversed back to include offices, as studies were carried out on the importance of creating a critical mass of office spaces to generate a wider ecosystem within Brixton.

Architecture articulation of the top was slightly revised in order to incorporate some of the comments from the authorities.

MARCH 2020

Following comments from LBL a number of changes were incorporated into the scheme.

The revised proposal shows a significant reduction in height on the west block, bringing the number of storeys down to ground plus nineteen. An adjustment to the floor to ceiling height further reduced the overall height.

Design adjustments on the façades were also included and presented following additional environmental imapct analysis.

The tonality of the building was also revised, proposing a much lighter material palette for both concrete and brick.

DESIGN PROPOSAL

4

4.1 BUILDING ORGANISATION

Our proposed building is mixed-use for flexible market and office with all users entering the building through common access points at ground floor level. Small informal lobbies for office workers are integrated within the market circulation. The scheme is split between two blocks, both for office spaces, with a restaurant on top of the lower elements. Affordable workspaces will be integrated into the proposal and their inclusion will be compliant with the emerging policy, however their final location within the development has not yet been decided. Ground and first level are communal to both, each enjoying market space and retail units, whilst second and third floors are linked to provide bigger floor plates.

West Block - Office

- The office space is organised around a central core that is positioned centrally in the floor plate, releasing the perimeter of the building and maximising the window area.

East Block - Office

- The office space has a core located in the centre of the floor plate at the east end of the volume, and two smaller cores located on the façade in the west end of the volume. This layout is the only one that allows the functioning of the market and office in view of the tapering shape of the site.

East Block - Restaurant

- A restaurant is located on the last floor of the East Block and can be reached via a lift directly from the ground and first floor.

Flexible Market Space

- Located on ground and first floor, market units are organised along the façade facing the market streets and accessed directly from them. This flexible market space will evolve over time, but comprise a mix of retail, commercial, creative and leisure / wellbeing uses

Plant

- Plant rooms servicing the building are located in the basements with just the minimum amount of equipment at roof level. On both roofs the equipment is entirely concealed behind metal screens specifically designed..

Key

Market Entrance

Office Entrance

Market Vertical Circulation

Office Vertical Circulation

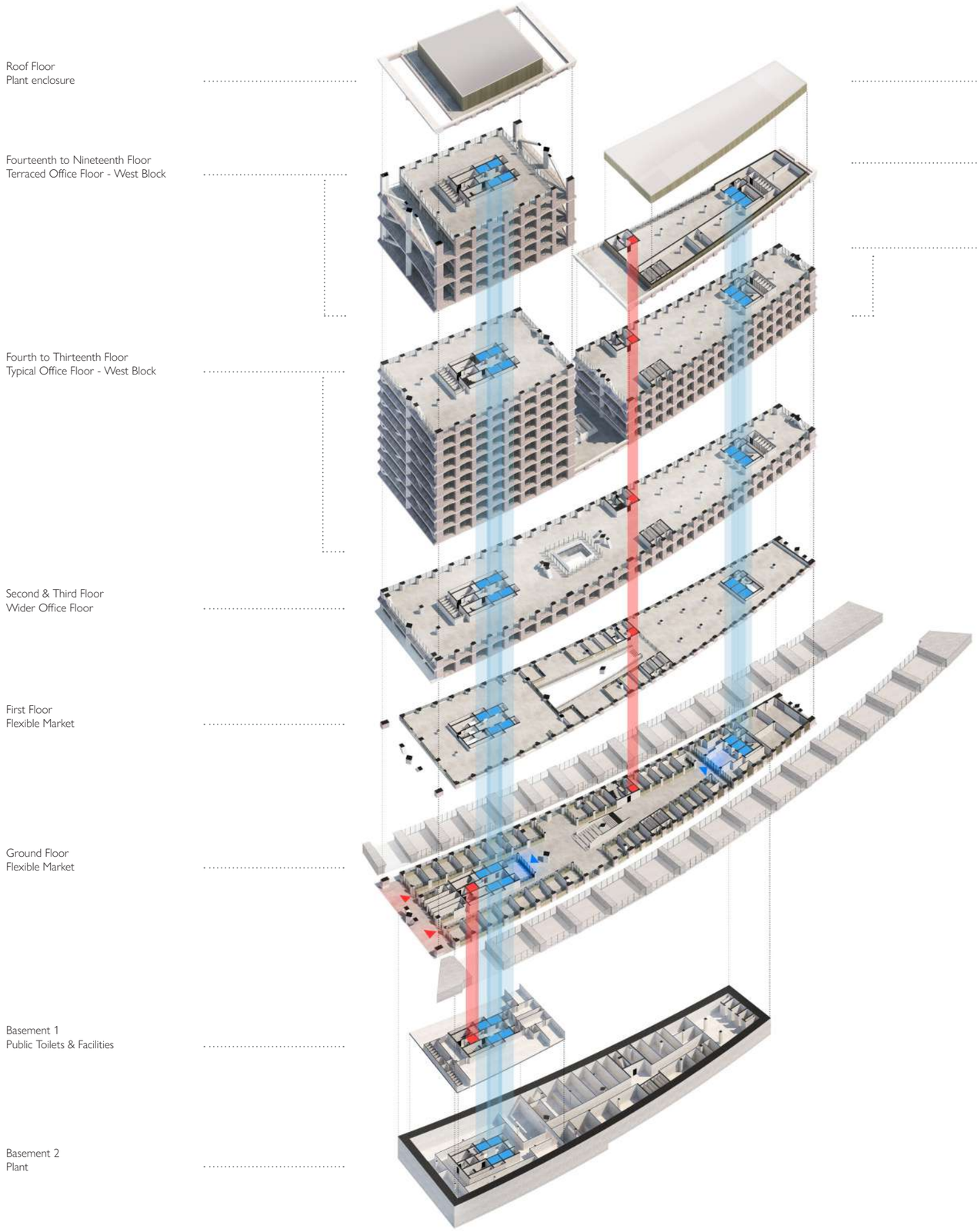


Fig. 4.1 Axonometric Exploded Diagram of the Building

4.2 BUILDING USES

4.2.1 BUILDING USES OVERVIEW

Fig. 4.2.1 Perspective Section Diagram of Building Uses

4.2 BUILDING USES

4.2.2 MARKET FLEXIBLE USES

GROUND FLOOR

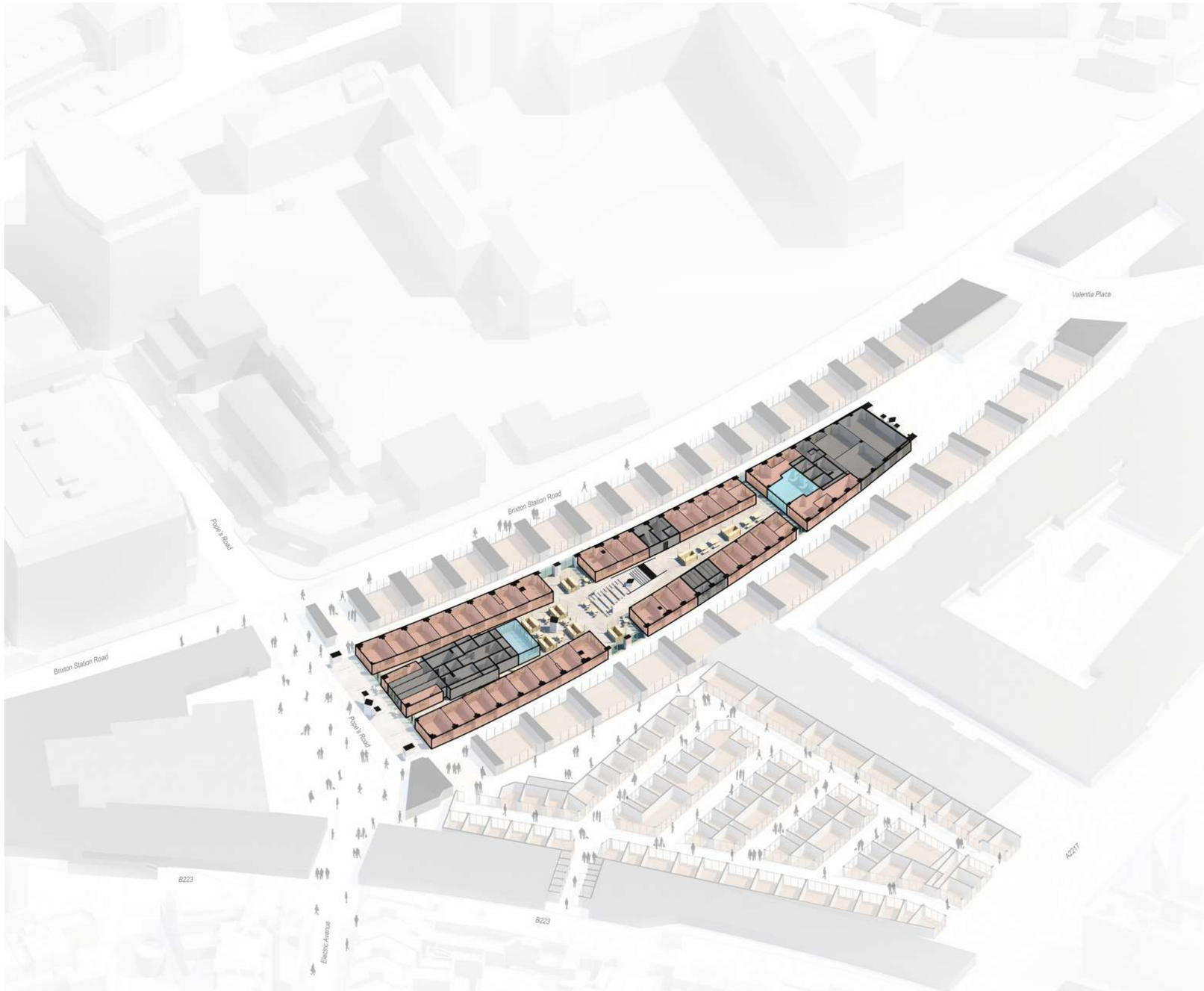
The proposal's ground floor aims to be an extension to the Brixton Village, both in terms of visual form and scale, with retail units distributed along the main circulation path.

The proposal's overall goal is to maintain a high level of flexibility of uses within the market floors – thus, multiple units can be combined to form larger units if required.

The ground floor is likely to be mostly made up of A1 use for shops and retailers; A3 use for restaurants and cafes and the sale of food and drink for consumption on the premises including restaurants and B1us.

The intended inclusion of a publicly accessible open space in the centre of the market will enable the utilisation of the area as a communal space, with optional scenario sets ups such as a food court, event and community spaces or exhibition space.

This space has been designed to also facilitate potential future pedestrian routes South-North through the site linking the Brixton Village with the Pop Brixton site.



Key

- A1-A3
- Office lobby
- Core/Servicing/Plant

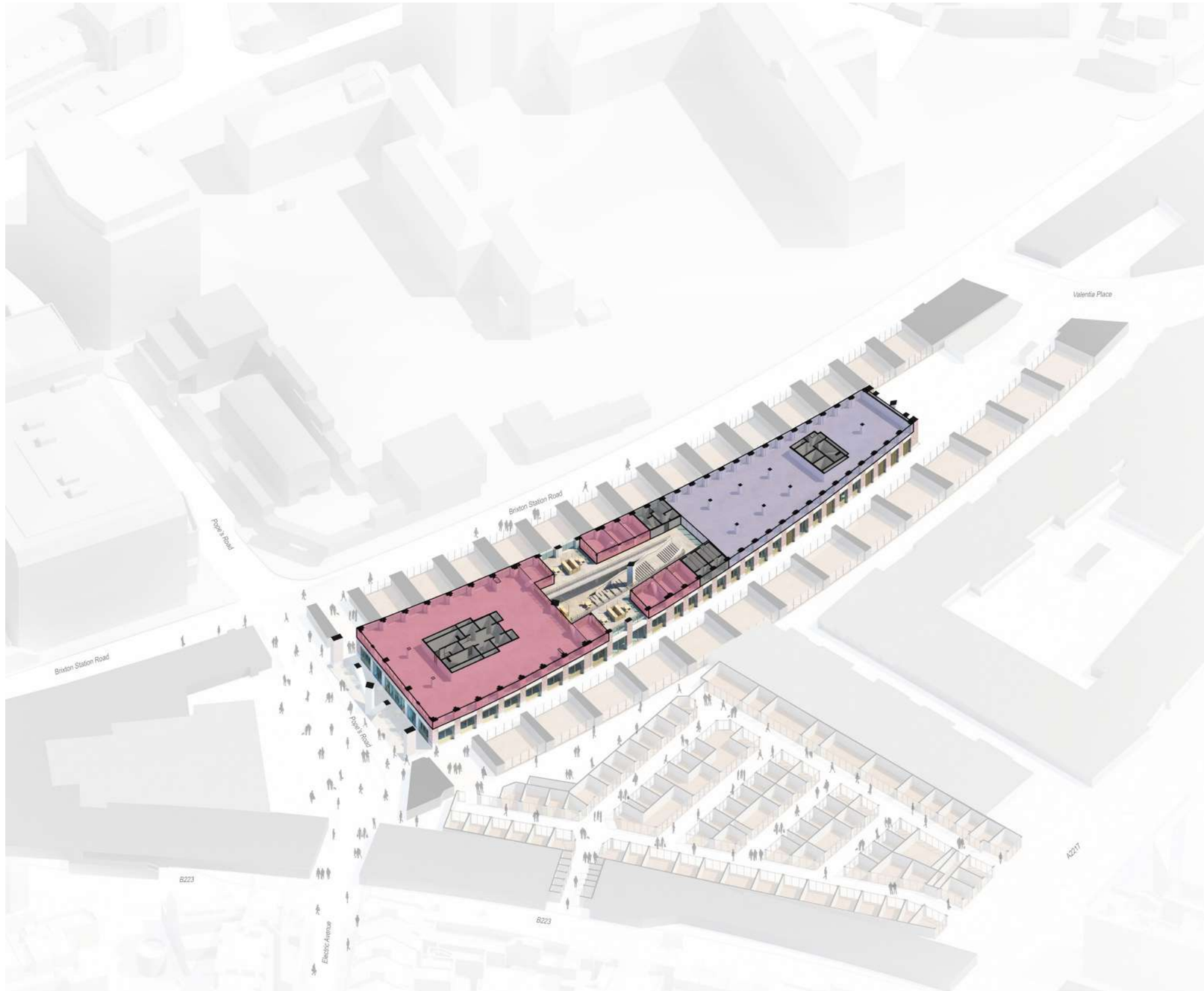
Please note that the uses indicated in the diagram are just illustrative of a potential day-one scenario.

Fig. 4.2.2.1 Ground floor uses axonometric diagram

FIRST FLOOR

Similarly to the ground floor, the market's first floor aims to achieve a high level of flexibility in terms of functionality and uses. It contains smaller units located around the void, with the main stair and two larger elements located on the east and west end.

Within this floor, a combination of A1, A3, B1, B3 and flexible D uses are envisioned.



Key

B1-B1C

D2

Core/Servicing/Plant

Please note that the uses indicated in the diagram are just illustrative of a potential day-one scenario.

Fig. 4.2.2.2 First floor uses axonometric diagram

4.2 BUILDING USES

4.2.3 WEST BLOCK USES

Second to Third Floors

The second and third floors provide a link between the two main building blocks, allowing for wider floor plates and greater office space provision. Sitting atop the fourth floor will be a garden, accessible only for maintenance

Fourth to Thirteenth Floors

The fourth to thirteenth floors represent the scheme's typical office floors, each allowing for single or double tenancy.

Fourteenth to Nineteenth Floors

The fourteenth to the nineteenth floors contain smaller office floor plates, with double height terraces that can be accessed from the lowest two floors by tenants only.

Roof

The roof contains enclosed plant equipment as well as subsequent maintenance elements.

Fig. 4.2.3 West Block uses axonometric diagram

4.2.4 EAST BLOCK USES

Second to Third Floors

The second and third floors provide a link between the two main building blocks, allowing for wider floor plates and greater office space provision. Sitting atop the fourth floor will be a garden, accessible only for maintenance.

Fourth to Seventh Floors

The fourth to seventh floors represent typical office floors that allow for single tenancy.

Eighth Floor

The eighth floor contains a restaurant, an A3 use, with a terrace space. This floor is also allocated with a degree of plant space.

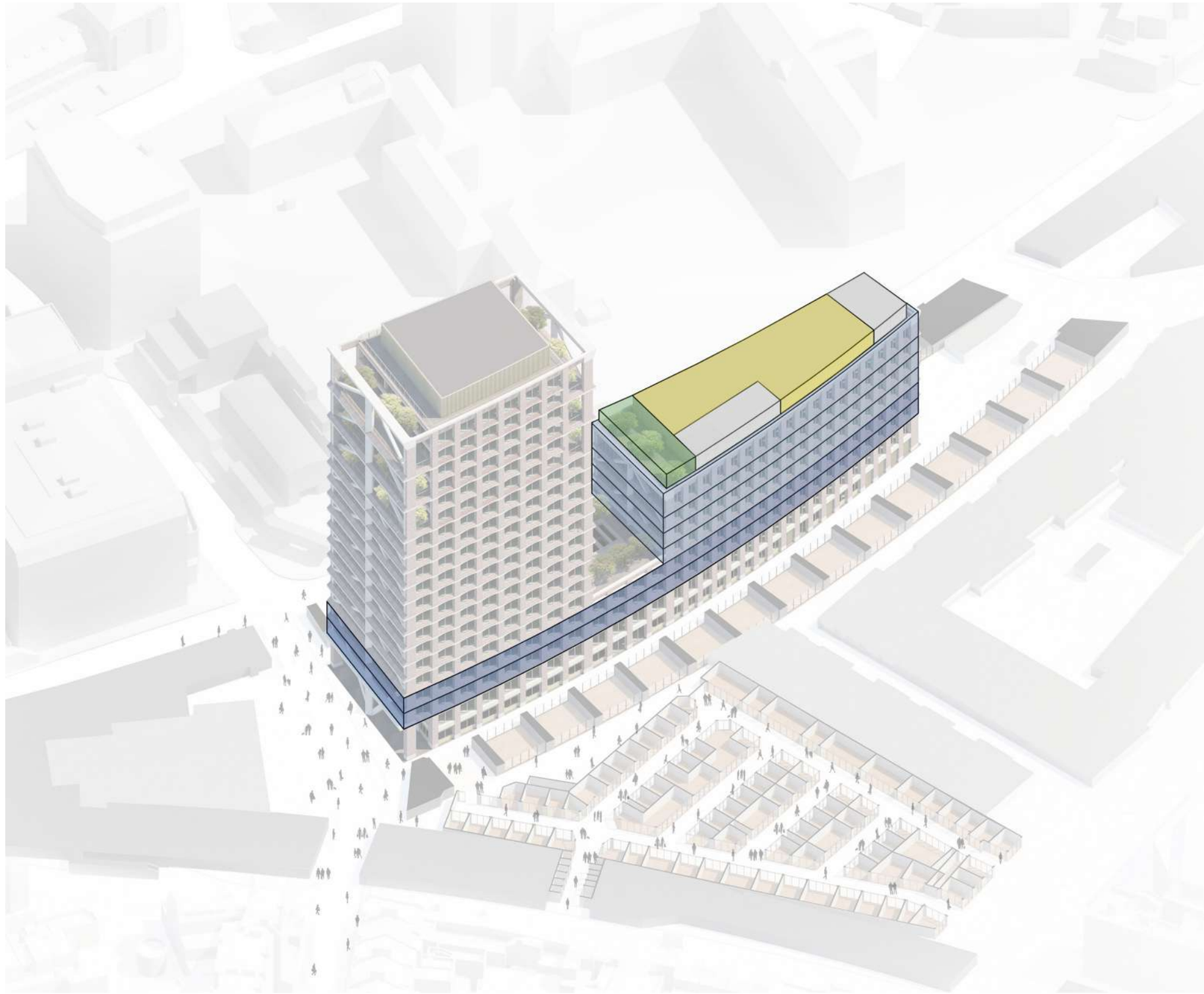


Fig. 4.2.4 East Block uses axonometric diagram

4.3 FLOOR PLANS

GROUND FLOOR

The main entrances of the market are located at ground floor level facing Pope's Road, with one more additional entrance on each east/west side street for servicing.

Office users will also be able to access the scheme through market entrances to enter the two office lobbies located within the market respectively on the west and east sides.

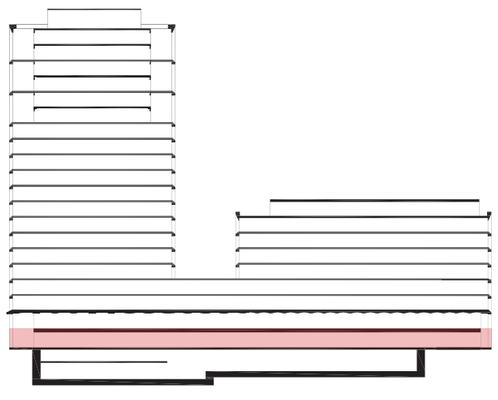
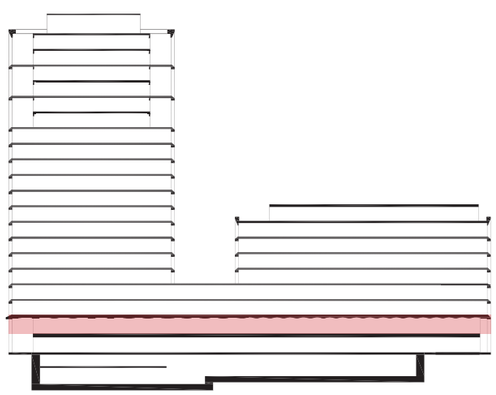


Fig. 4.3.1 Ground floor plan

FIRST FLOOR

The first floor is occupied by flexible market units. This floor can be reached by users via the main stair located in the centre of the market, or via lifts.



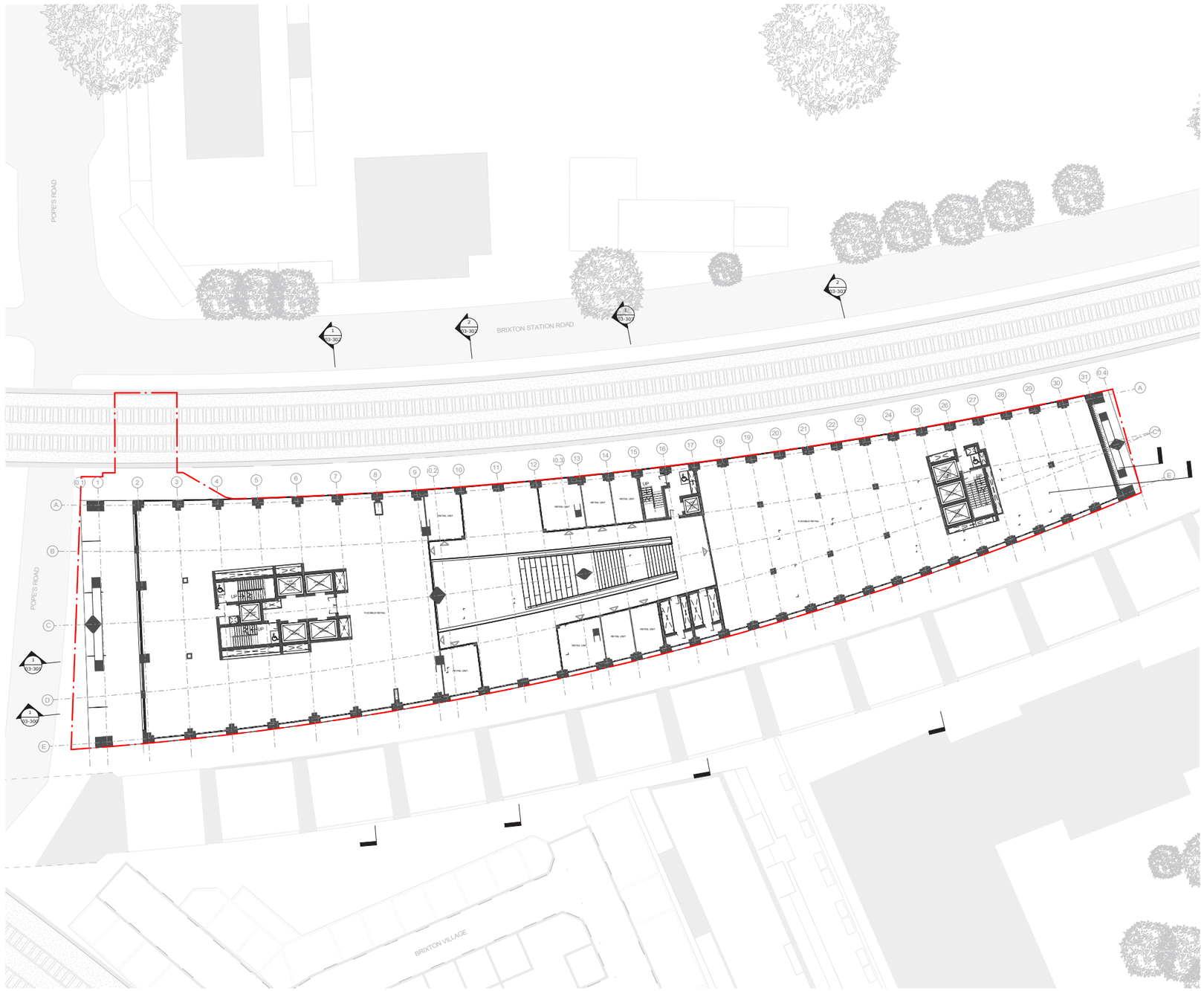


Fig. 4.3.2 First floor plan

4.3 FLOOR PLANS

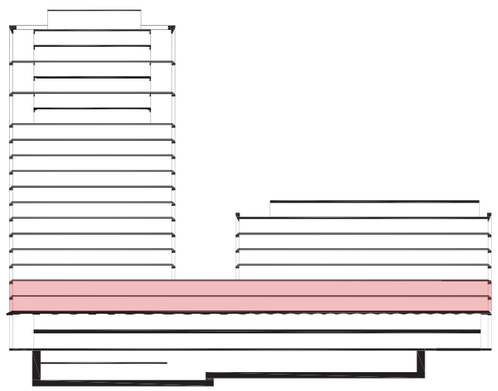
SECOND & THIRD FLOOR

The second and third floors offer a much wider floor plate compared with subsequent floors. Their increased floor plate is organised around a central core located in the west, a void in the centre, and in between the cores on the east.

The void in the centre will overlook the flexible market space, and will be enclosed by a breakout corridor that protects the office spaces from ambient noise from below. It will simultaneously allow for visual permeability and interaction with the market beneath.

The core in the west will house the main vertical circulation elements for offices, as well as fire and escape circulation elements. In the east block, the eastern core will house the main vertical circulation for offices, as well as fire and escape circulation elements.

The two smaller cores present on these floors will house risers and fire escape elements.



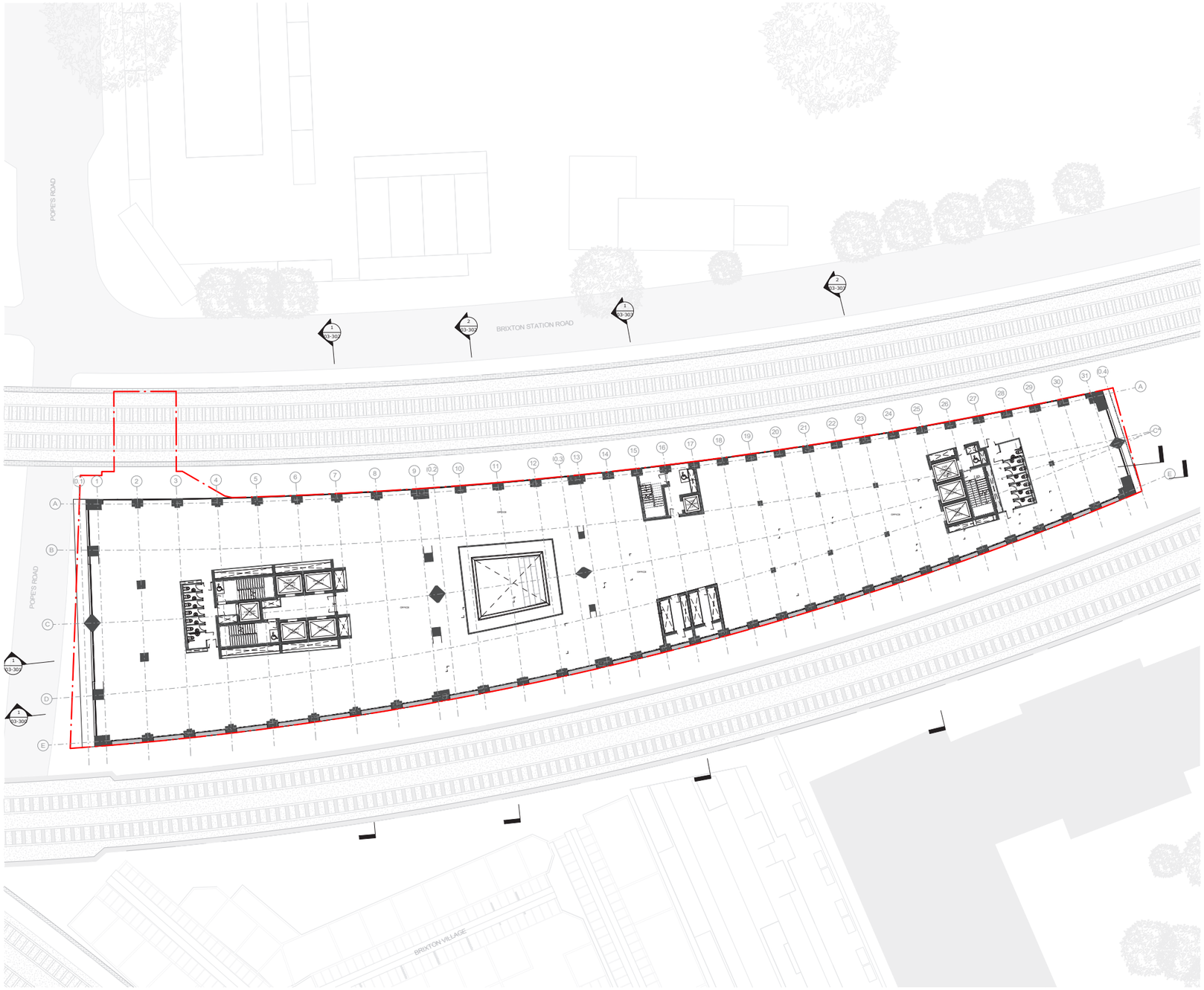
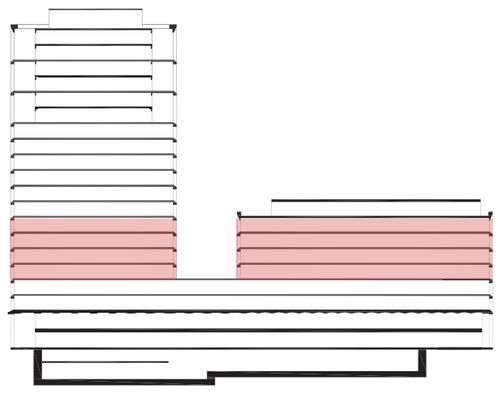


Fig. 4.3.3 Second and third floor plan

FOURTH TO SEVENTH FLOOR

The fourth to seventh floors represent typical office floors for both blocks, with each overlooking the garden located in between blocks on the fourth floor.

The layout of the two blocks showcases the way in which in the west block is organised around a central core, whereas in the east block, organisation is built in between the three cores.



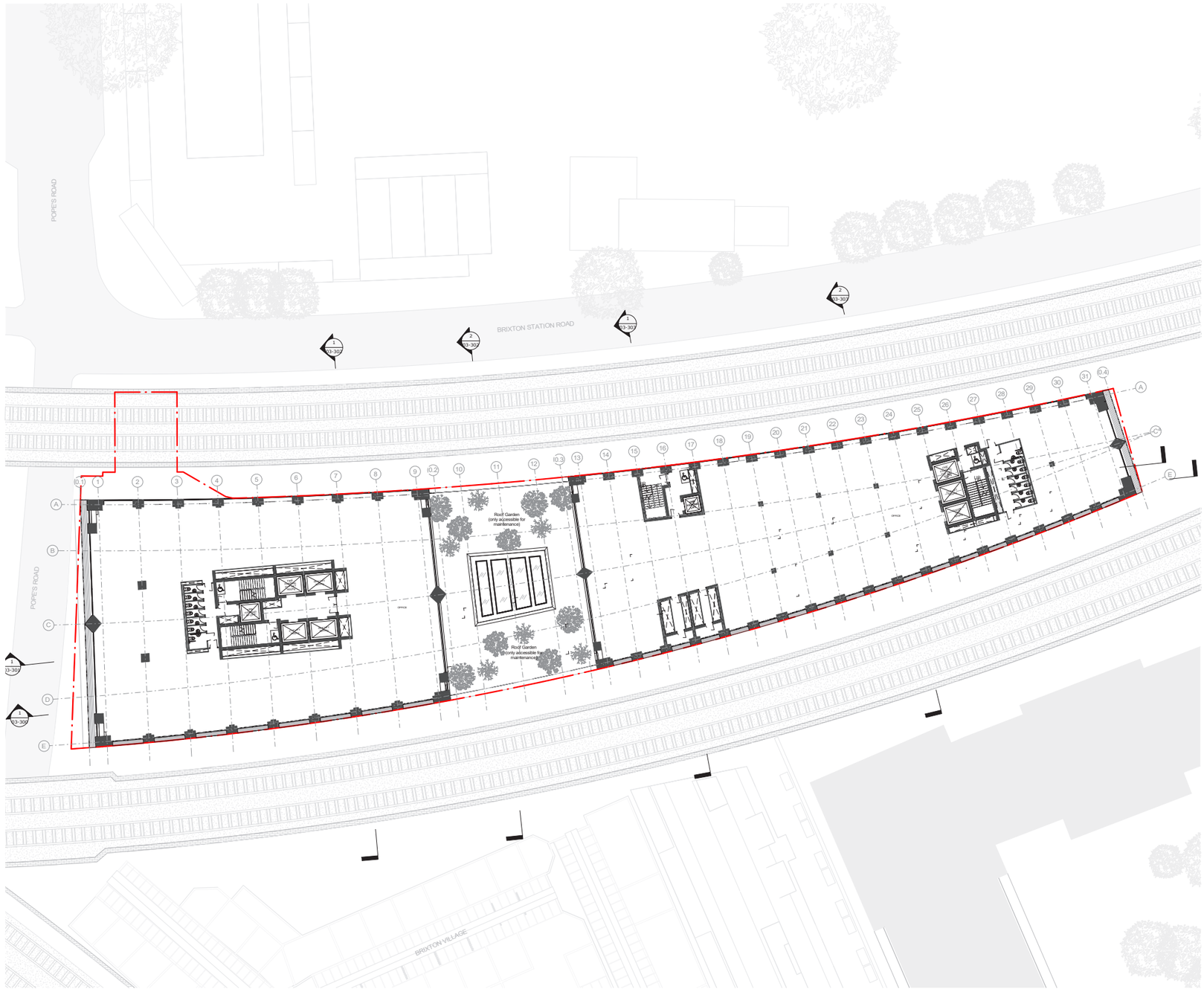


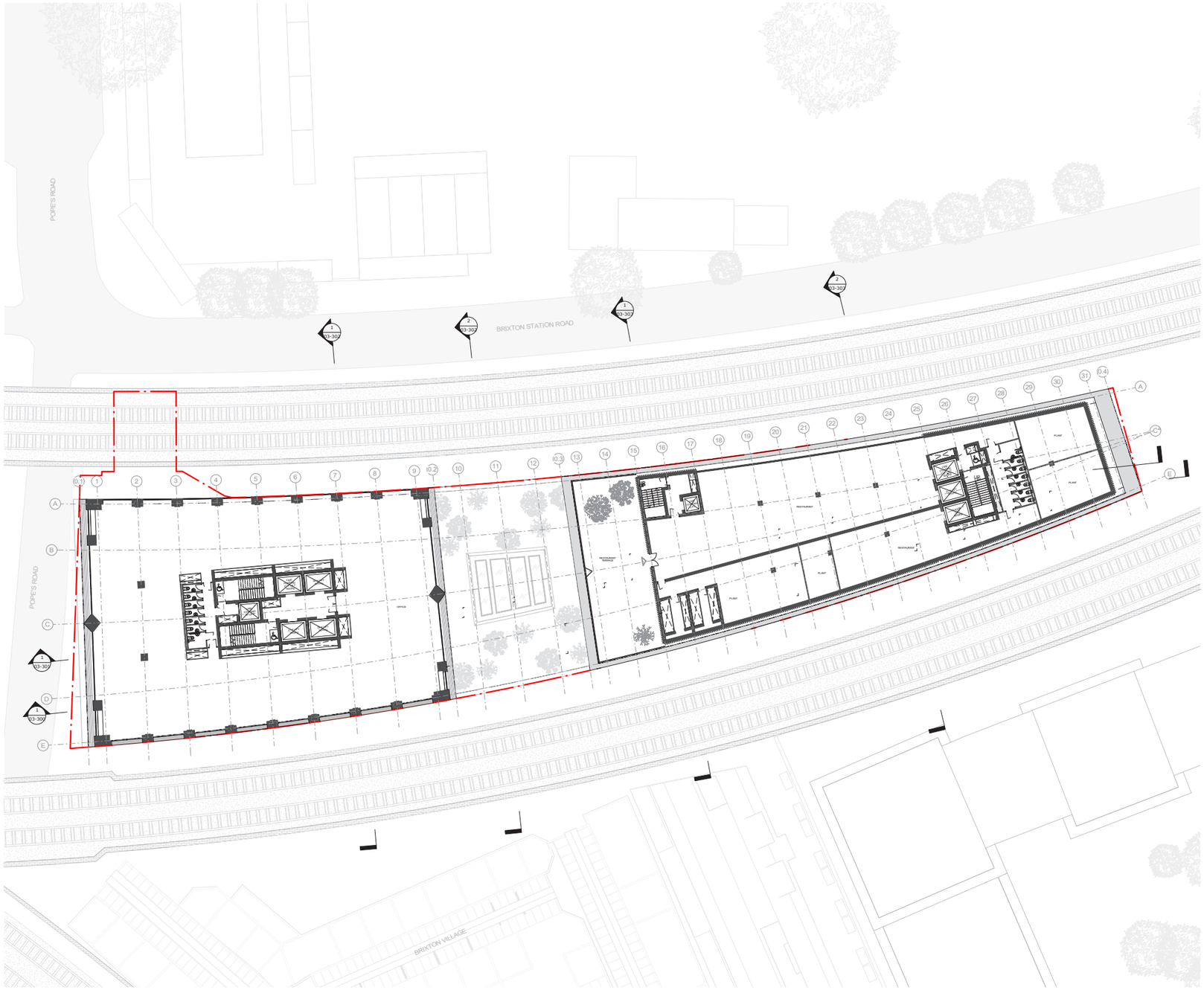
Fig. 4.3.4 Fourth to seventh floor plan

4.3 FLOOR PLANS

EIGHTH FLOOR

The eighth floor represents a typical office floor in the west block, whilst a restaurant is located in the east block. The latter can be accessed by the public via a lift directly from ground floor.

The restaurant will offer views towards the city of London, as well as access to a terrace. Plant spaces are also allocated in the east block, each contained in plant enclosures.



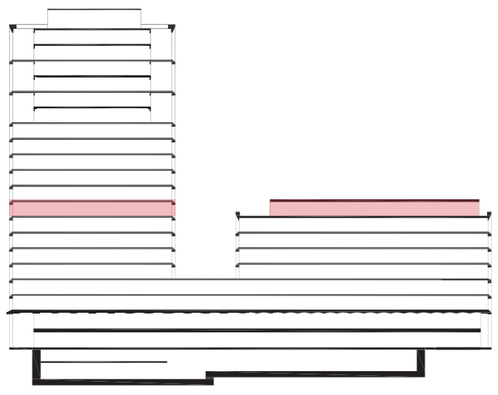
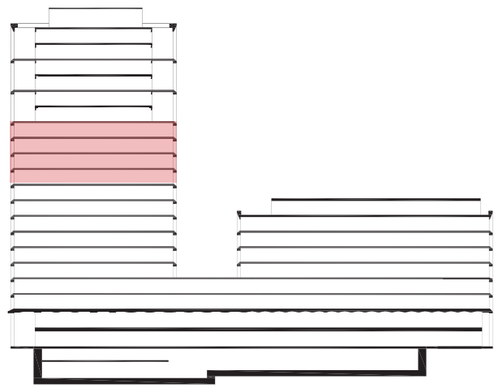


Fig. 4.3.5 Eighth floor plan

NINTH TO THIRTEENTH FLOOR

The ninth to thirteenth floors represent typical office floors for the west block, with the omission of façades facing the lower block to the east. Views over the building's surrounding have now been fully opened on all the sides of the west block.



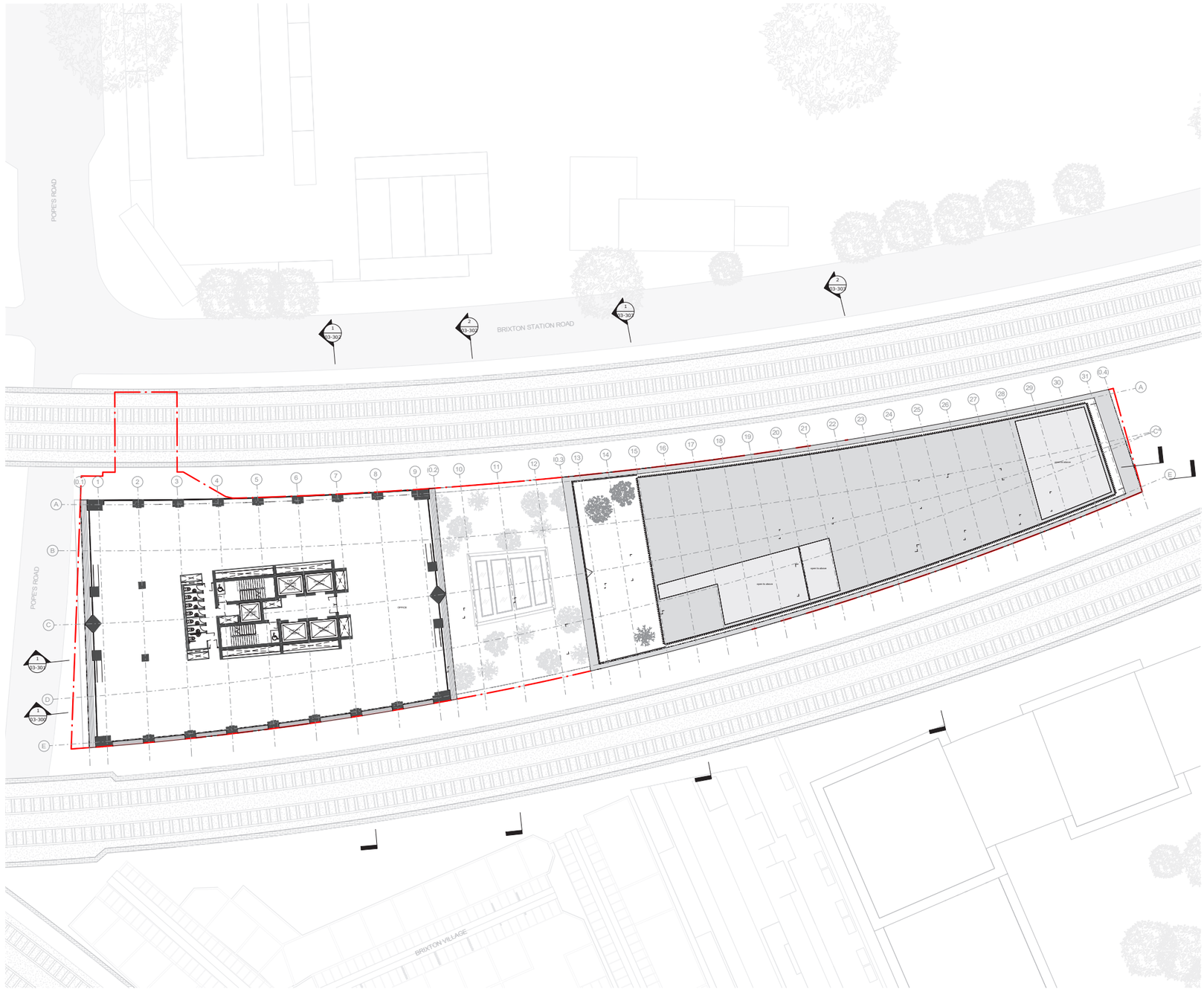


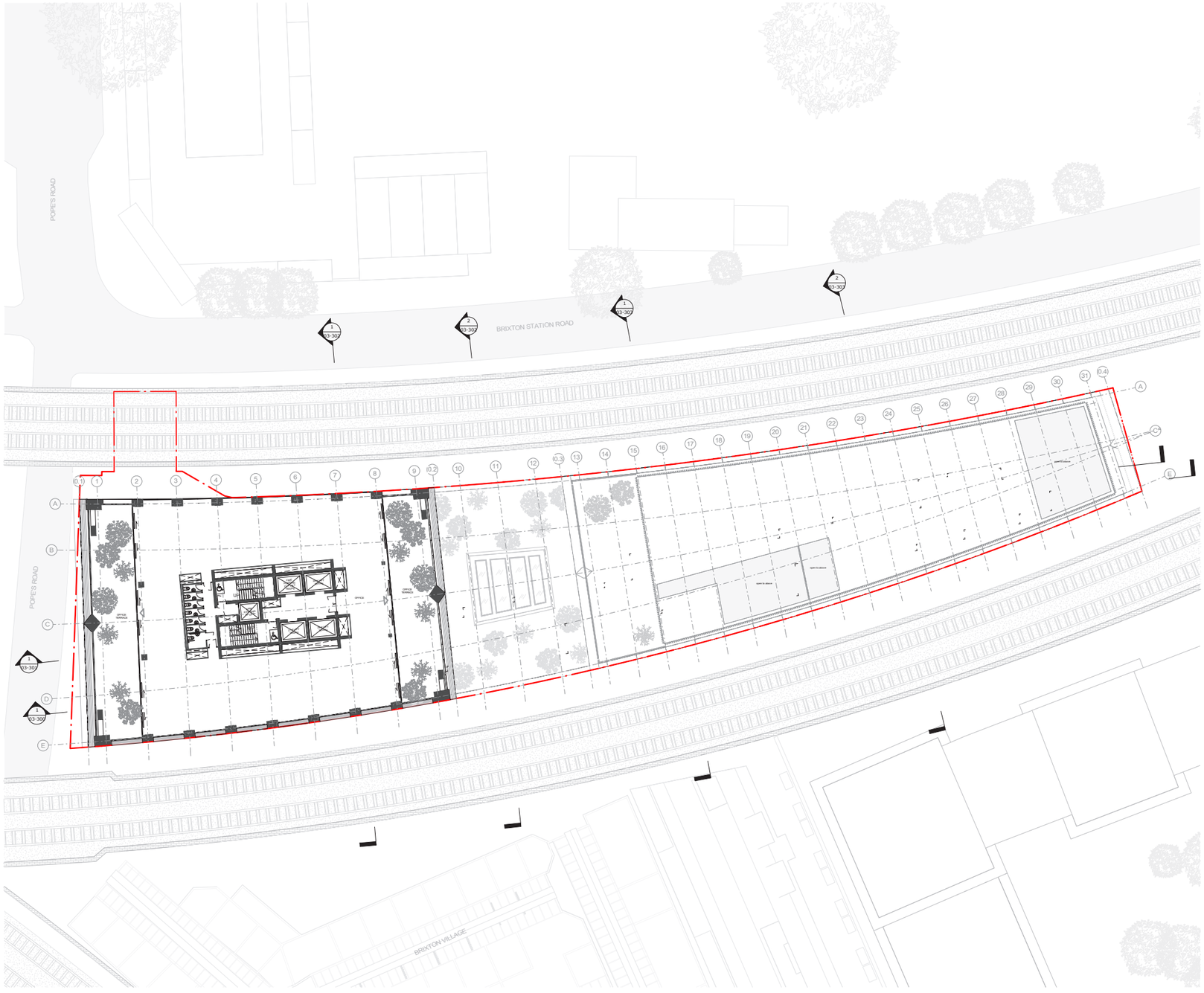
Fig. 4.3.6 Ninth to thirteenth floor plan

4.3 FLOOR PLANS

FOURTEENTH, SIXTEENTH AND EIGHTEENTH FLOOR

The fourteenth, sixteenth and eighteenth floors represent the typical of office spaces with access to the terraces on the west and east side of the west block.

The terraces are double height and accessible only from the lower floor by tenants only. The floor plates on these levels remain similar to the floors below, with the exception of the east and west side, which are one bay smaller.



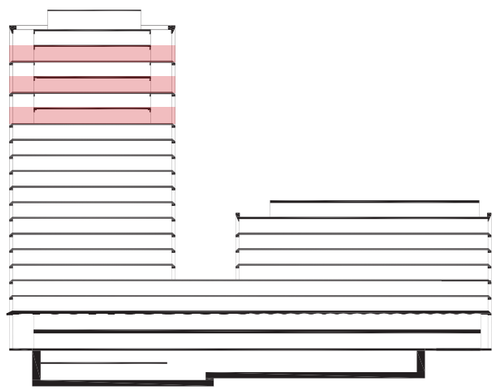
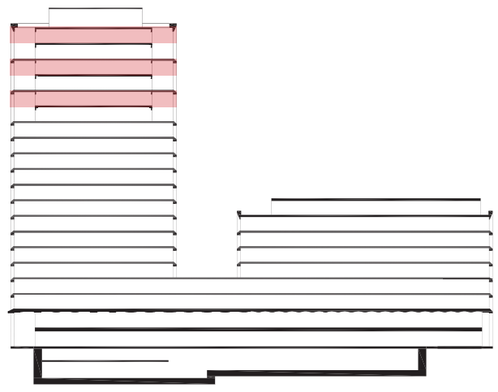


Fig. 4.3.7 Fourteenth, sixteenth and eighteenth floor plan

FIFTEENTH, SEVENTEENTH AND NINETEENTH FLOOR

The fifteenth, seventeenth and nineteenth floors represent typical office spaces with no access to the terraces on the west and east side of the west block.

Users inhabiting these floors will benefit from terrace views but will not have direct access to the terrace itself. The floor plates on these levels remain similar to the floors below, with the exception of the east and west side, which are one bay smaller.



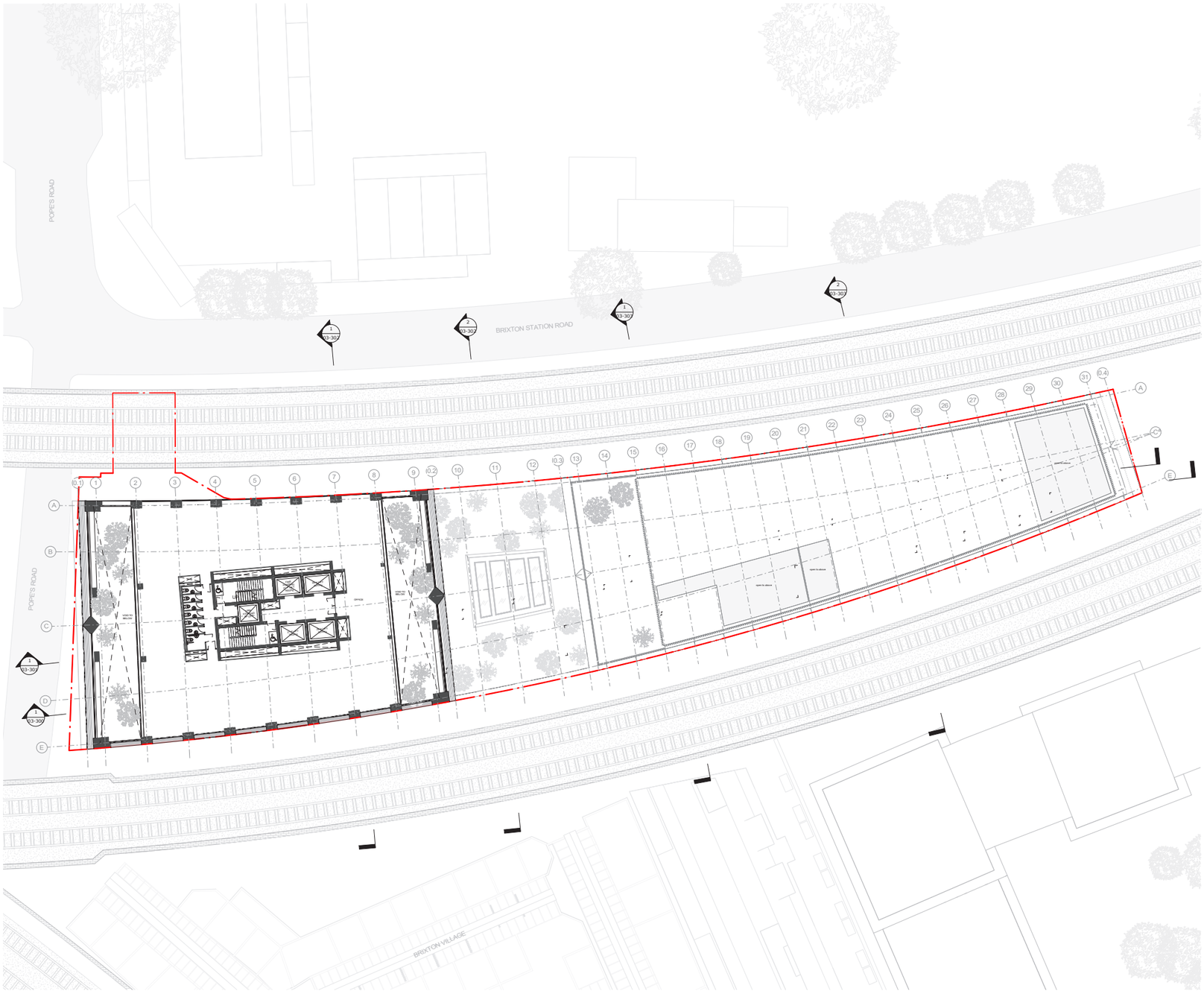
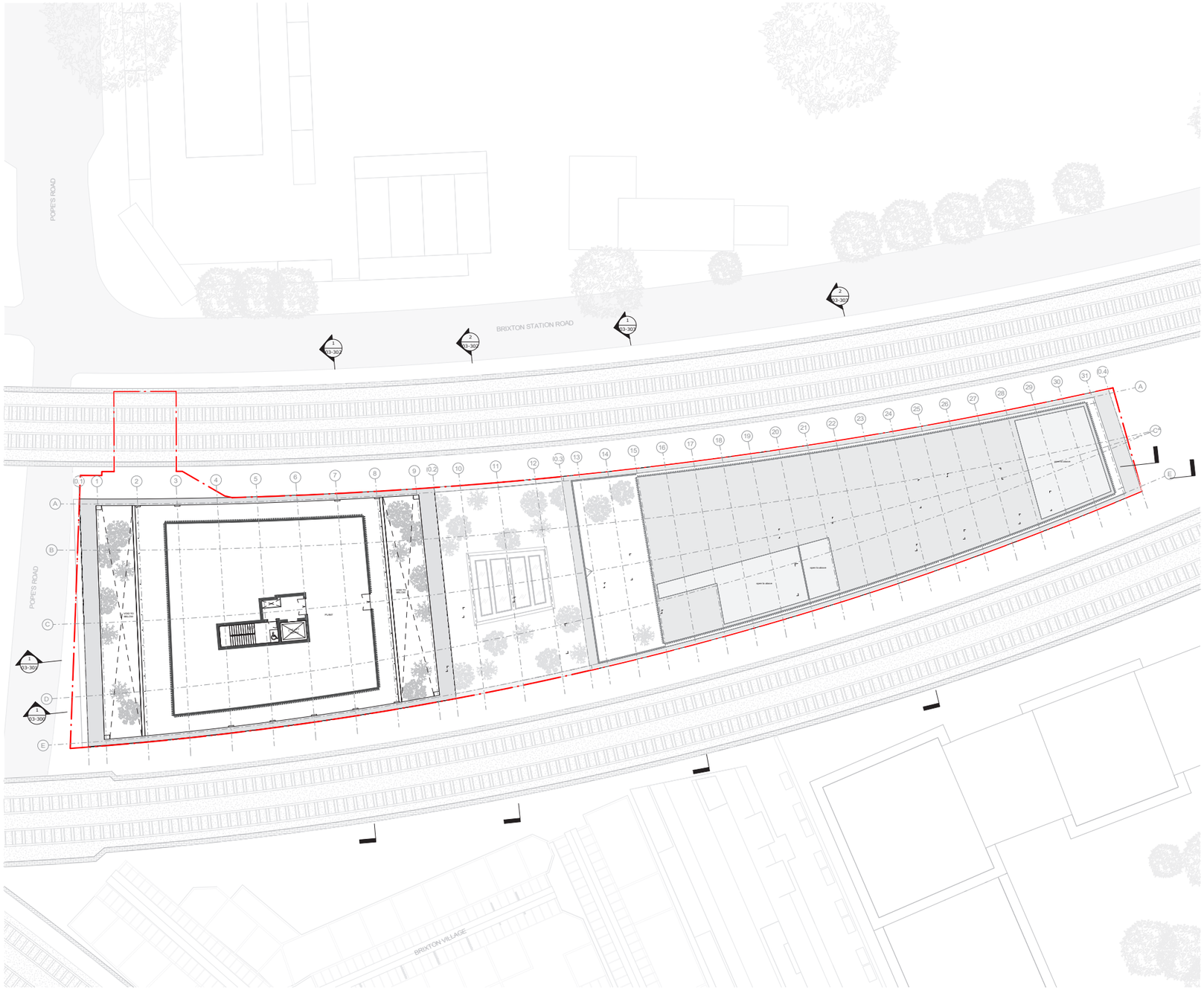


Fig. 4.3.8 Fifteenth, seventeenth and nineteenth floor plan

4.3 FLOOR PLANS

ROOF LEVEL

The roof contains equipment that serves both blocks. Plant is contained in an enclosure that is offset from the perimeter of the block, allowing for circulation that can accommodate maintenance activities. This floor can only be accessed by authorised personnel.



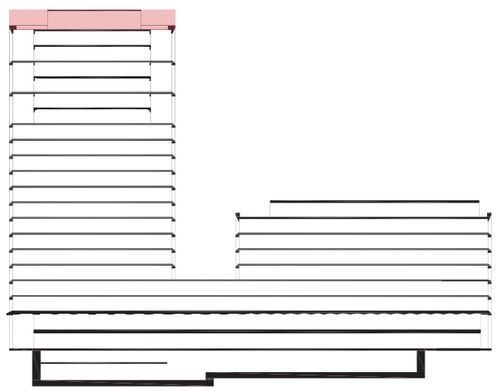
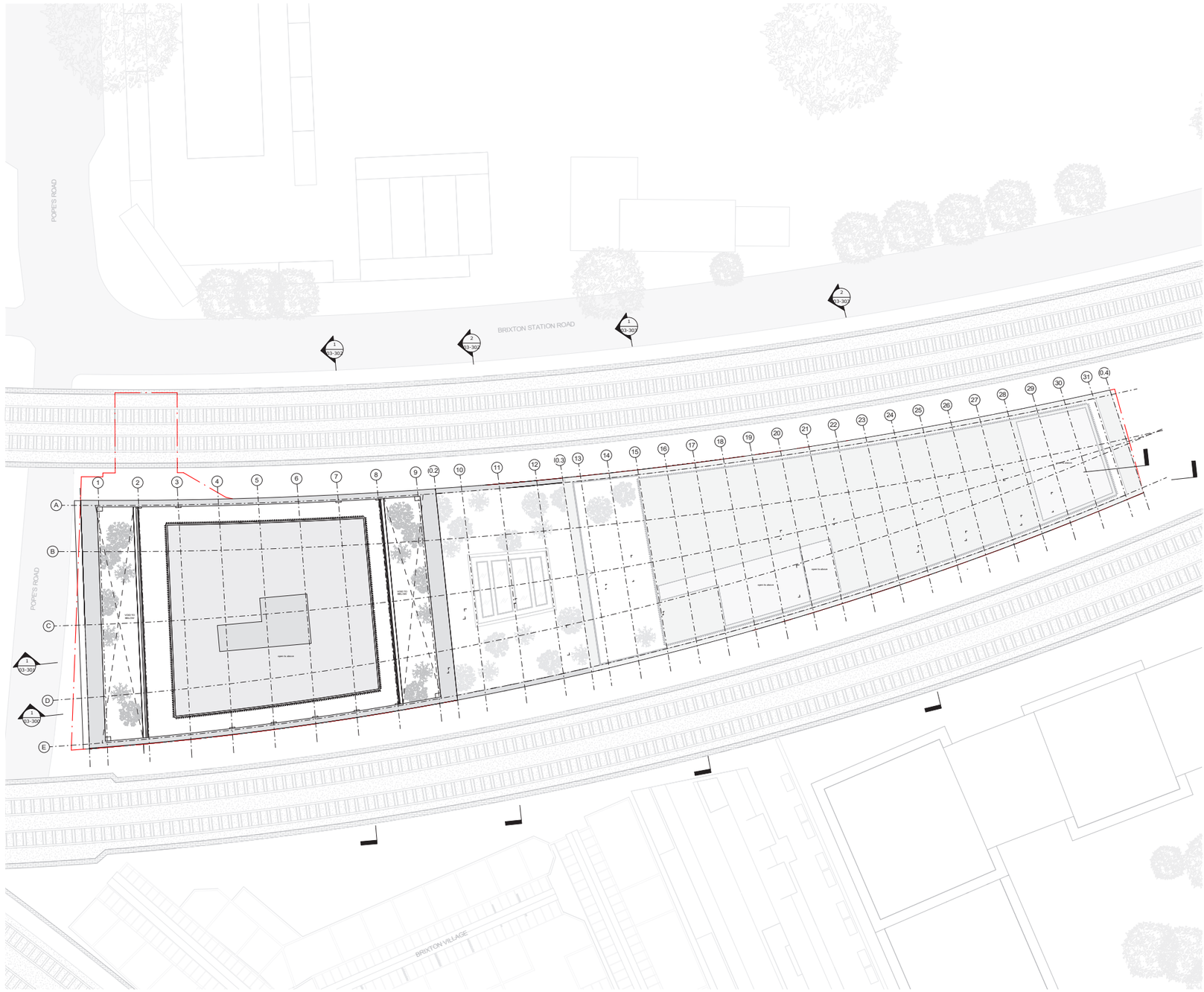


Fig. 4.3.9 Roof Level floor plan

ROOF PLAN

The scheme's roof plan displays the articulation of the massing, with the two blocks appearing to the west and east, and the garden appearing with a skylight in the centre.



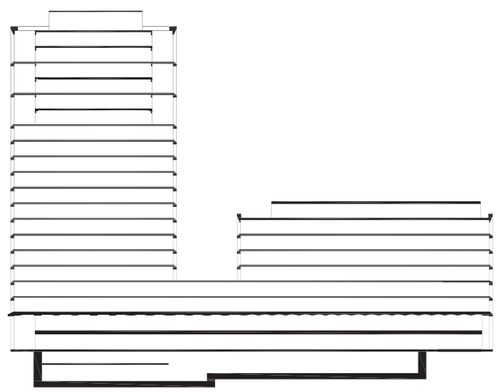


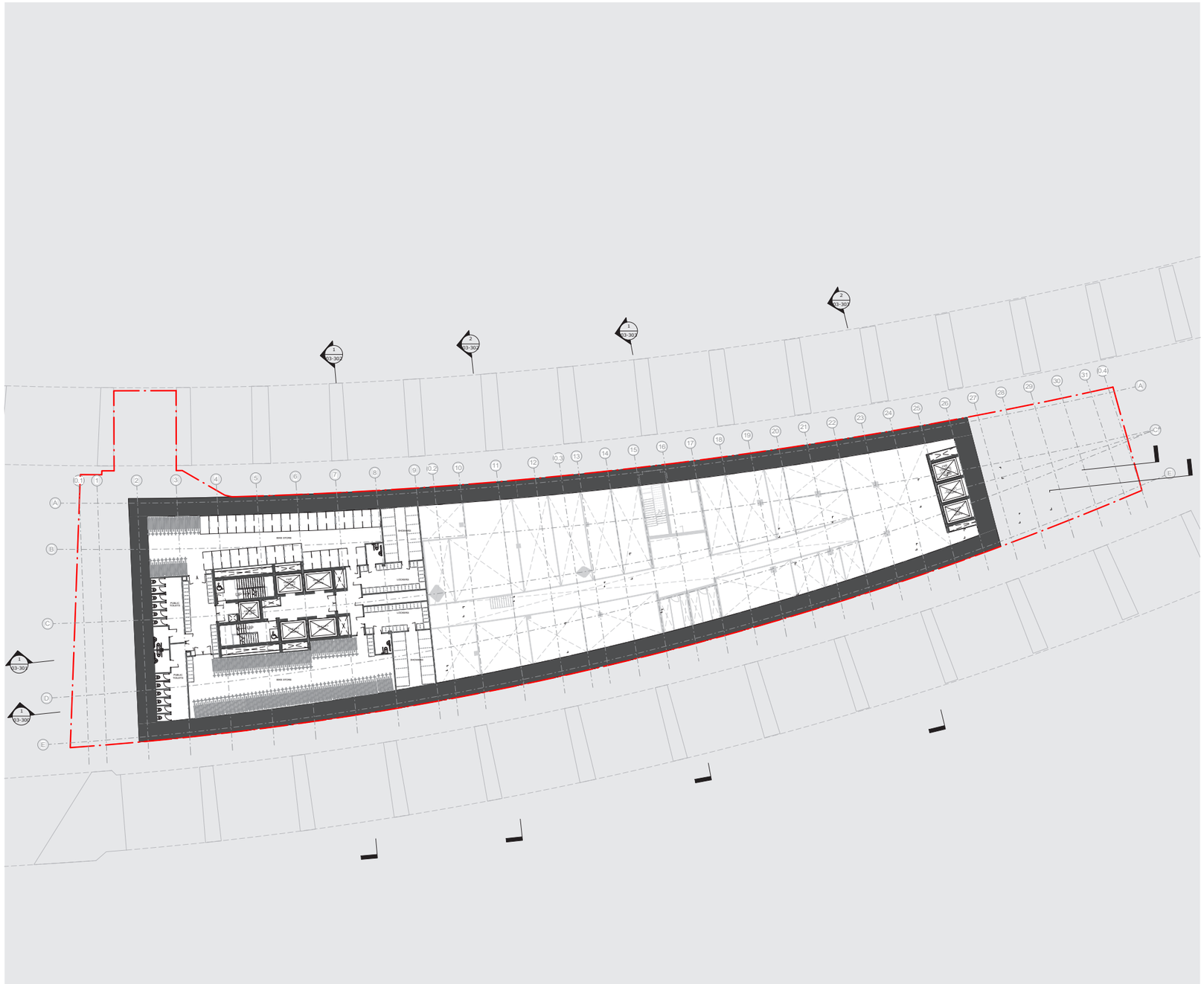
Fig. 4.3.10 Roof plan

4.3 FLOOR PLANS

BASEMENT MEZZANINE

This basement mezzanine floor contains a number of facilities for both office and market users. Public toilets are located in the western part of this floor and are accessible directly from the ground floor via a lift. Bike storage is available for office users and is directly accessible from ground floor via a dedicated lift.

The storage is subdivided into two parts to accommodate all the parking spaces, and is connected to a corridor that leads to lockers and showers. These are located on the eastern part of the floor and directly connected to the office lifts to facilitate the circulation of users within the building.



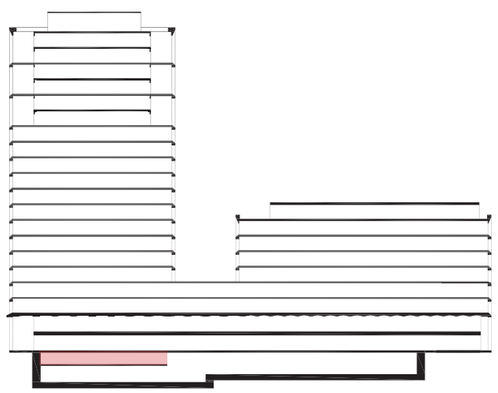
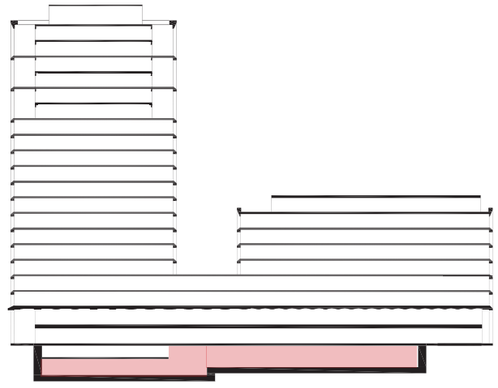


Fig. 4.3.11 Basement mezzazine floor plan

BASEMENT 2

This basement floor contains plant equipment that serves both blocks. Part of the equipment is located below Basement mezzanine level, whilst in the eastern part of the scheme, the floors step up to reduce the overall height of the basement.

Tanks are located in the western side below Basement mezzanine, electrical equipment is situated in the central zone, whilst mechanical equipment and generators in are located in the western side.



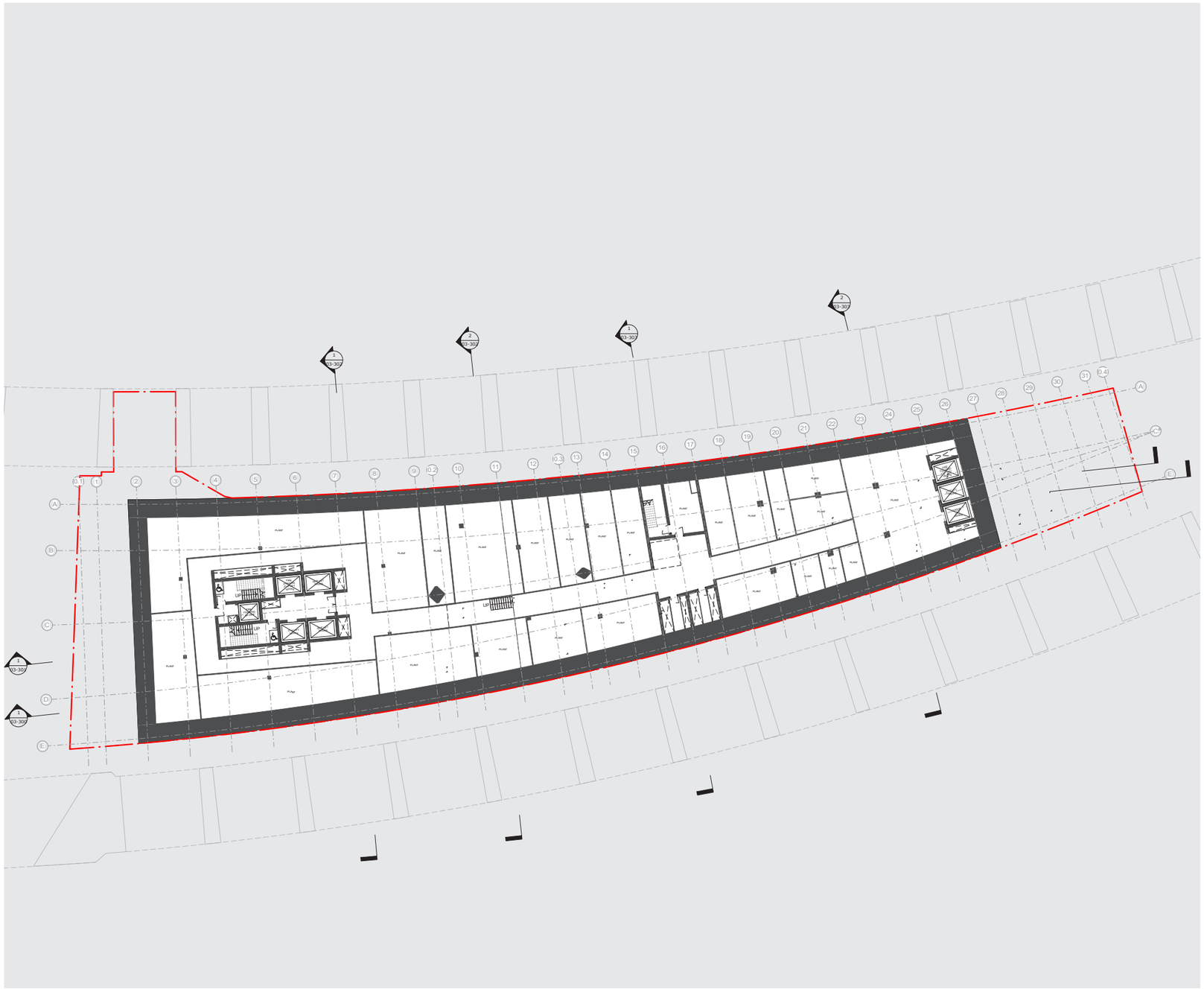


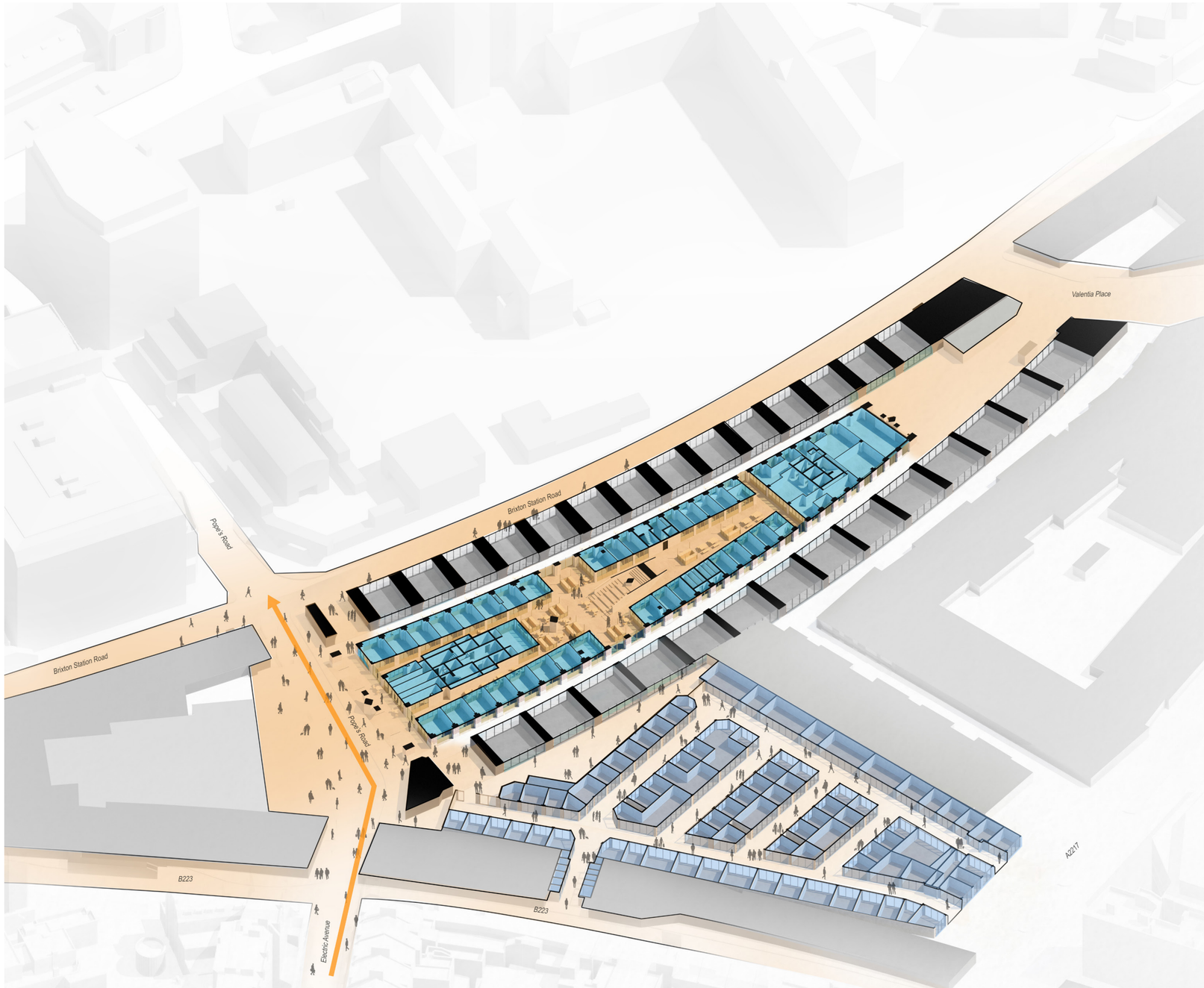
Fig. 4.3.12 Basement 2 floor plan

4.4 PUBLIC REALM

4.4.1 GROUND FLOOR PERMEABILITY - MARKET

The ground floor of the proposed scheme aims to enhance permeability of the urban grain, facilitating the connectivity between Brixton Village and Pope's Road, and more generally from south to north.

The proposed urban grain is directly inspired in size and layout by one of the existing markets' islands of retail, running from east to west. The entire in-between space is conceived as publicly accessible.



Key

- Existing Market Urban Grain
- Proposed Urban Grain
- Publicly Accessible Space
- Facilitated Pedestrian Connections North-South

Fig. 4.4.1.1Ground floor permeablity axonometric diagram

The proposed ground floor layout is highly permeable and seeks to enhance the porosity of the public realm, providing a soft threshold between inside and outside spaces.

Office lobbies are included within the market in order to facilitate the mix of uses sought after in the proposed scheme. The current layout with internal market streets and units allocated on the perimeter creates activated frontages.

Key

- Market internal street
- Market units
- Hotel lobby
- Railway arches
- Existing Brixton Village Units

Fig. 4.4.1.2 Proposed ground floor axonometric diagram

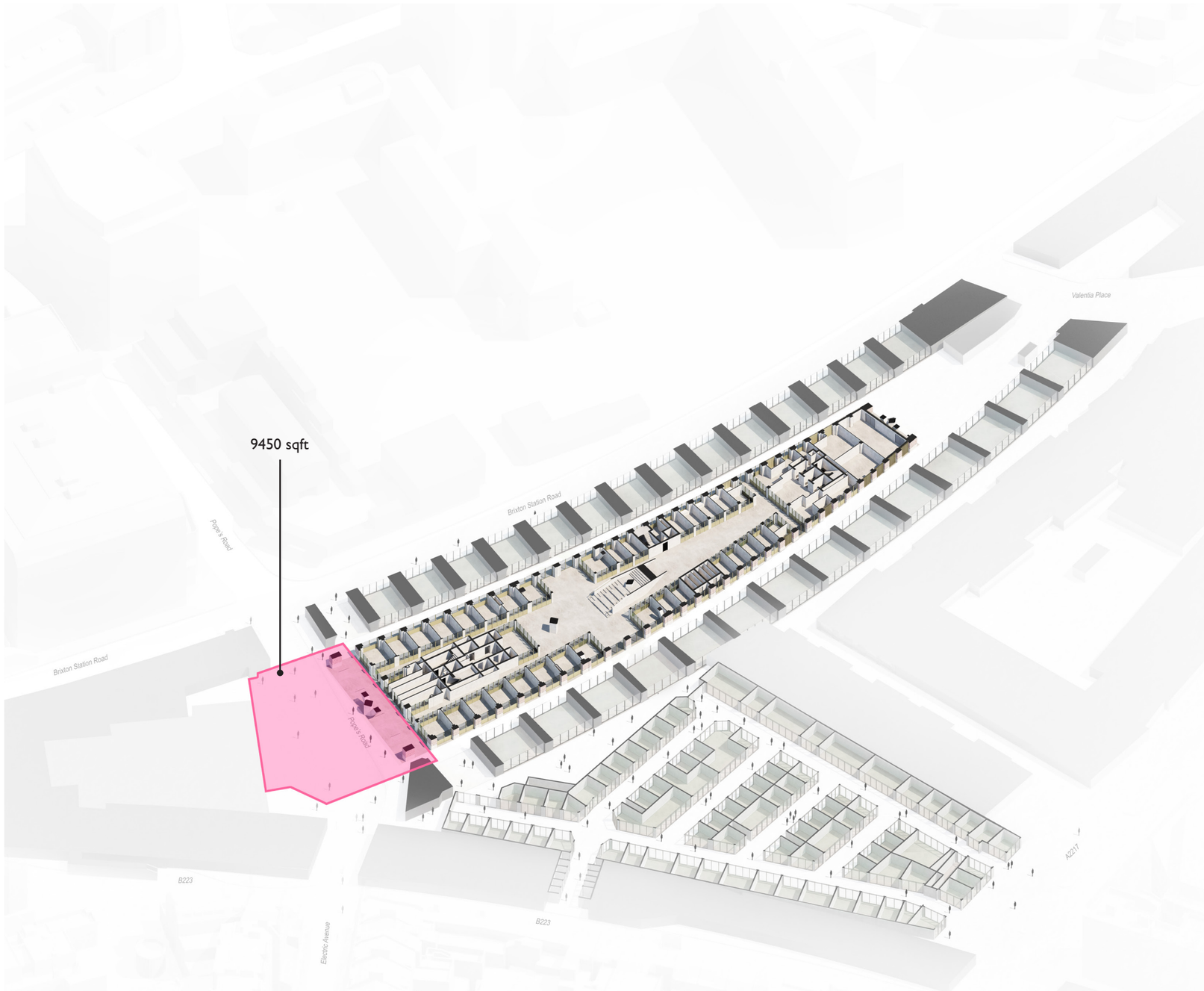
4.4 PUBLIC REALM

4.4.2 PUBLIC REALM COMPONENTS

External Public Real - Pope's Road

The proposal aims to implement the public realm through two main components. The first component is "external" which facilitate the implementation of Pope's Road.

As highlighted in the Public Realm Impact Assessment Report this proposed space shows over 80% capacity to accomodate more pedestrian movement and to accomodate nearly 700 people comforatably at any one time.



Key

External Component of the Public Realm

Fig. 4.4.2.1 Ground floor public realm component axonometric diagram

Internal Public Realm - Market Streets and Publicly Accessible Open Space

The second component is “internal” and comprehend all the covered publicly accessible spaces of the market, i.e the market streets and the open space.

Key

Internal Component of the Public Realm

Fig. 4.4.2.2 Ground floor public realm component axonometric diagram

4.4 PUBLIC REALM

4.4.3 PUBLIC REALM ON POPE'S ROAD

The existing public realm surrounding Pope's Road is generally compressed, especially during active street market hours. However, despite the poor urban quality as highlighted in the Public Realm Impact Assessment Report this space does accomodate the existing peak usage, which relates to street market at the weekend. Modelling has shown that this will remain the peak period even after the development is completed. Additionally the poor condition of the existing public toilets block, as well as the existing fence blocking the back of the station, encourages the accumulation of garbage and antisocial behaviour.

1. EXISTING SITUATION

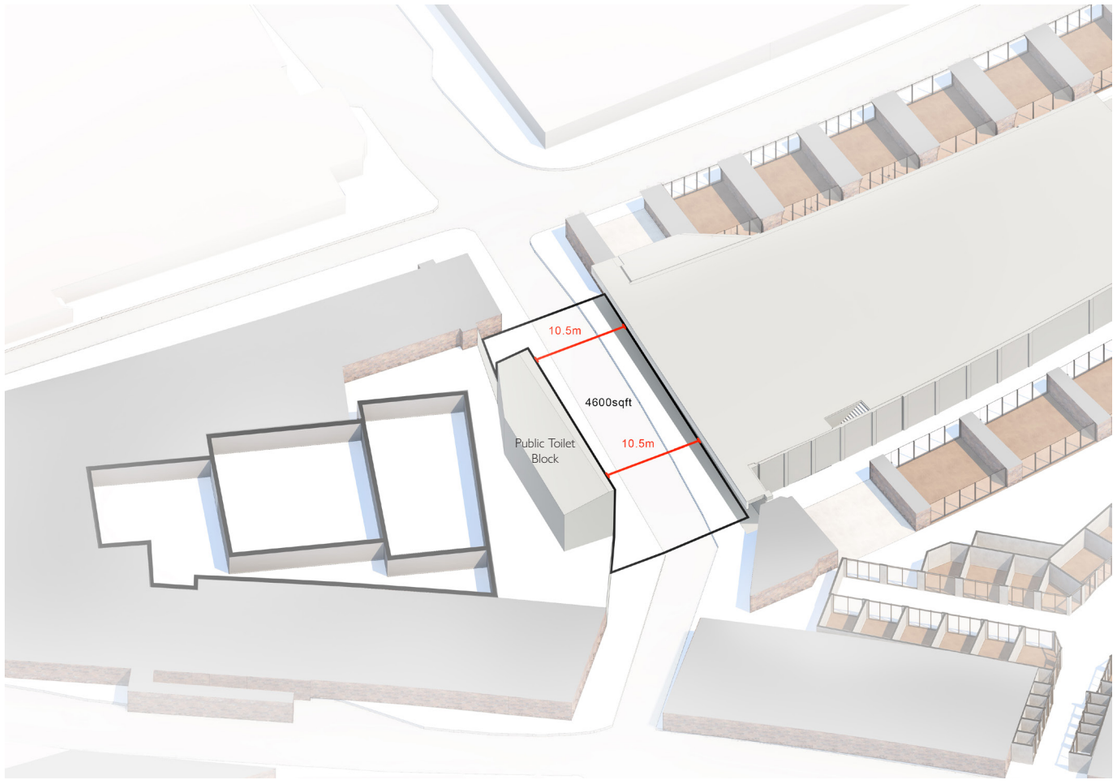


Fig. 4.4.3.1 Existing situation diagram

Our proposed building frontage is repositioned one metre east as compared with the current existing building.

2. PROPOSED BUILDING FRONTAGE ALIGNMENT

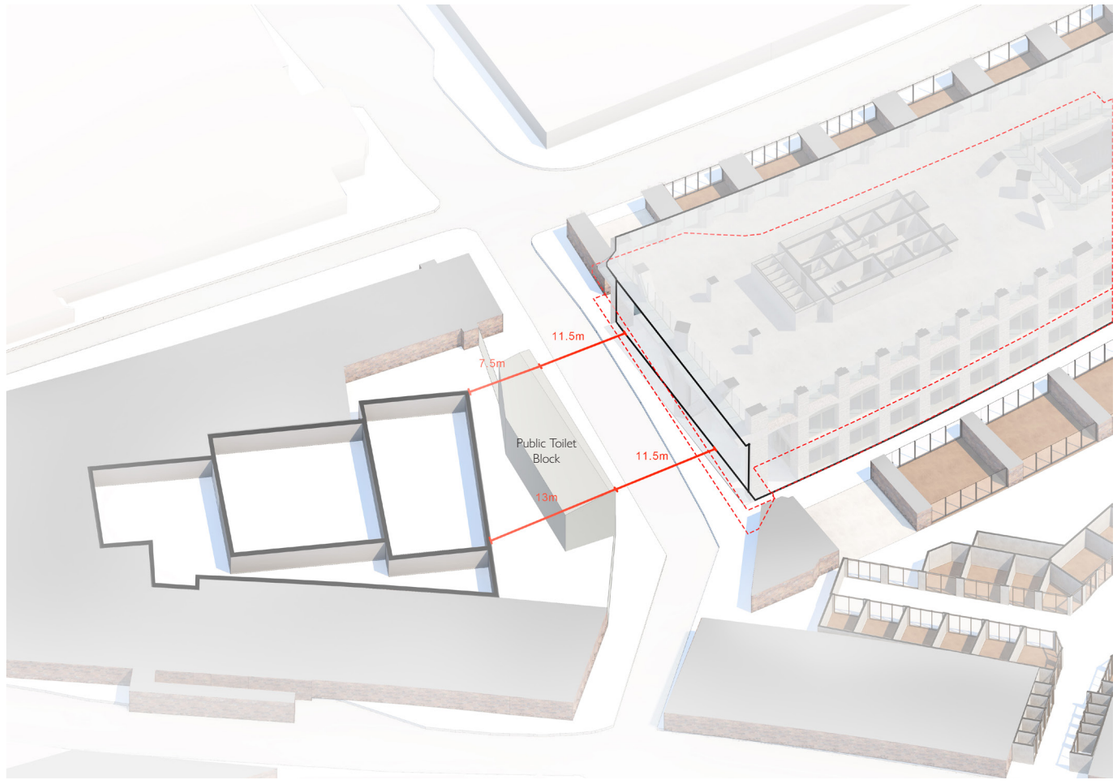


Fig. 4.4.3.2 Proposed building frontage alignment diagram

By relocating the public toilets, we have created an opportunity for an unobstructed, carefully landscaped public area with the potential to provide a central spacious area for Brixton Railway Station users and the local area.

The removal of this structure will require approval and review from local authorities and appointed contractor in line with sequencing and phasing of the construction process.

3. PUBLIC TOILET RELOCATION

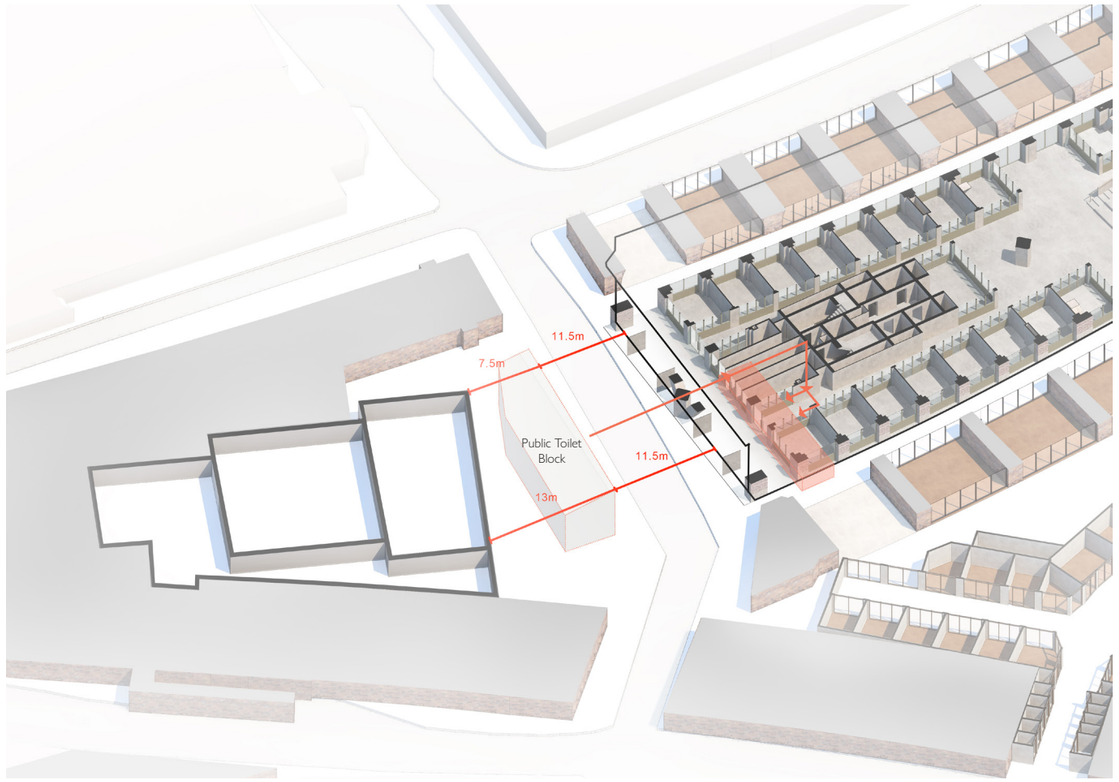


Fig. 4.4.3.3 Public toilet relocation diagram

Public toilets will be relocated to the basement of our proposed scheme, and will be directly accessible from ground floor via lifts.

The public toilets will be available to the public, free of charge, maintained to a high standard, with current provisions adequately increased.

4. PUBLIC TOILET RELOCATION ACCESS

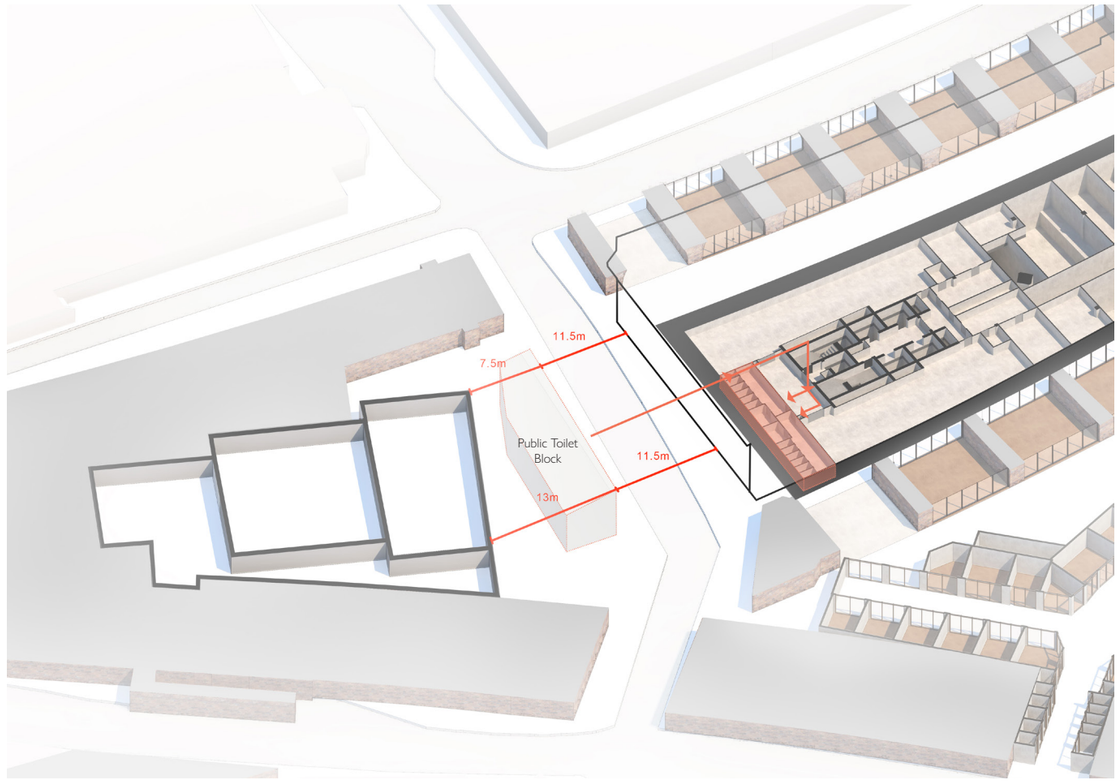


Fig. 4.4.3.4 Public toilet relocation access diagram

4.4 PUBLIC REALM

4.4.3 PUBLIC REALM ON POPE'S ROAD

The relocation of public toilets will generate a significantly larger and more permeable public realm for Pope's Road and a drastic improvements for Brixton Central.

This newly proposed public space will create a relief area of a considerable size that currently can't be found in the vicinity. The newly proposed public toilet will also address the public urination problem which is currently a big issue within this part of the town centre.

To further extend this public space, we propose a setback of six metres that will create shelter for users and traders, whilst also generating a soft threshold between Pope's Road and the proposed market.

5. INCREASED PUBLIC REALM

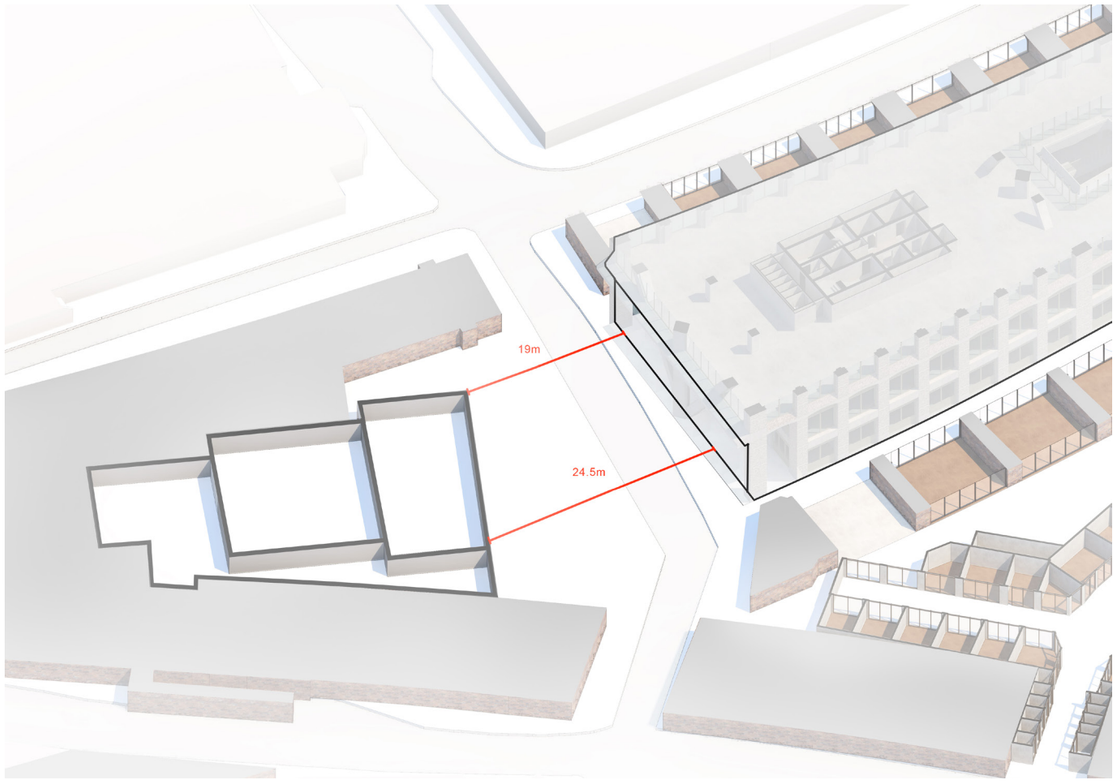


Fig. 4.4.3.5 Increased public realm diagram

6. PROPOSED BUILDING ADDITIONAL SETBACK

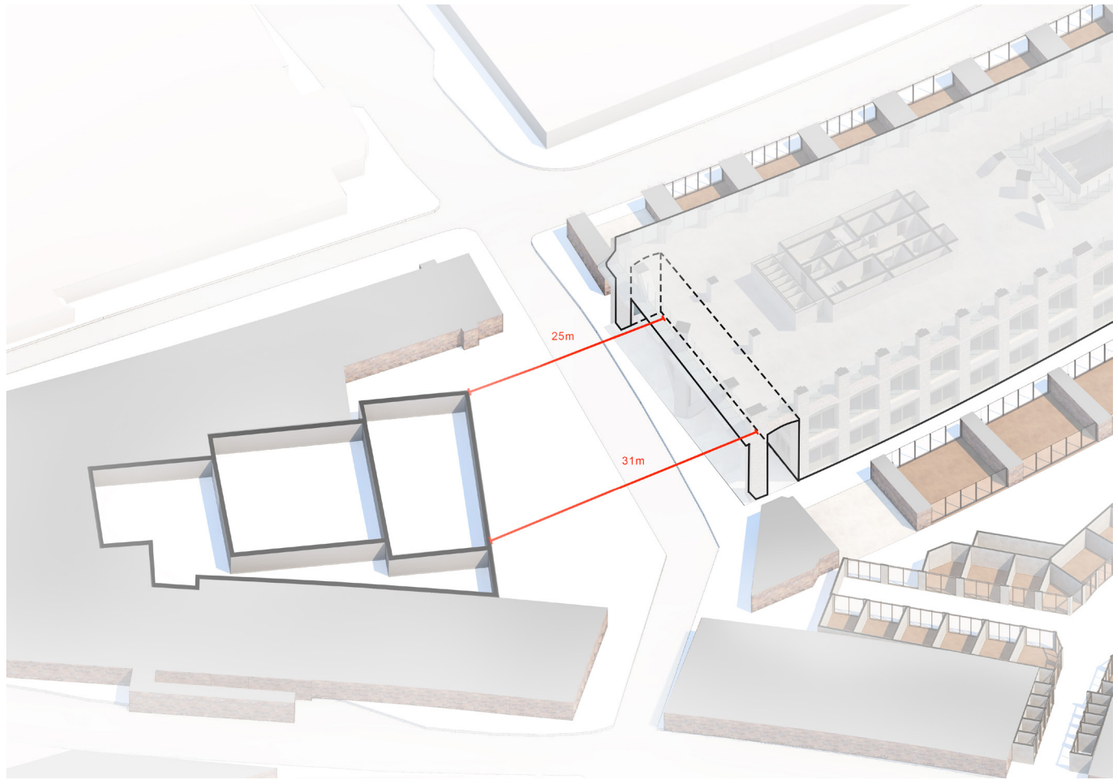


Fig. 4.4.3.6 Proposed building additional setback diagram

Our newly proposed public realm will provide considerably greater space and will be larger in size compared to the existing, creating a new, improved public space for central Brixton. The new scheme will allow up to 9450 sqft of enhanced open space.

7. PROPOSED PUBLIC REALM

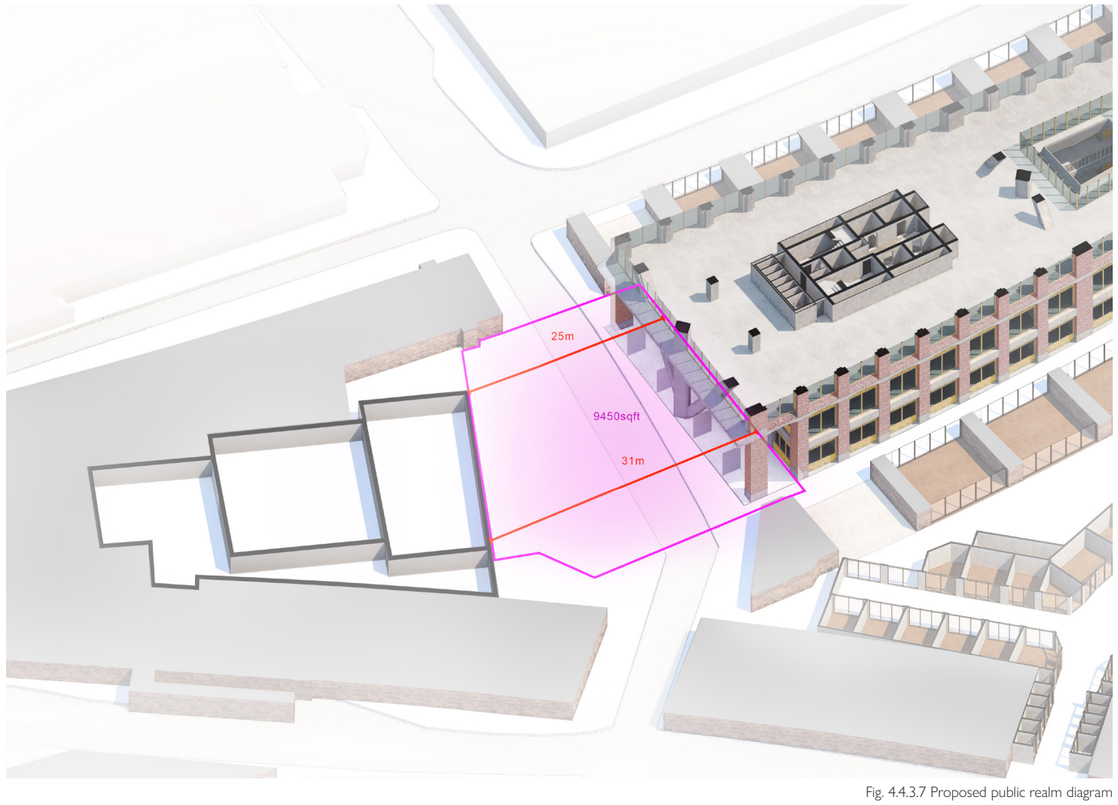


Fig. 4.4.3.7 Proposed public realm diagram

The new activated public realm will be a porous and permeable human-scaled public space, providing increased place for leisure and interactivity within Brixton's urban core.

It will reconcile the needs of the often conflicting congestion of the road alongside the compact nature of the market streets and create good quality and shared space that encourages social interaction and creates a more enjoyable and safer environment.

8. ACTIVATED PUBLIC REALM

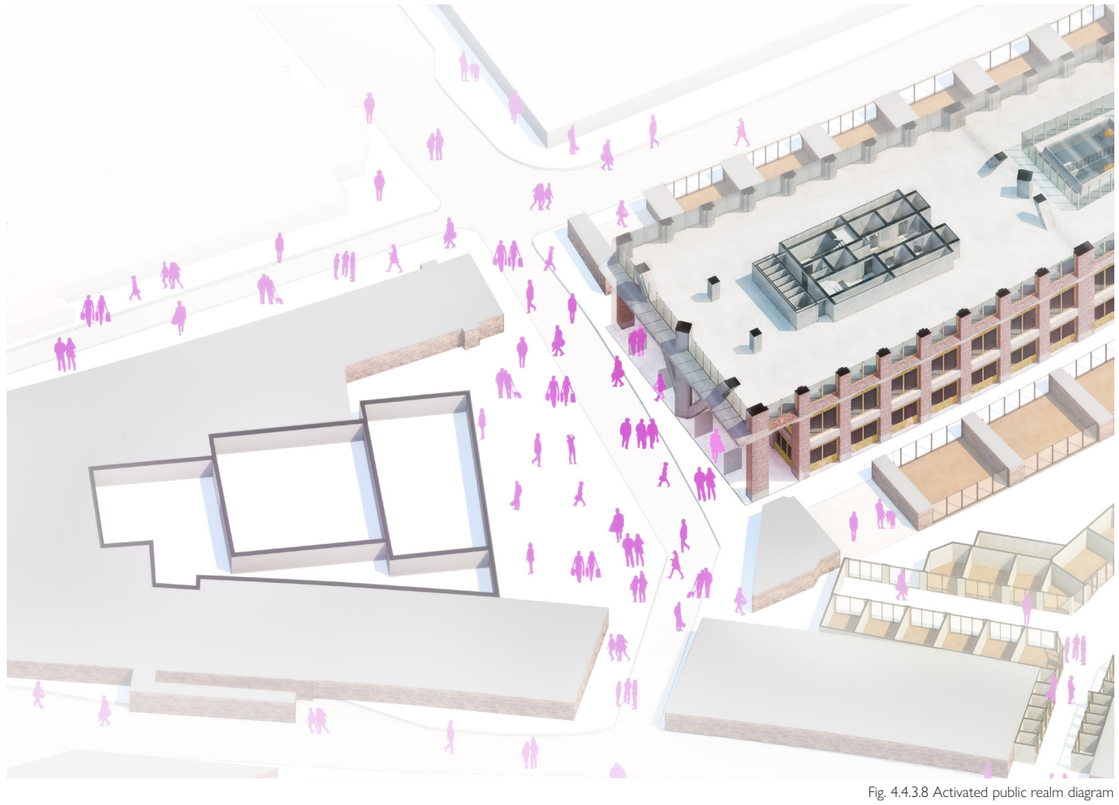


Fig. 4.4.3.8 Activated public realm diagram

4.4 PUBLIC REALM

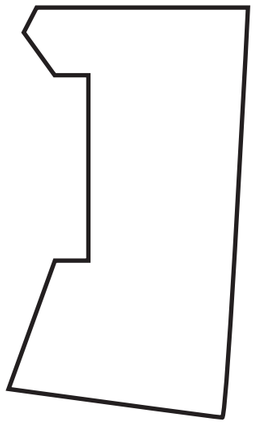
4.4.3 PUBLIC REALM ON POPE'S ROAD

The proposed public realm will amount to almost twice the size of the existing, and will offer considerable space as compared with other successful examples of public realm within the borough

The increase of permeability will open up the surrounding markets that lead to Brixton Station and offer a new access route that enables easier navigation and and travel from Station Road's various amenities and open up towards Atlantic Road and Electric Avenue.

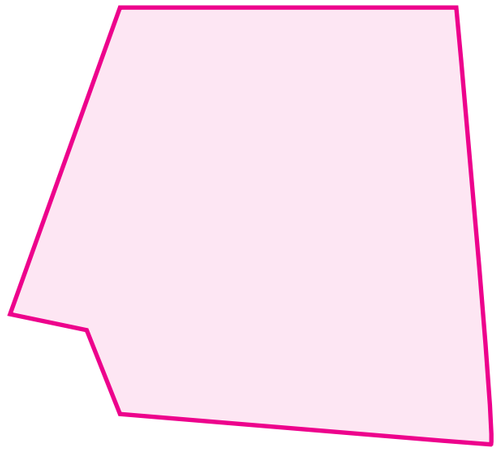
This will also provide improved network routes and wider accessibility to other covered market spaces such as the nearby Brixton Village.

As outlined in the Public Realm Impact Assessment Report the proposed space would be able to accomodate 12,500 people an hour compared to the existing peak at the weekend of c. 2,400. In addition it would be able to accomodate nearly 700 people comfortably at any point in time.



4600 sqft

Pope's Road
Existing Condition



9450 sqft

Pope's Road
Proposed Public Realm

Fig. 4.4.3.9 Diagramatic comparison between existing and proposed public realm

The proposed public realm on Pope's Road will facilitate the potential future opportunity for the redevelopment of Brixton Station, particularly owing to the possibility of the introduction of a new entrance directly on Pope's Road.

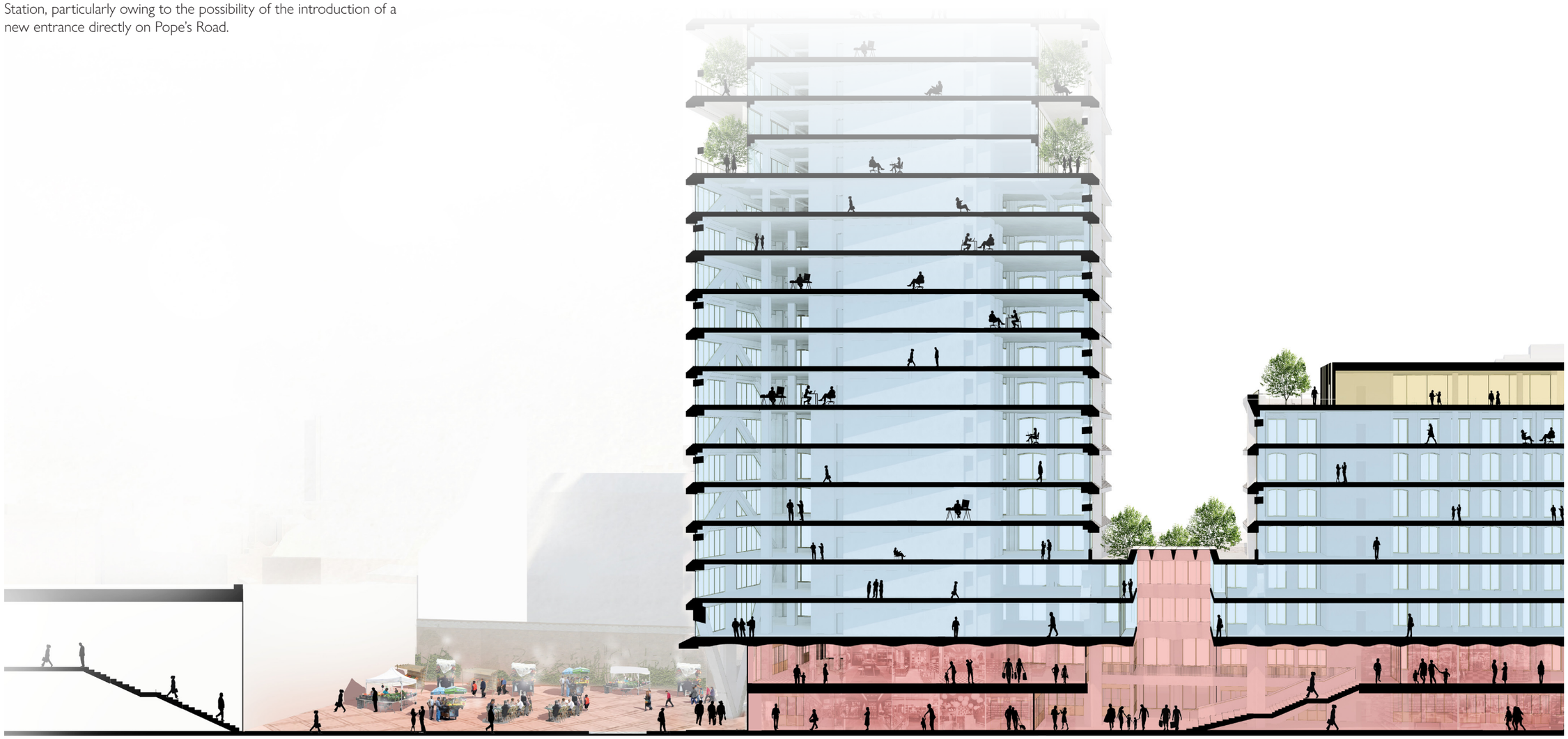


Fig. 4.4.3.10 Proposed diagramatic longitudinal (East-West) section

Fig. 4.4.3.11 Illustrative View of the proposed public realm on Pope's Road during the week end when street market is present

Fig. 4.4.3.12 Illustrative View of the proposed public realm on Pope's Road from the entrance of the proposed market looking at the indicative remodelled Brixton Station entrance

4.4 PUBLIC REALM

4.4.4 MARKET STREETS

The market streets act as circulation paths, activated by frontages of the retail units directly inferred from the existing Brixton Market.

These are inspired by the current aesthetic and style of the existing markets, designed in a bid to extend and preserve the character and authenticity that has made the market so popular over the years.

Fig. 4.4.4.1 Existing market street in the Brixton Village

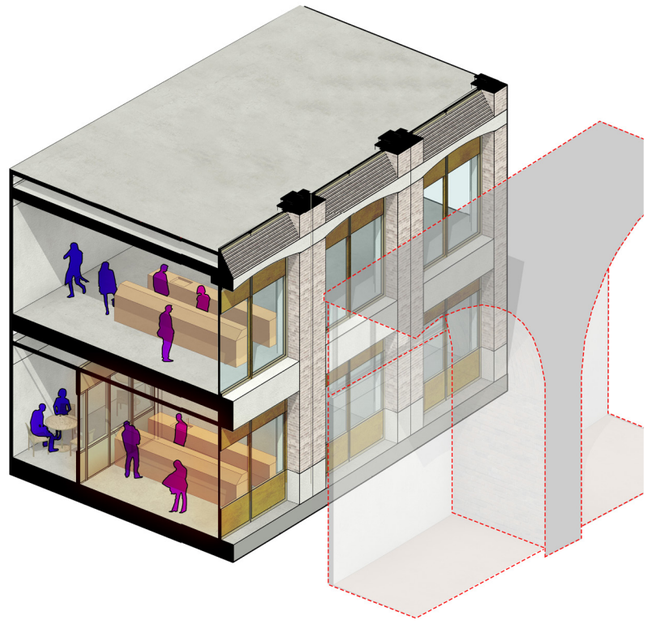
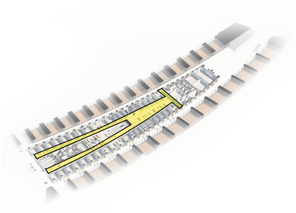


Fig. 4.4.4.2 Axonometric sectional diagram of the proposed internal market streets

Fig. 4.4.4.3 Illustrative view of the proposed internal market streets



The market layout offers the perfect environment for a diverse range of uses and functions.

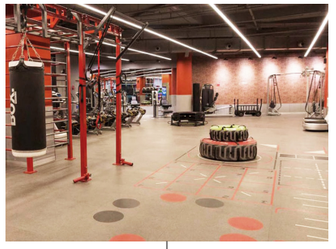
This diagrammatic section illustrates the vivacity brought about by the diverse mix of uses proposed.

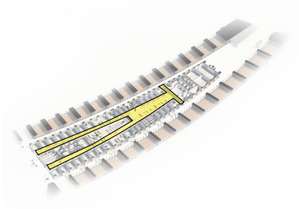
The centre of the section showcases the flexible market space and the visual permeability that is maintained with offices above.

Potential indicative uses are indicated in the diagrammatic section.

Flexible Spaces (B1 - B1C)

Leisure / Wellness (D2)





Market Units (A1-A3)

Flexible Spaces (B1 - B1C)

Food Hall

Fig. 4.4.4.4 Illustrative diagrammatic perspective section of the proposed internal market streets

4.4 PUBLIC REALM

4.4.5 PUBLICLY ACCESSIBLE OPEN SPACE

The proposed flexible market facilitates permeability across the ground floor and accommodates the overall porosity of the urban grain by stitching it together.

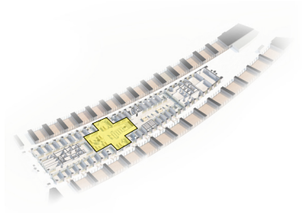
Within the continuous and expansive ground floor plane, the space is proposed to accommodate units of various scales and sizes within this central part of the scheme.

The publicly accessible and open space takes inspiration from the archetypal form of an open courtyard which becomes a central atrium once it is enclosed within the building.

Acting as a catalyst, it intends to bring together the continuous activity and flow of people from ground floor level, creating vertical circulation through visual continuity and transparency that culminates in the sky visible through the glass roof.

Several public buildings today employ this typology as a key feature of their spatial organization, which due to their character has often become a key strength and element for the success of the overall proposal.

Along with the implementation of Pope's Road, this is the second major component proposed within the scheme to further formulate the creation of a new and accessible public realm that maximises the potential of interaction within the building.



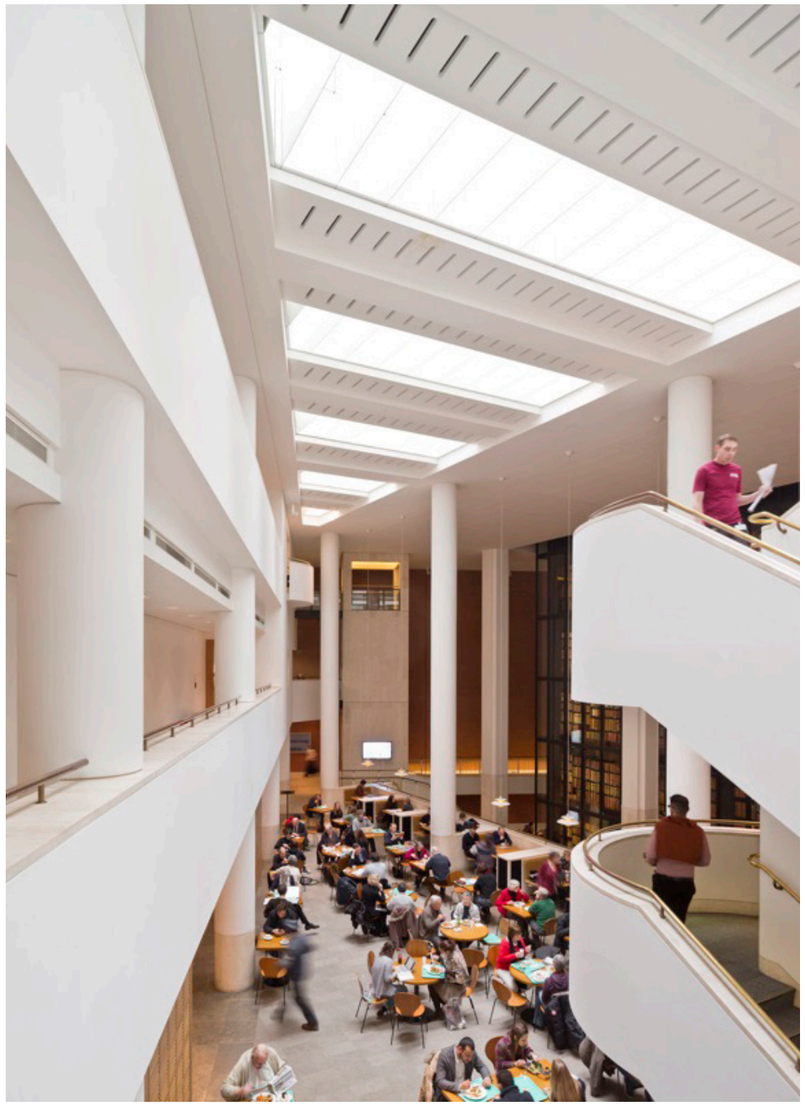


Fig. 4.4.5.1 British Library Internal Public Space, London

Fig. 4.4.5.2 St. Peter's Seminary, Cardross

Fig. 4.4.5.3 Spitalfields Market, London

Positioned within the centre of the site, it influences the overall massing of the scheme and provides a designated split in between the two volumes. This allows for visual permeability but most impotently allow natural light to flood into the interior spaces.

Its central location represents a key moment within all the circulation flows of the building from both inside and outside emphasizing its relevance and importance for the success of the proposed market.

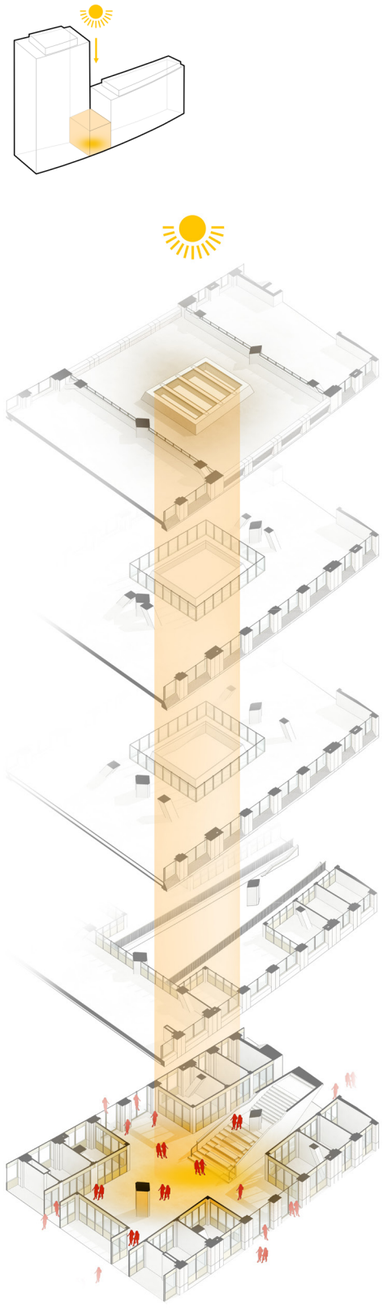


Fig. 4.4.5.4 Publicly Accessible Open Space vertical continuity axonometric diagram

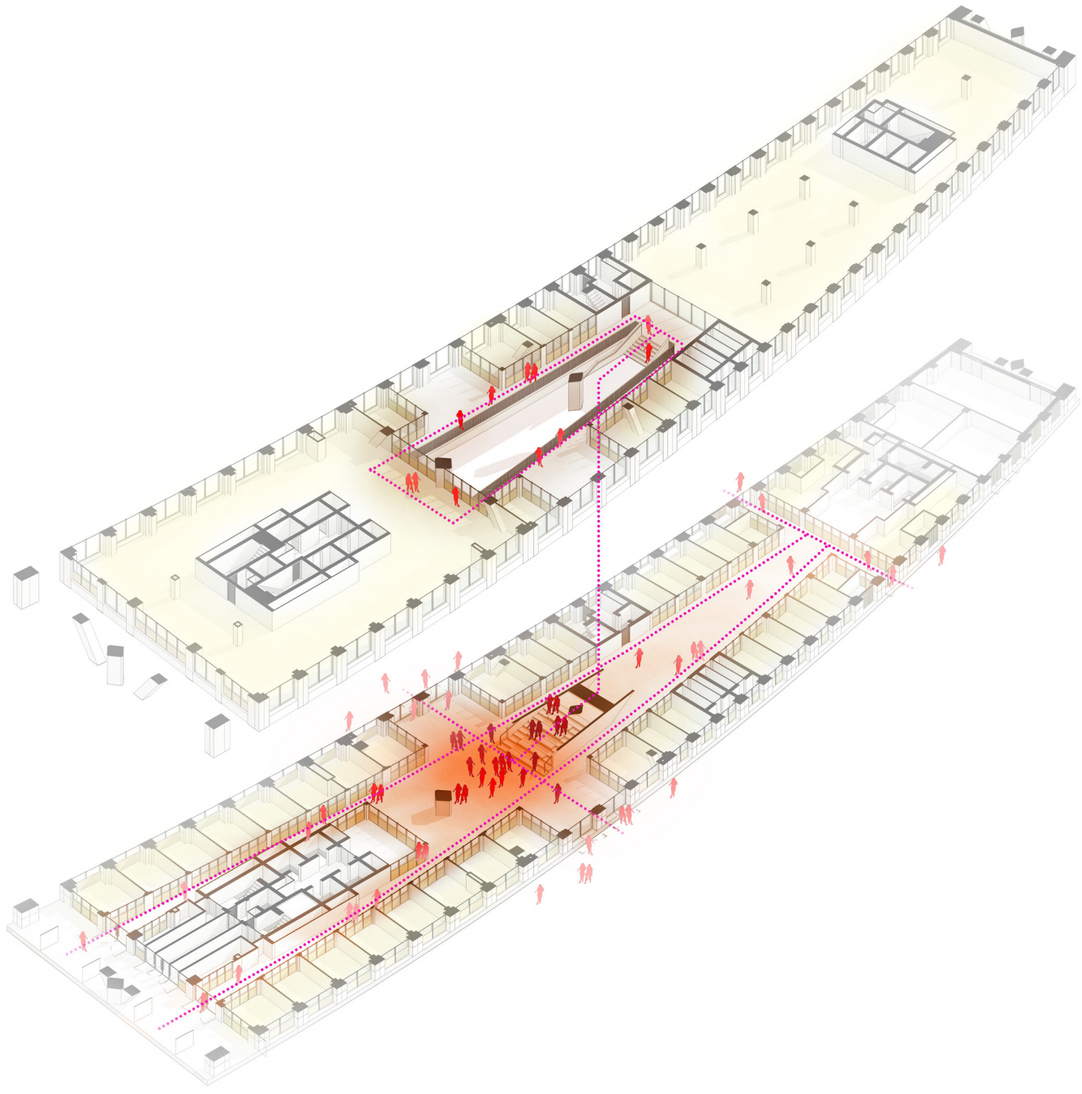
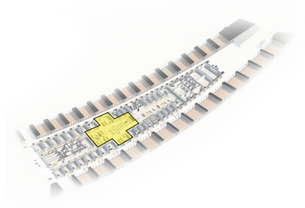


Fig. 4.4.5.5 Publicly Accessible Open Space ground & first floor circulation axonometric diagram



4.4 PUBLIC REALM

4.4.5 PUBLICLY ACCESSIBLE OPEN SPACE

The publicly accessible and open space is located in the heart of the proposed market and represents a transformative moment that is heightened by communal activity.

It is conceived as a network and hall intended for collective public use . It functions primarily as a central square, providing places to sit and enjoy, whilst also acting as a food hall and extension of the market on a normal day scenario, but also offers the potential to host a number of various activities and events.

The space sits in between the two office lobbies making it a convergence point for office employees, traders and the general public. A generous triple height space visually connects the retail activities at ground and first floors with the offices located above.

A large skylight fills the space with natural light enhancing the area as a pleasant and airy space to engage and spend time in, which is pivotal for the proposal.

Occupying the void and perimeter of this space and on the first two floors, is a balcony that overlooks the space and facilitates visual permeability.

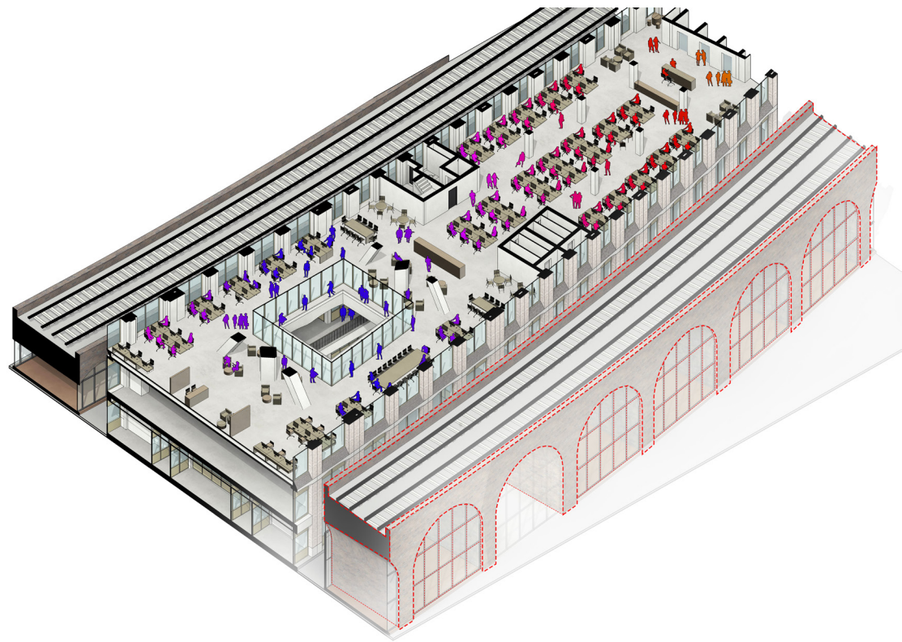
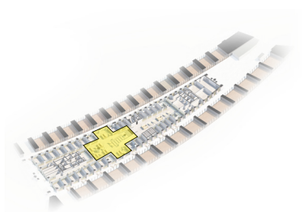


Fig. 4.4.5.8 Publicly Accessible Open Space second floor axonometric diagram



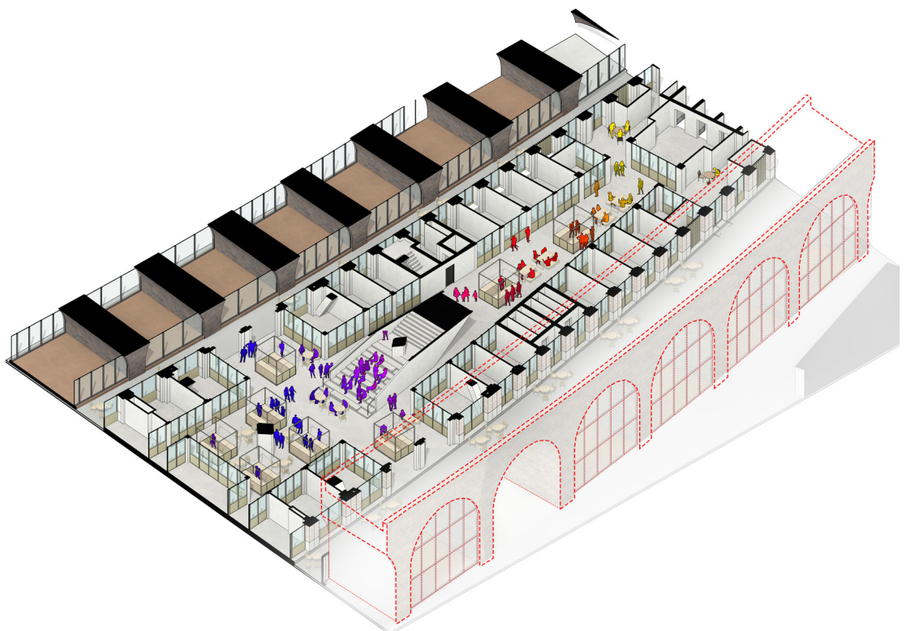


Fig. 4.4.5.6 Publicly Accessible Open Space ground floor axonometric diagram

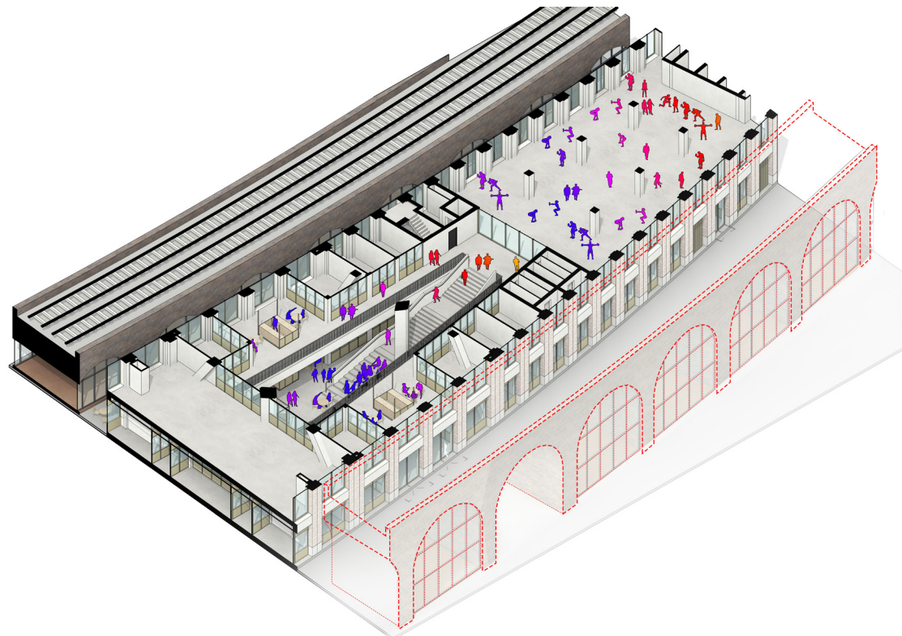
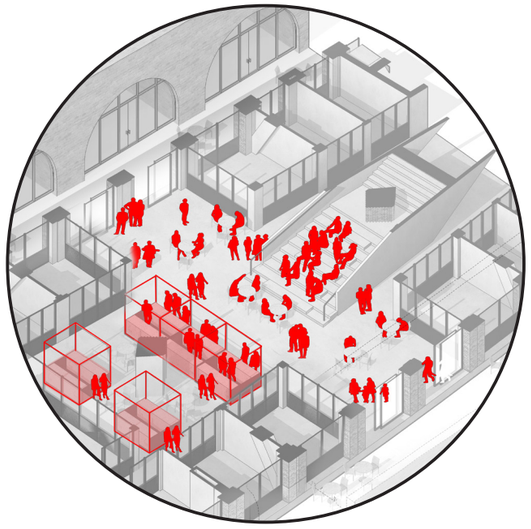
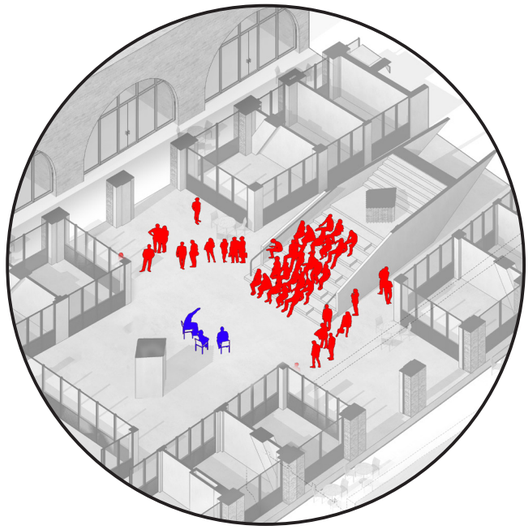


Fig. 4.4.5.7 Publicly Accessible Open Space first floor axonometric diagram

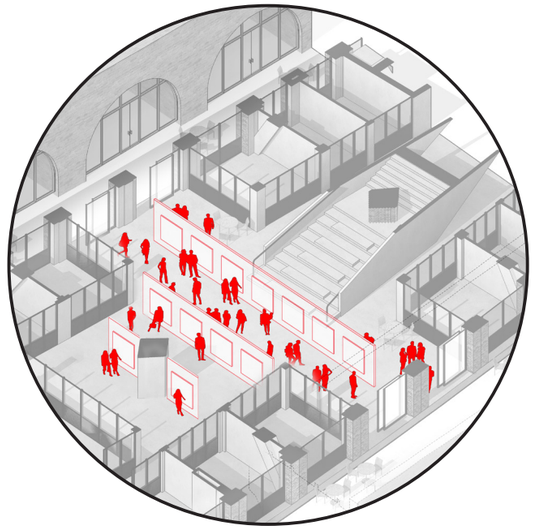
The publicly accessible open space has been designed to be exceedingly versatile, and has the capacity to host a wide range of events from community events to exhibitions.



Main Scenario
Market Food Court



Scenario 2
Event Space Available for Community Use



Scenario 3
Exhibition Space

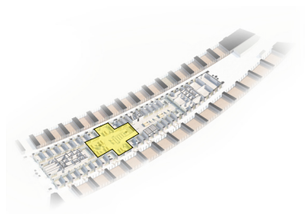


Fig. 4.4.5.9 Publicly Accessible Open Space food hall scenario diagram

Fig. 4.4.5.10 Publicly Accessible Open Space community events space scenario

Fig. 4.4.5.11 Publicly Accessible Open Space exhibition space scenario diagram

Fig. 4.4.5.12 Illustrative view of the Publicly Accessible Open Space from ground floor

Fig. 4.4.5.13 Illustrative view of the Publicly Accessible Open Space from the second floor

Fig. 4.4.5.14 Illustrative detail view of the Publicly Accessible Open Space from ground floor

Fig. 4.4.5.15 Illustrative view of the Publicly Accessible Open Space from ground floor - community event scenario

Fig. 4.4.5.16 Illustrative view of the Restaurant

4.4 PUBLIC REALM

4.4.6 PUBLIC REALM BENEFITS

The proposed “external” and “internal” public realm implemented by the scheme sums up to a total of 19,130 sqft.

Key

External Component of the Public Realm

Internal Component of the Public Realm

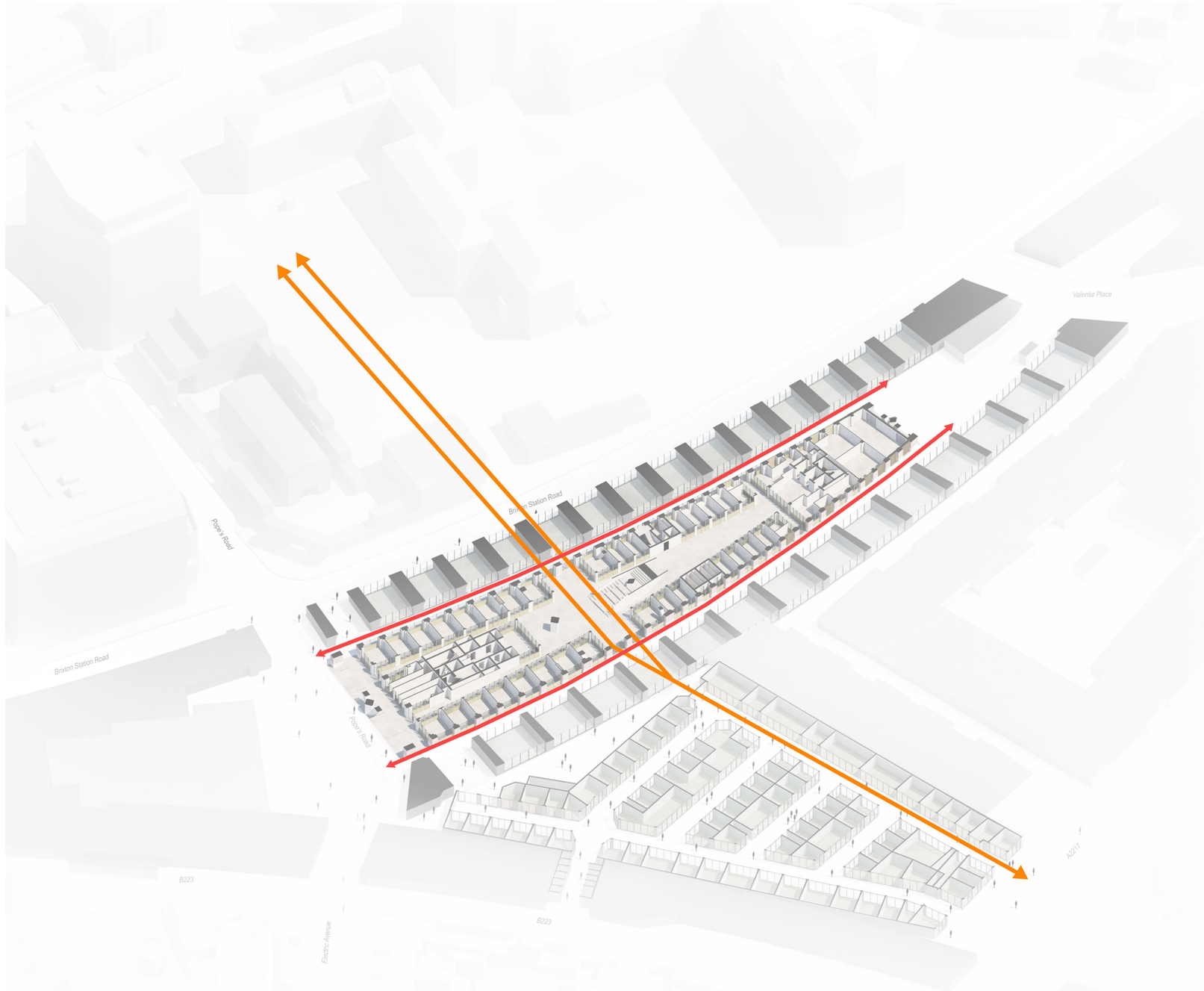
Fig. 4.4.6 Ground floor public realm components axonometric diagram

4.4.7 POTENTIAL FUTURE CONNECTIONS

The current layout of the ground floor of the scheme facilitates two major potential connections that might come forward in the future.

One is represented by the north-south connection from the Brixton Village through the publicly accessible open space into the Pop Brixton site.

The second is the public accessibility of the side streets with the potential implementation of the existing arches.



Key

North-South Potential connectivity

East-West Potential connectivity

Fig. 4.4.7 Ground floor potential future connectivity axonometric diagram

4.5 FAÇADE LANGUAGE

4.5.1 CONTEXTUAL CONSIDERATIONS

Our research into Brixton's built context informed our understanding of some of the most prelevent architectural features found in neighbouring buildings.

As such, our scheme presents a reinterpretation of many of these features, with their profound and creative re-imagining constituting the starting point of our design approach.

The tripartite organisation of the façades, the horizontal lintels, arched and triangular forms of the window heads, and the rich detailing were some of the influential elements considered throughout the design evolution of the scheme.

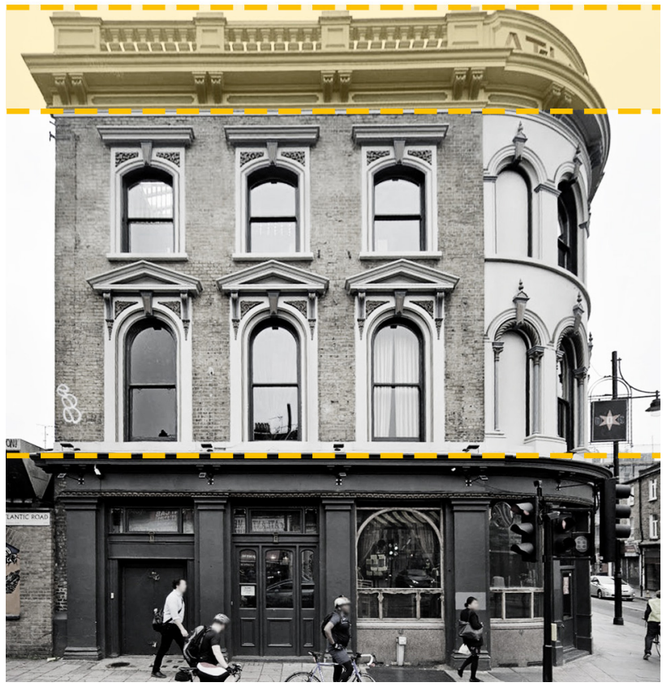


Fig. 4.5.1.1 Building façade on Atlantic Road

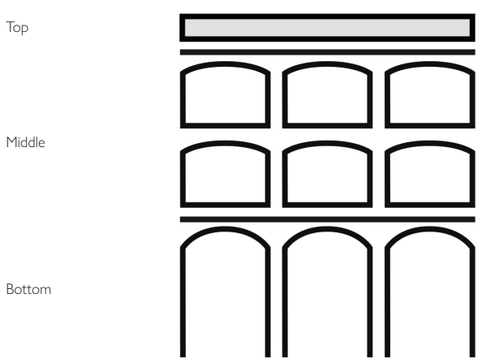
Fig. 4.5.1.2 Building façade detail on Electric Avenue

Fig. 4.5.1.3 Building façade detail on Acre Lane

Contextual Reference
Façade Tripartite Structure

Contextual Reference
Arch Form & Horizontal Lintel

Contextual Reference
Triangular Form & Linear Elements



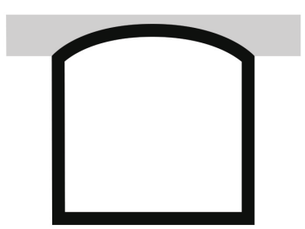


Fig. 4.5.1 Building façade on Brixton Station Road

4.5.2 SITE CONSIDERATIONS

In addition to the various contextual references that influenced the design of our proposal, there were other key considerations specifically related to our site that have been taken into account during the project's design process.

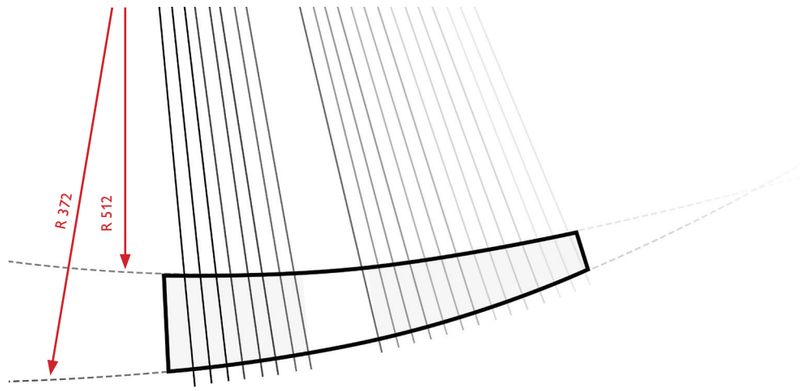
Firstly, the orientation of the site, which is almost perfectly orientated on the north/south and east/west axis, provided an opportunity to embrace natural sustainable strategies very early on in the design process.

Every façades can be treated in a different way in oder to perform better: the south has a considerable depth to deal with direct sunlight, the north is flat to maximize solar gain and the east-west deep horizontal elements that deal with low sun rays.

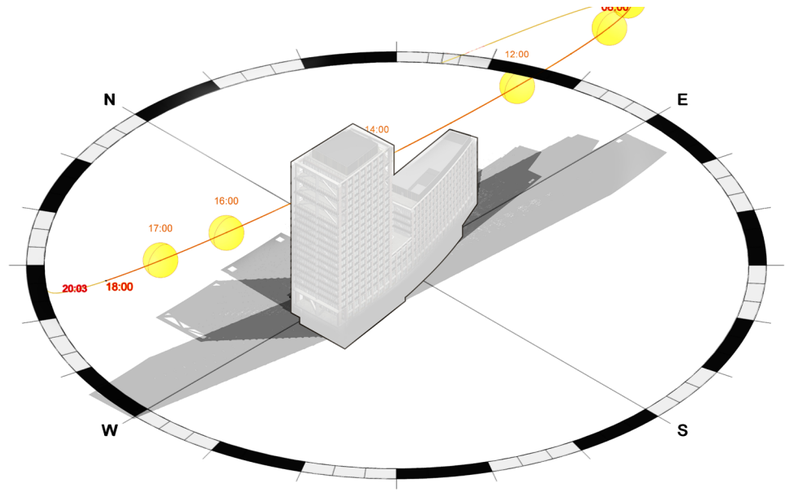
Secondly, the shape of the site creates two distinctive types of façades: the north/south frontage, which is long and curved, and the east/west frontage, which is short and linear.

This differentiation offered the opportunity for treating the two façades differently, through the creation of a modular and horizontal façade that addressed the curve on the north/south, and a linear and vertical one on the east/west.

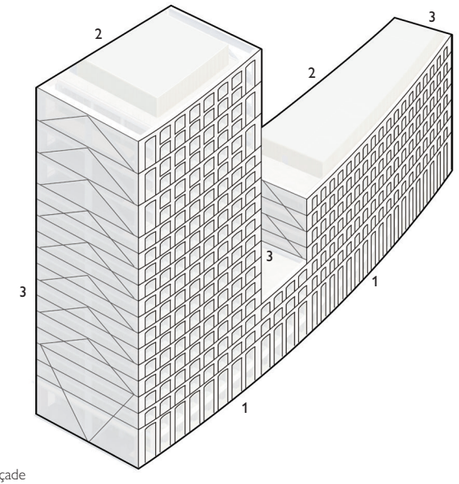
Site Shape
Radial Structure



Site Orientation
Site Light Exposure & Shadows



Site Shape
Façade Types & Articulation



1. Long Façades: South Façade
2. Long Façades: North Façade
3. Short Façades: West/East Façade

Site Orientation
Façade Passive Design

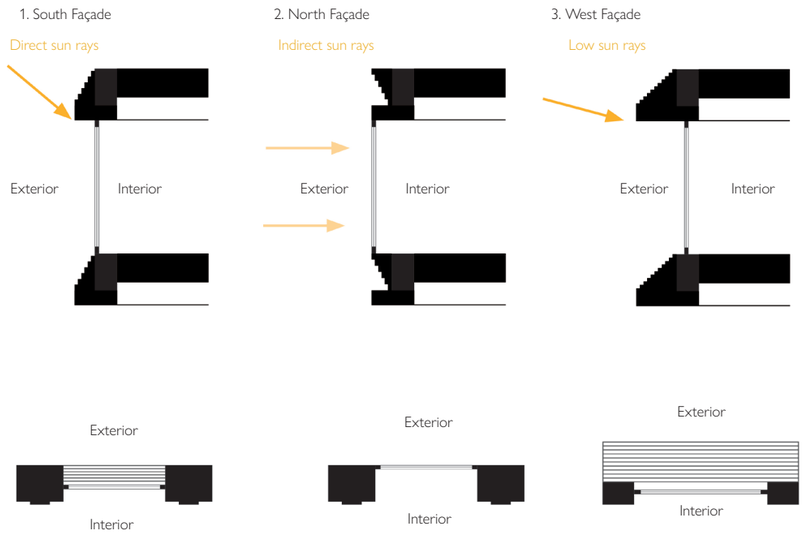


Fig. 4.5.2 Site considerations diagrams

4.6 FAÇADE CONSIDERATIONS

4.6.1 FAÇADE ASPECTS

There are three different façade aspects to the building, each of which respond to their immediate context.

Façade 1- East/West

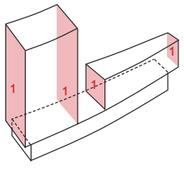
The east/west façades are the shortest, the most vertical and the only two that face the streets, respectively Valentia Place and Pope's Road. A linear language of deep horizontal trays has been used, along with a central vertical pier and exposed structural bracing, which creates a pattern that introduces verticality throughout the façade.

Façade 2- South

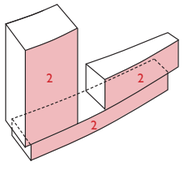
The south façade is composed of arch openings organised in a linear modular grid, which emphasises the curved shape of the site. These openings have a deep reveal that controls the sunlight intake, whilst windows are enriched by a brick stepped apron in the lower register. Slight variations in the composition occur between the west and east block.

Façade 3: North

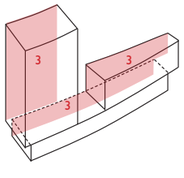
The north façade maintains a similar composition to the south façade with arch openings organised in a linear modular grid, but without obtaining depth in a bid to maximise solar gain. As a consequence of this change in depth, the brick stepped apron at the bottom of the openings is flipped upside down, generating a different visual effect. Slight variations in the composition occur between the west and east block.



West/East Façades



South Façade



North Façade

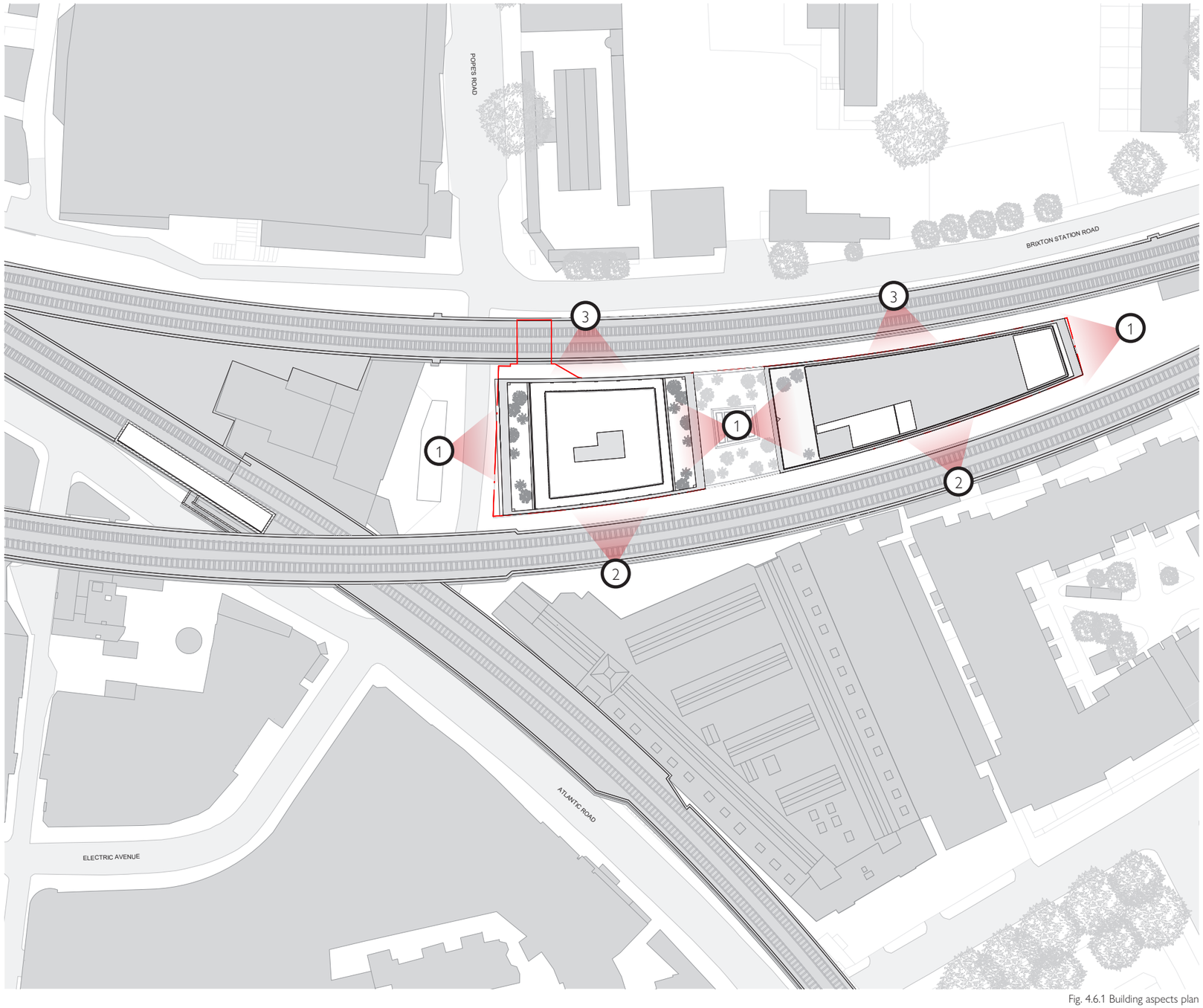


Fig. 4.6.1 Building aspects plan

4.6.2 EAST/WEST FAÇADES

A1 Area

A double height setback at ground and first floor has been employed to create a distinct base, and to mark the entrance to the building. The bracing is also reversed upside down to facilitate circulation and increase verticality.

A2 Area

The reversed bracing continues on the second and third floor suggesting a more thicker base which will be evident in the North/South façades .

B Areas

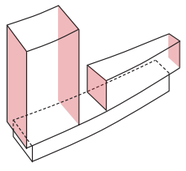
The proposed bracing provides rhythm and modularity to the façade, which spans two floors. Its interface with the glazing defines the middle section of the façade.

C1 Area

The bracing is detached from the glazing as the façade is set back to create double height terraces, which define the upper section of the façade.

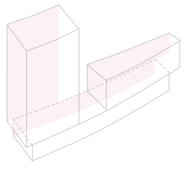
C2 Area

A horizontal spandrel and a setback box on the roof have been introduced, having a different materiality cap to the façade.



West/East Façades

South Façade



North Façade

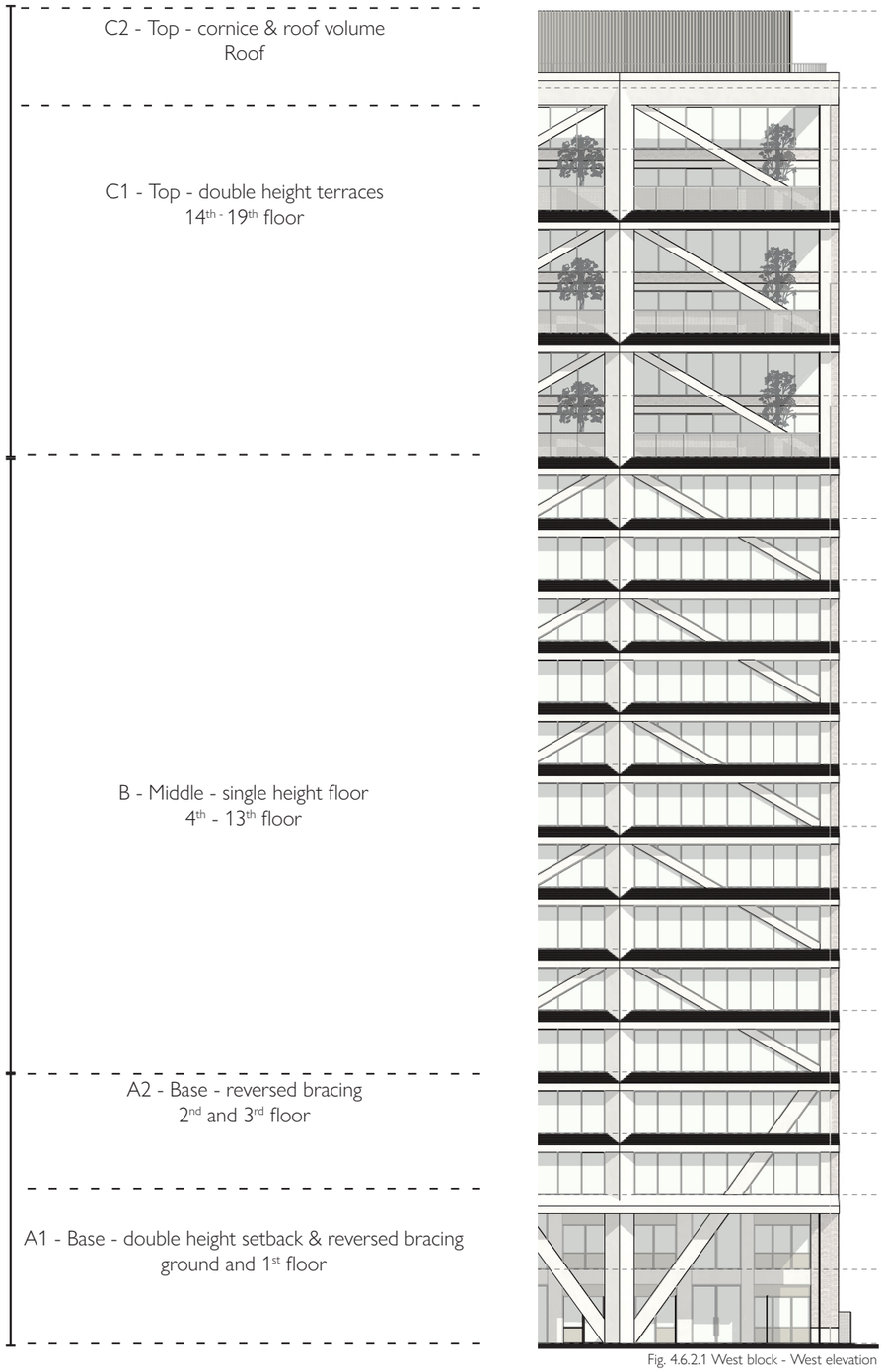


Fig. 4.6.2.1 West block - West elevation

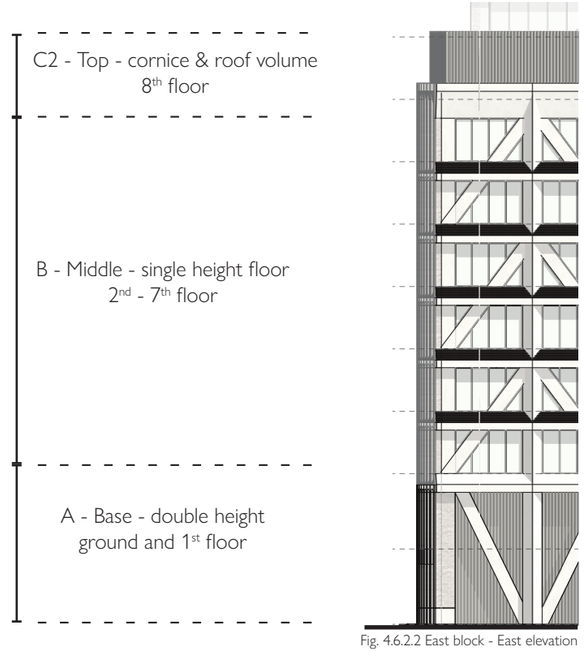


Fig. 4.6.2.2 East block - East elevation

4.6 FAÇADE CONSIDERATIONS

4.6.3 SOUTH FAÇADE - WEST BLOCK

A1 Area

A double height setback at ground and first floor is used to inform the modularity of the façade and create double height brick piers with deep reveals topped with arched lintels.

A2 Area

The second and third floor have single height arched modules with deep reveals that are continuous throughout the two blocks, and that reinforce the base of the building.

B Areas

A single height arched module with a deep reveal defines the middle section of the building. Continuous lintels serve to mark each floor.

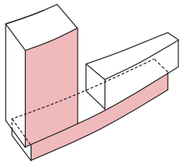
C1 Area

Double height terraces inform the modularity of the façade creating double height brick piers with deep reveals, and interrupting the arched lintels on every other floor.

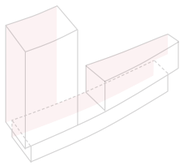
C2 Area

A horizontal spandrel, intricate brickwork, and a setback box on the roof appear with a different materiality cap the façade.

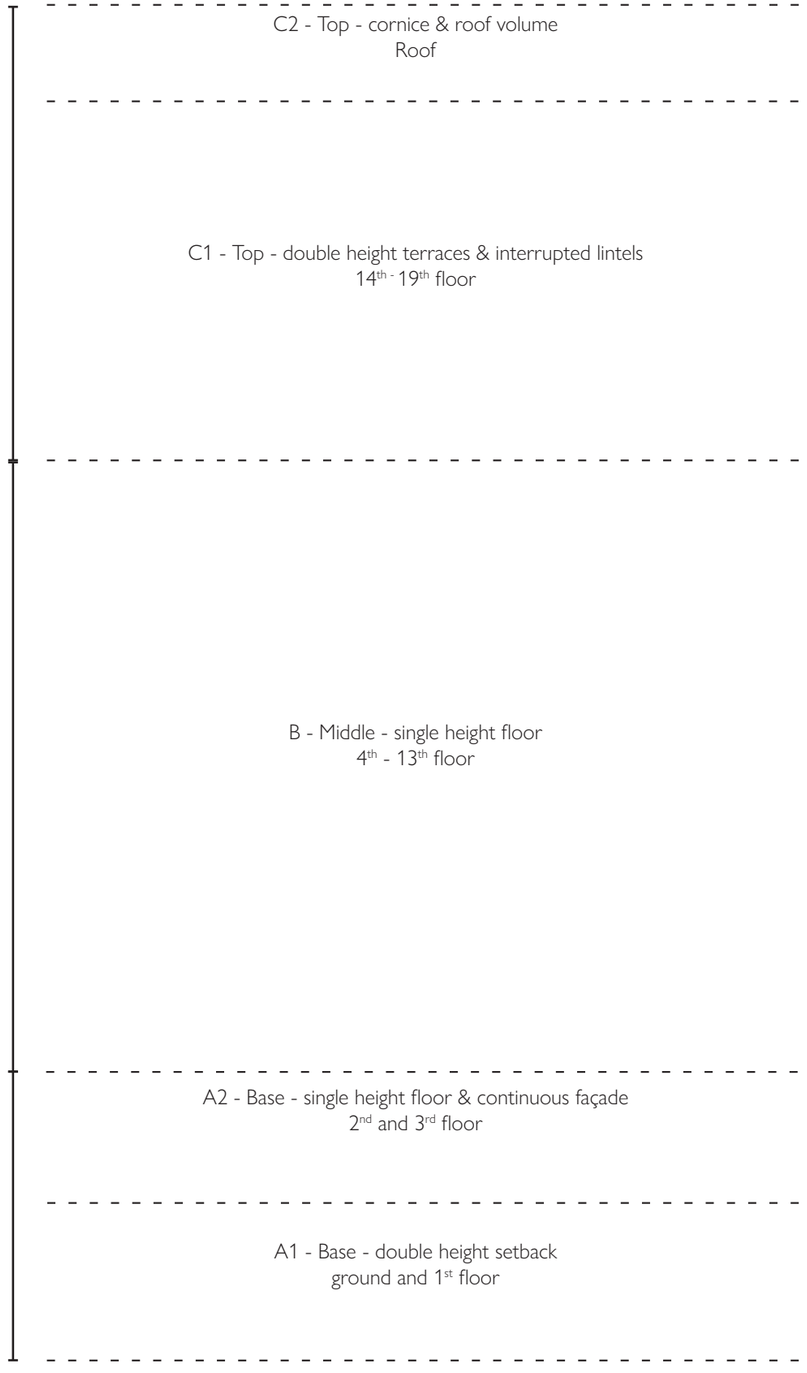
West/East Façades



South Façade



North Façade



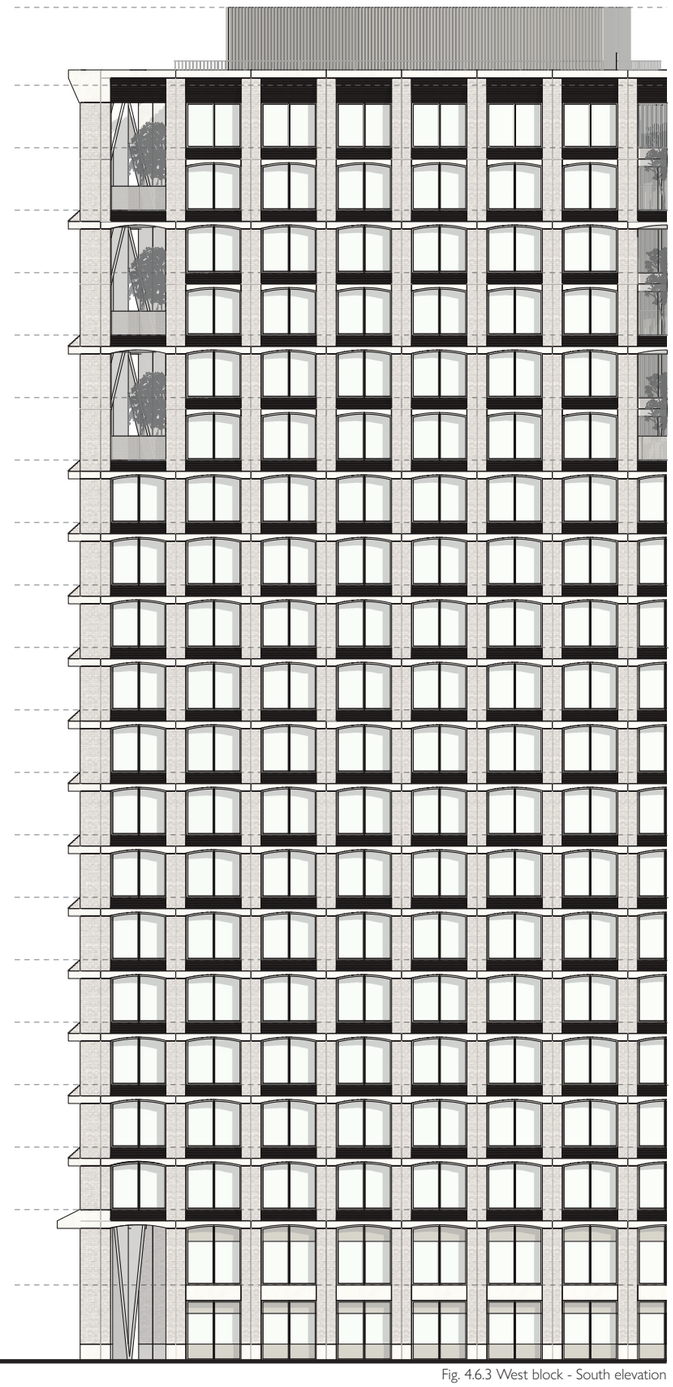


Fig. 4.6.3 West block - South elevation

4.6.4 SOUTH FAÇADE - EAST BLOCK

The composition of the façade on the east block is similar to the west block, with a few variations occurring. Most notably, the module is narrower compared to its west block counterpart.

A1 Area

A double height setback at ground and first floor is used to inform the modularity of the façade, creating double height brick piers with deep reveals topped with arched lintels.

A2 Area

The second and third floor include single height arched modules with deep reveals that are continuous throughout the two blocks, reinforcing the base of the building.

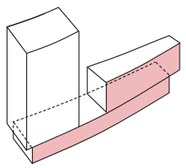
B Areas

A single height arched module with a deep reveal defines the middle section of the building, with continuous lintels marking each floor.

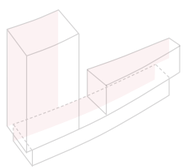
C2 Area

A horizontal spandrel, intricate brickwork, and a setback box on the roof appear with a different materiality cap the façade.

West/East Façades



South Façade



North Façade

C2 - Top - cornice & roof volume
8th floor

B - Middle - single height floor
2nd - 7th floor

A2 - Base - single height floor & continuous façade
2nd and 3rd floor

A1 - Base - double height setback
ground and 1st floor

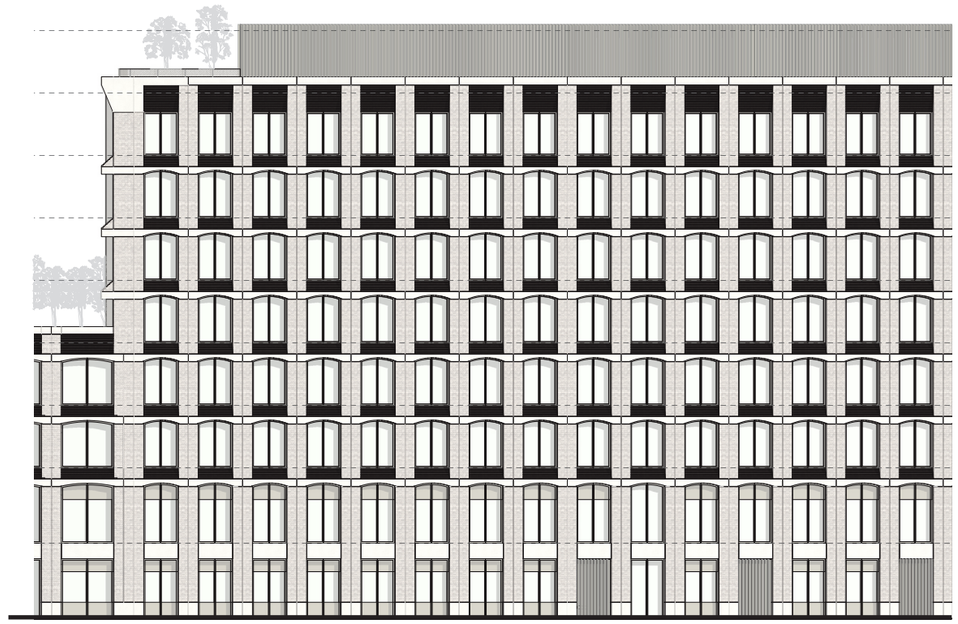


Fig. 4.6.4 East block - South elevation

4.6 FAÇADE CONSIDERATIONS

4.6.5 NORTH FAÇADE - WEST BLOCK

A1 Area

A double height setback at ground and first floor is used to inform the modularity of the façade, creating double height brick piers that are flush with the glazing and topped with arched lintels.

A2 Area

The second and third floor have single height arched modules that are flush with the glazing and continuous throughout the two blocks, reinforcing the base of the building.

B Areas

A single height arched module, flush with the glazing, defines the middle section of the building. Continuous lintels serve to mark each floor.

C1 Area

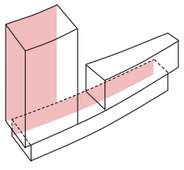
Double height terraces inform the modularity of the façade creating double height brick piers flush with the glazing, and interrupting the arched lintels every other floor.

C2 Area

A horizontal spandrel, intricate brickwork, and a setback box on the roof appear with a different materiality cap the façade.

West/East Façades

South Façade



North Façade

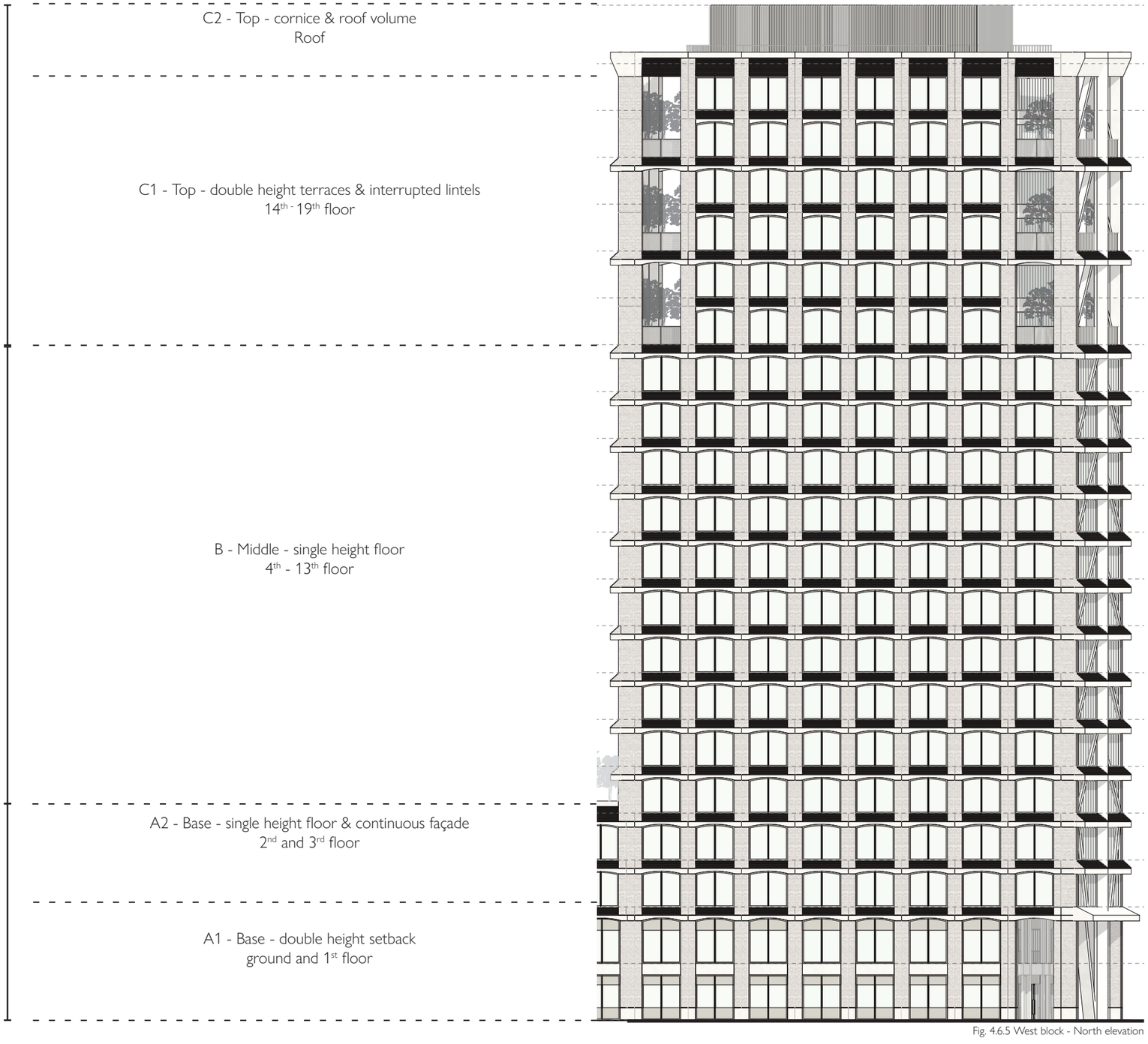


Fig. 4.6.5 West block - North elevation

4.6.6 NORTH FAÇADE - EAST BLOCK

The composition of the façade on the east block is similar to the west block, with a few variations occurring. Most notably, the module is narrower compared to its west block counterpart.

A1 Area

A double height setback at ground and first floor is used to inform the modularity of the façade, creating double height brick piers that are flush with the glazing and topped with arched lintels.

A2 Area

The second and third floor include single height arched modules with deep reveals that are continuous throughout the two blocks, reinforcing the base of the building.

B Areas

A single height arched module with a deep reveal defines the middle section of the building, with continuous lintels marking each floor.

C2 Area

A horizontal spandrel, intricate brickwork, and a setback box on the roof appear with a different materiality cap the façade.

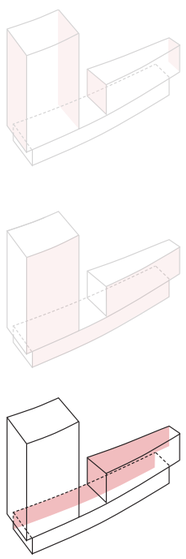


Fig. 4.6.6 East block - North elevation

4.7 EXTERNAL APPEARANCE

Our façade design takes into consideration a number of elements, such as programme and brief, as well as site constraints, contextual architectural influences and environmental performance.

The result is a façade design that adapts and reflects its context and reacts differently depending on its orientation. Moreover, the building consistently presents a unique façade on each of its four elevations, generating a continuous changing appearance from street level surrounding the site.

Two main types of façade have been created respectively on the east/west and north/south as an immediate reaction to the shape and conditions of the site.

The façade gives a contemporary reinterpretation of a number of architectural features present in the surrounding built context, creating a design and dialogue which is at once innovative and sympathetic to its context.

Fig. 4.7 View of the south/west façade from Electric Avenue

4.7.1 WEST FAÇADE - POPE'S ROAD

The façade on Pope's Road is effectively the only street-facing façade within the scheme. The building exhibits a tripartite articulation that relates directly to the surrounding façades, each exhibiting an active and porous base, repetitive and modular middle section, and an articulated top.

The front elevation is articulated through the combination of deep concrete linear trays topped by a stepped brick apron, a central triangular concrete pier, and concrete structural bracing.

A double height setback at ground and first floor, in conjunction with a reversed bracing spanning four floors, create a solid plinth.

The building's middle section is composed of single height floors with the structural bracing spanning two floors. The upper floors are characterised by double height terraces that expose the structural bracing, as well as a thicker horizontal spandrel that caps the façade.

A metal clad box, positioned on the roof and setback from the façade, completes the elevation, relating it back to articulated rooftop of the neighbouring buildings.

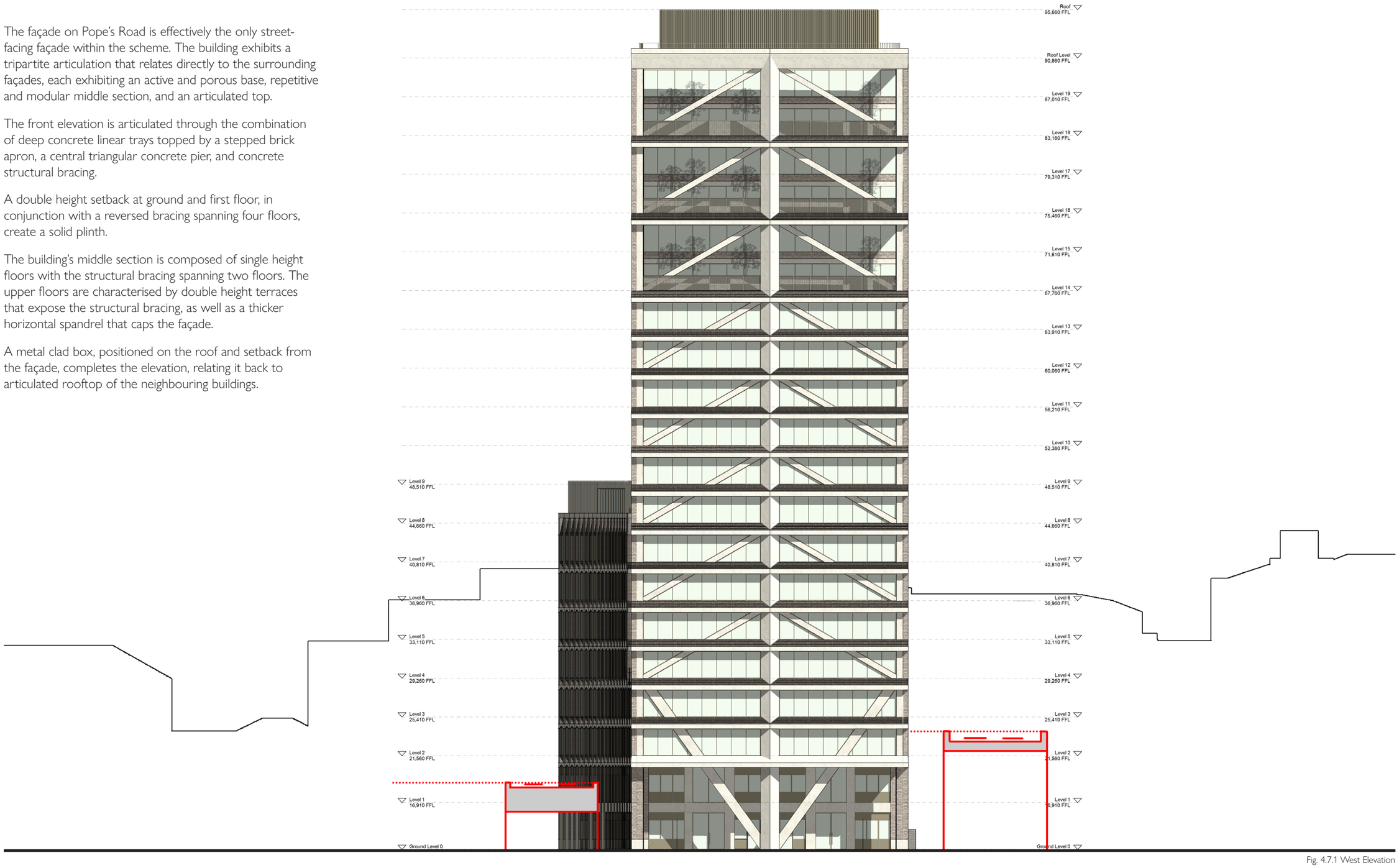


Fig. 4.7.1 West Elevation

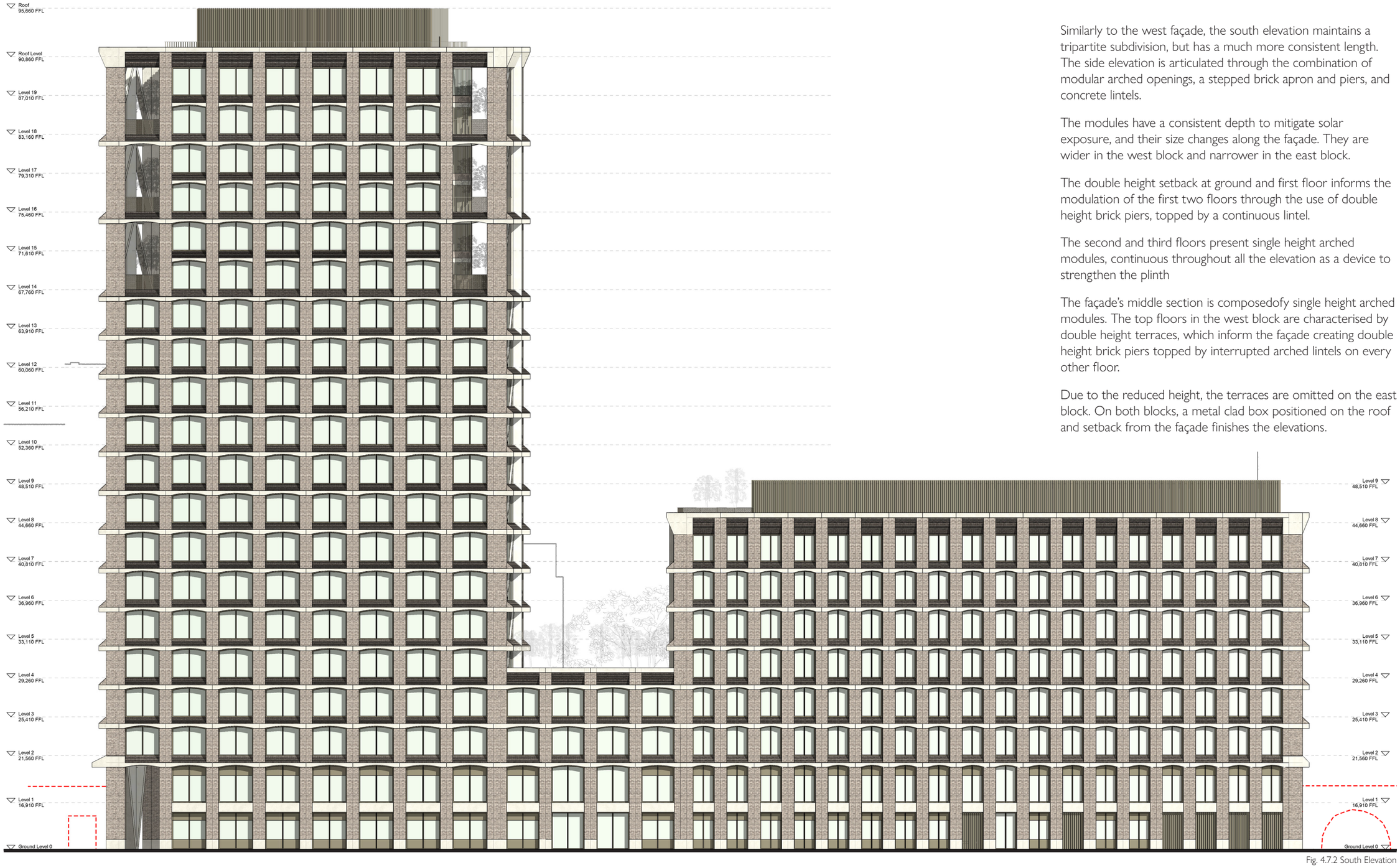


Fig. 4.7.2 South Elevation

4.7.2 SOUTH FAÇADE

Similarly to the west façade, the south elevation maintains a tripartite subdivision, but has a much more consistent length. The side elevation is articulated through the combination of modular arched openings, a stepped brick apron and piers, and concrete lintels.

The modules have a consistent depth to mitigate solar exposure, and their size changes along the façade. They are wider in the west block and narrower in the east block.

The double height setback at ground and first floor informs the modulation of the first two floors through the use of double height brick piers, topped by a continuous lintel.

The second and third floors present single height arched modules, continuous throughout all the elevation as a device to strengthen the plinth

The façade's middle section is composedofy single height arched modules. The top floors in the west block are characterised by double height terraces, which inform the façade creating double height brick piers topped by interrupted arched lintels on every other floor.

Due to the reduced height, the terraces are omitted on the east block. On both blocks, a metal clad box positioned on the roof and setback from the façade finishes the elevations.

4.7.3 NORTH FAÇADE

The north façade follows the same principles of the south façade but differ in the module articulation. The size of the modules is the same on the west and east block but the façade doesn't have depth, the brick piers, concrete lintels and the glazing are flushed to maximize the solar gain.

As a consequence the stepped brick apron is flipped upsidedown emphasizing the concrete lintels and also becoming more evident from street level.

The top metal volume on the roof of the east block present a very long and horizontal glazed opening that allows views on the City of London and reveals the presence of a different type of program and space inside.

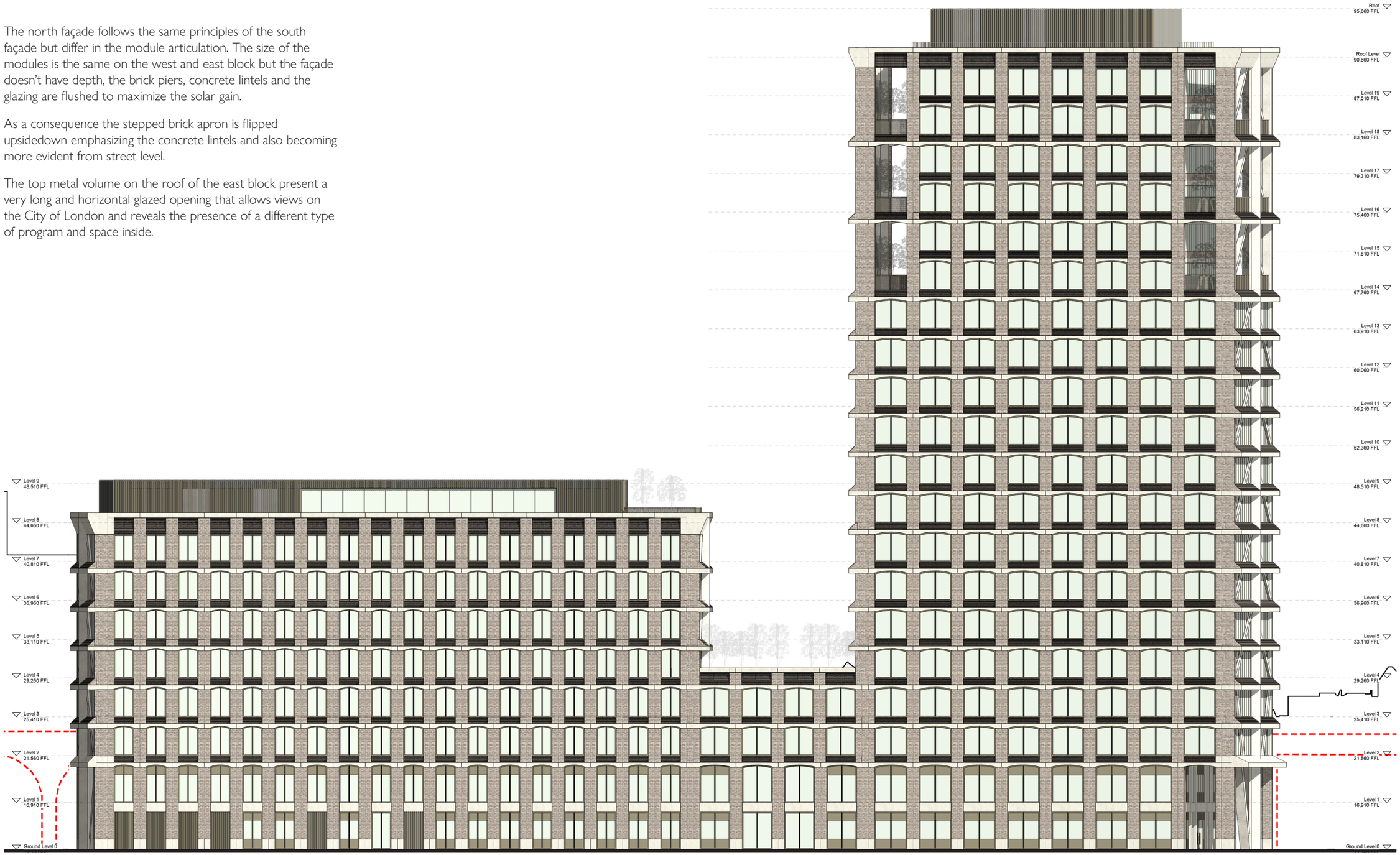


Fig. 4.7.3 North Elevation

4.7.4 EAST FAÇADE - VALENTIA PLACE

The east façade facing Valentia Place maintains a similar treatment to that of Pope's Road. It presents an inverted bracing that spans the first two floors, which are also clad in metal – mirroring the finish of the box on the roof.

In addition, due to the reduced height of the east block, the double height terraces are also omitted and the elevation is capped in the same way as its counterpart.

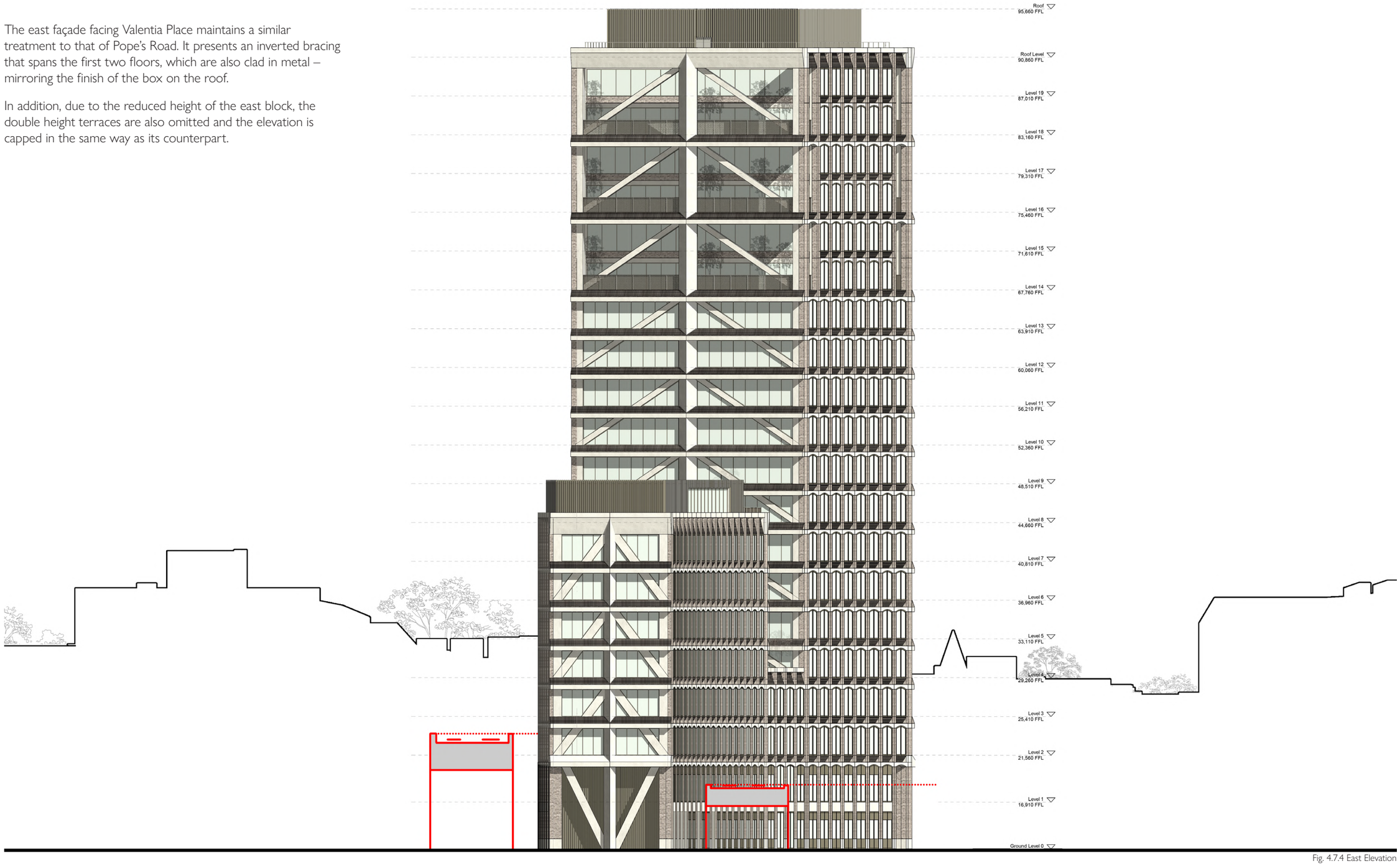


Fig. 4.7.4 East Elevation

4.7.5 INTERNAL FAÇADES

The scheme's two internal façades occupy the east face of the west block and the west face of the east block.

These façades are articulated in the same way as their equivalent external façades, depending within which block they are located.

Both internal façades overlook a garden created on top of the third floor, which is the last floor continuous through both blocks

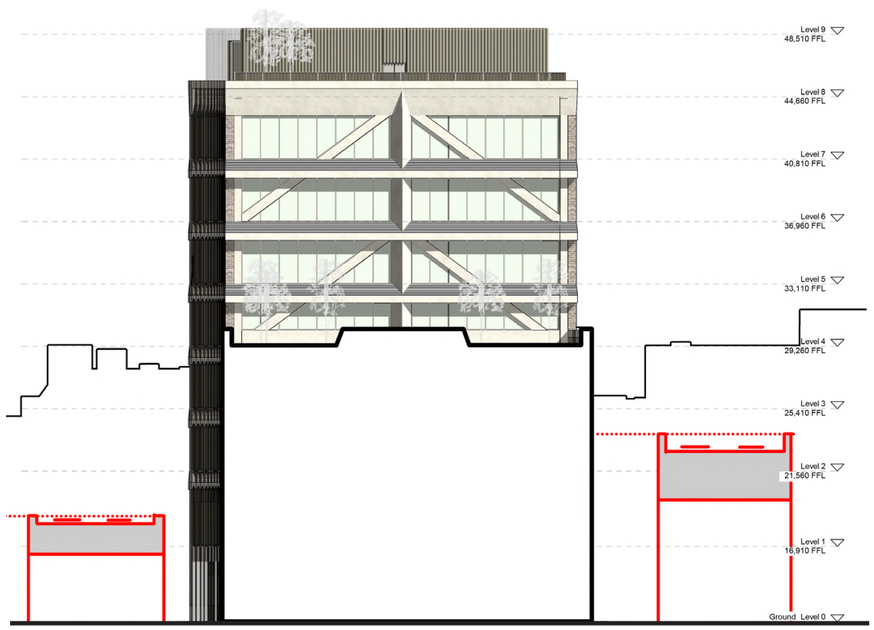


Fig. 4.7.5.2 East Block West Elevation

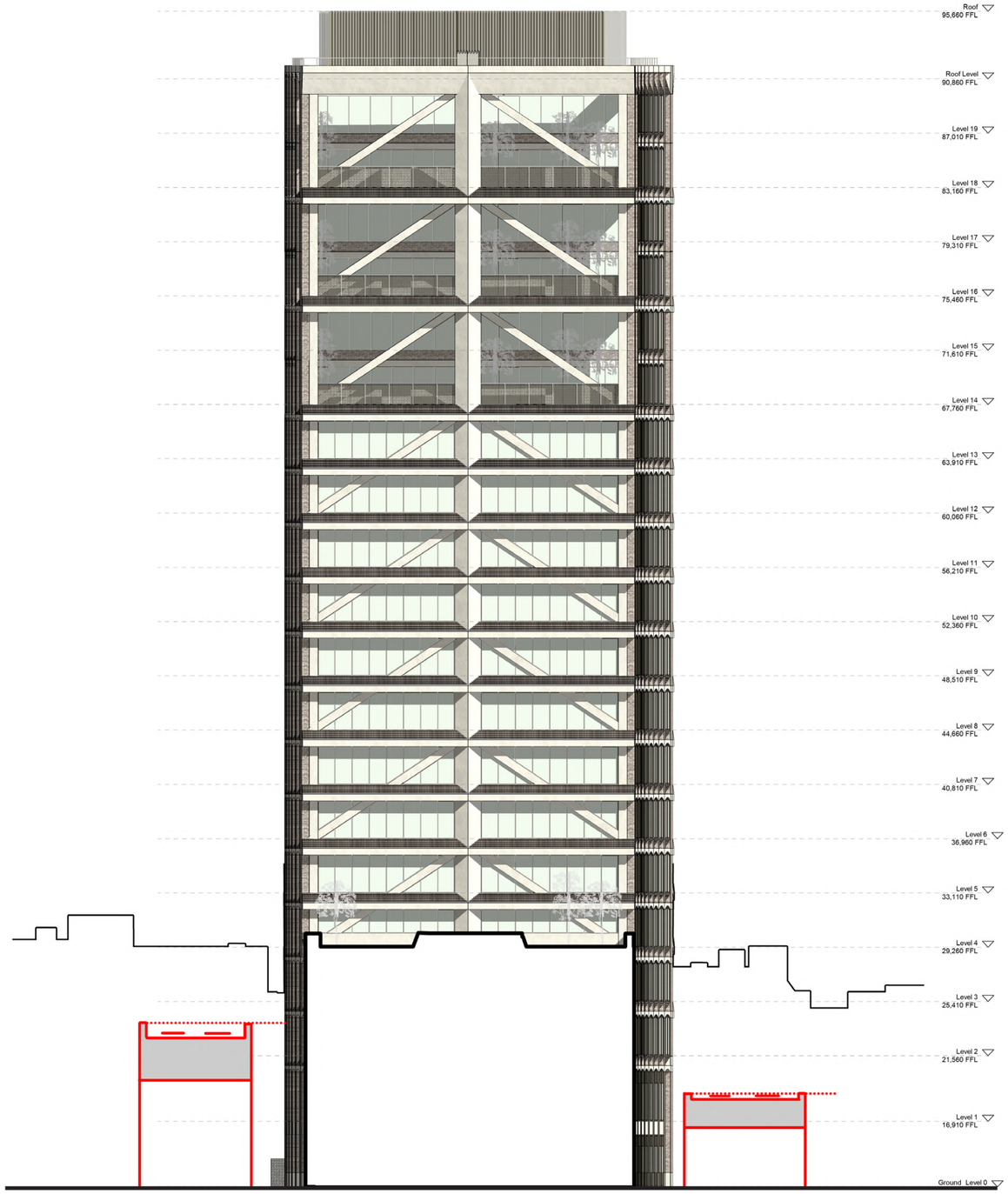


Fig. 4.7.5.1 West Block East Elevation

4.8 TYPICAL FAÇADE MODULATION

4.8.1 WEST BLOCK - WEST FAÇADE

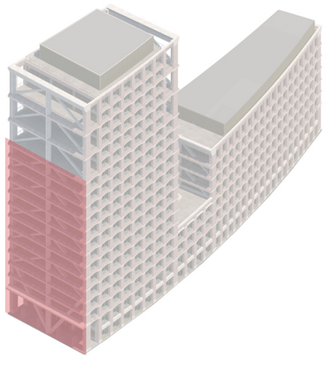
Bottom - Ground to 3rd Floor

Being the main façade, at ground level its most remarkable aspect is that it has been recessed to allow for extended public space creating a canopy that invites visitors into the building and also a north to south pedestrian flow.

The inverted structural truss facilitates circulation into the market and is combined with a deep concrete tray, which serves as a wind mitigation element and clearly marks the bottom part of the building.

Middle - 4th to 13th Floor

The middle portion is characterised by the rhythm of the truss which spans two floors in conjunction with the linear appearance of the concrete and brick trays that mark every floor.



Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

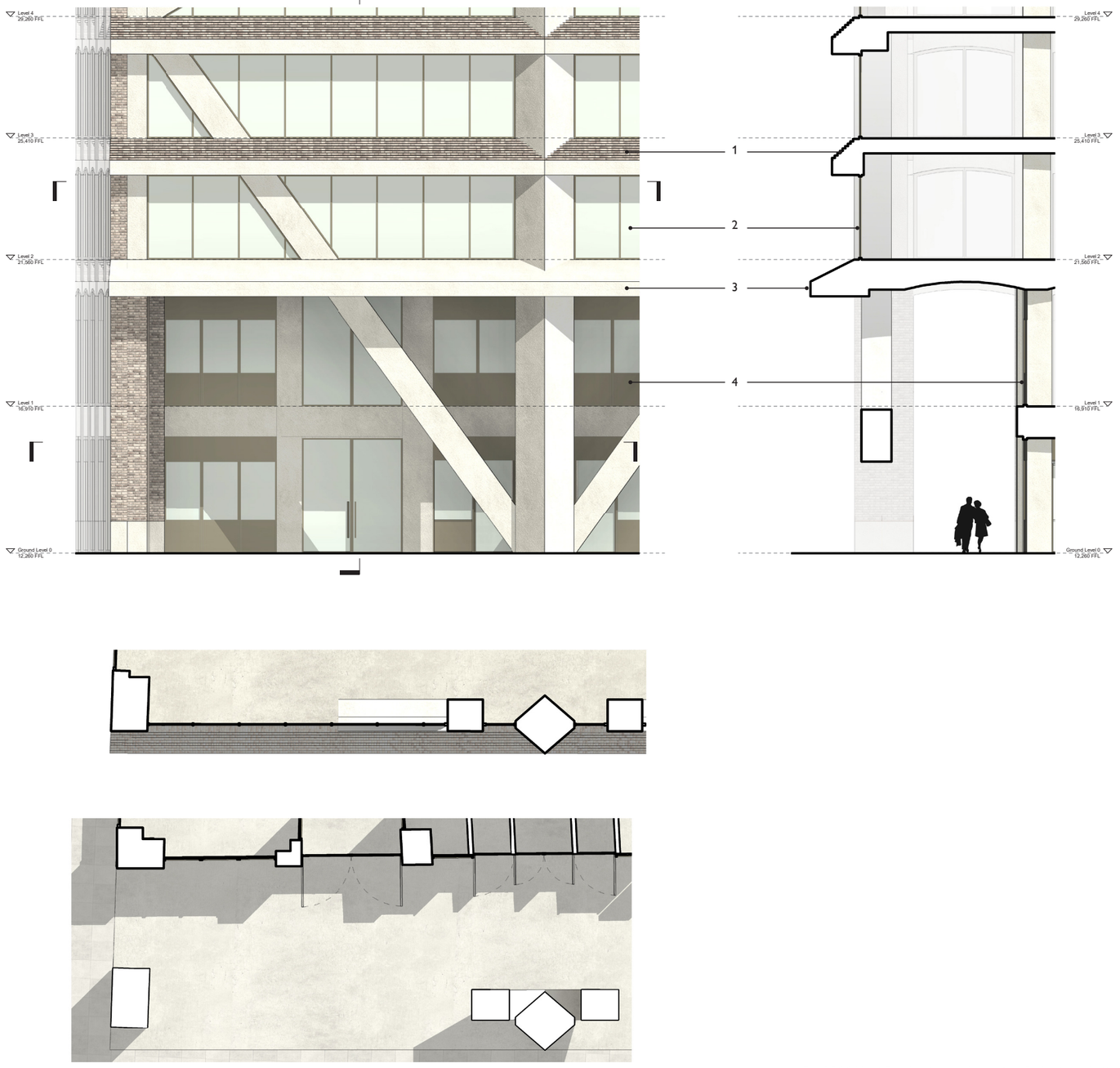


Fig. 4.8.1.1 West Façade - typical plan, section & elevation - bottom & middle

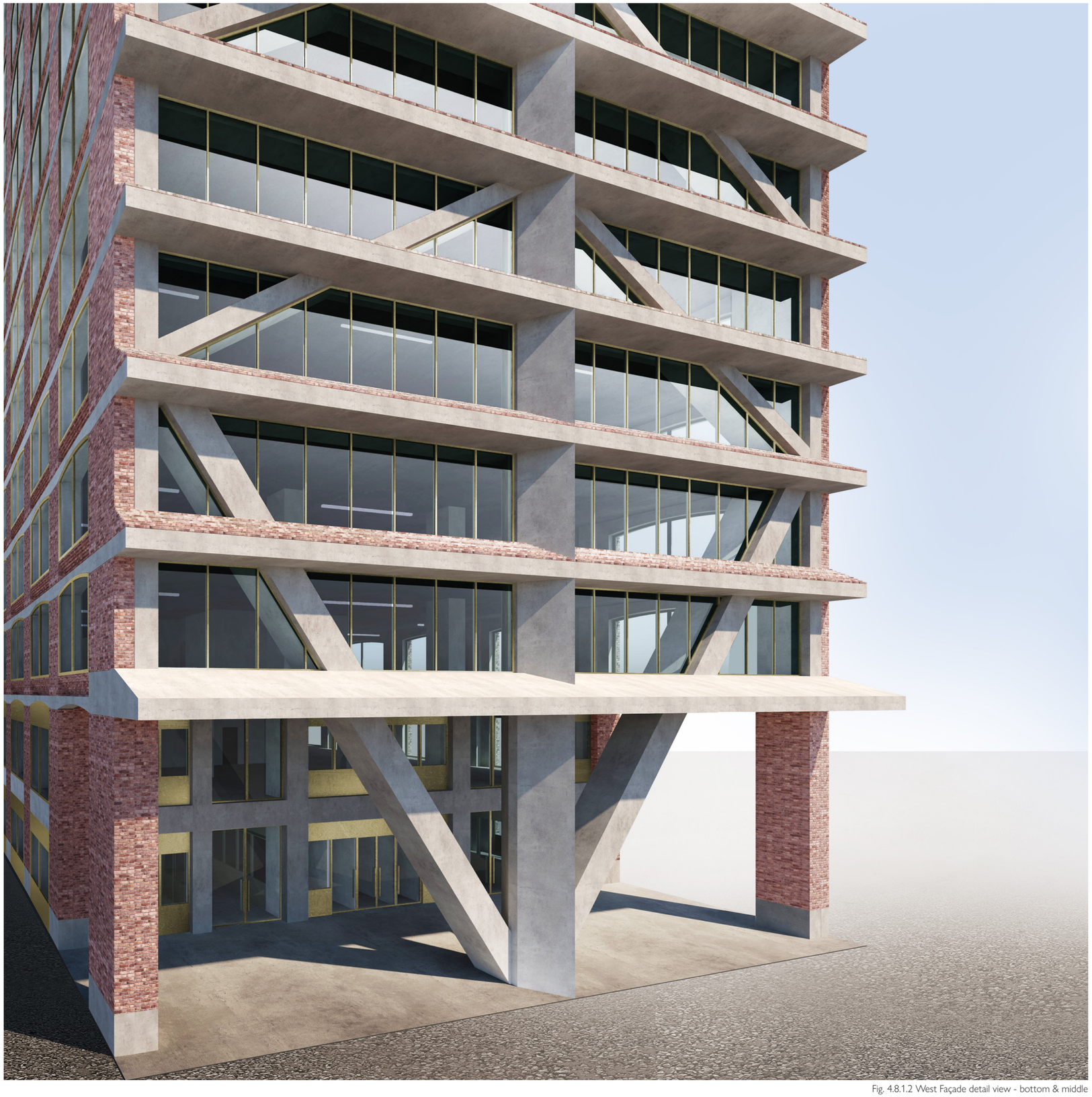


Fig. 4.8.1.2 West Façade detail view - bottom & middle

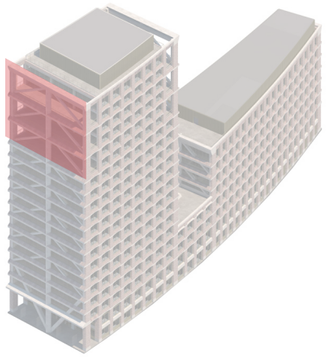
4.8 TYPICAL FAÇADE MODULATION

4.8.1 WEST BLOCK - WEST FAÇADE

Top - 14th to 19th Floor

The top of the building on the west façade is clearly marked by the introduction of double height terraces. In these terraces the glazing is recessed one bay east, a move that exposes the structural bracing and allow for the growth of vegetation on the terraces every other floor.

The last arbitrable floor is marked by a concrete horizontal spandrel which caps the bracing and the vertical concrete pier. The roof is recognizable by the introduction of a clad metal box that contains plant elements.



Material Key:

1 Brick

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5 Bronze coloured balustrade with solid wind screen

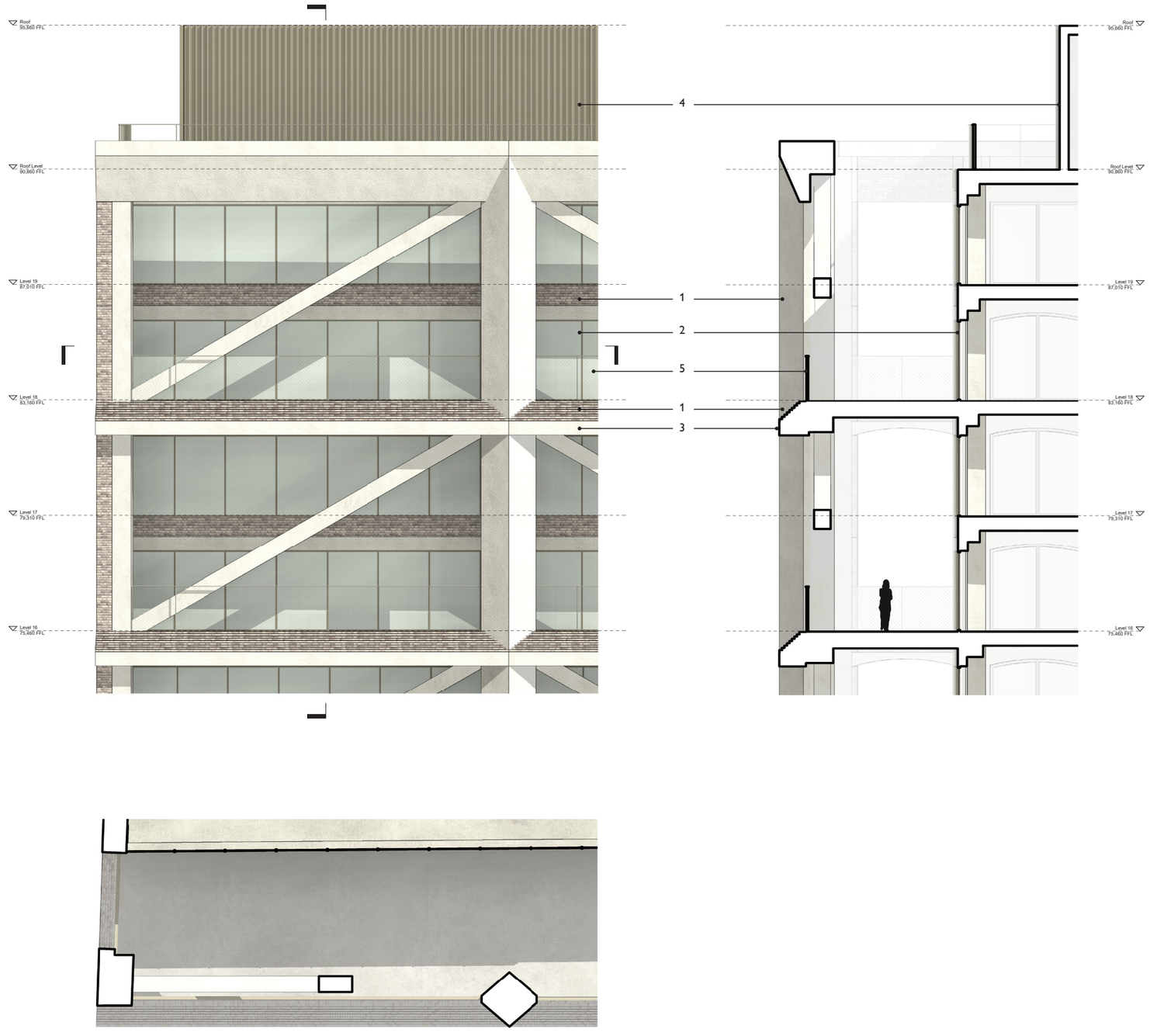


Fig. 4.8.1.3 West Façade - typical plan, section & elevation - top

Fig. 4.8.1.4 West Façade detail view - top

4.8 TYPICAL FAÇADE MODULATION

4.8.2 WEST BLOCK - SOUTH FAÇADE

Bottom - Ground to 3rd Floor

The setback on the west façade occupies one module of the south façade and informs the modulation of the first two floors.

Double height brick piers anchored to the ground by concrete pedestals and topped by a concrete arched lintels frame the first two floors that contain the market whilst single height brick piers with concrete arched lintels that runs for the whole length of the façade on the 3rd and 4th floor conclude the plinth of the building.

Middle - 4th to 13th Floor

A more rhythmic and repetitive façade constitutes the middle portion of the building. The single height piers with continuous concrete arched lintels are expressed in all their strength.

The stepped brick apron and the depth of the brick piers gives depth to the façade reacting passively to the solar exposure. Where the south façade meets the west in the corner the stepped brick apron and the concrete lintel creates a silhouette that defines the profile of the building.

Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

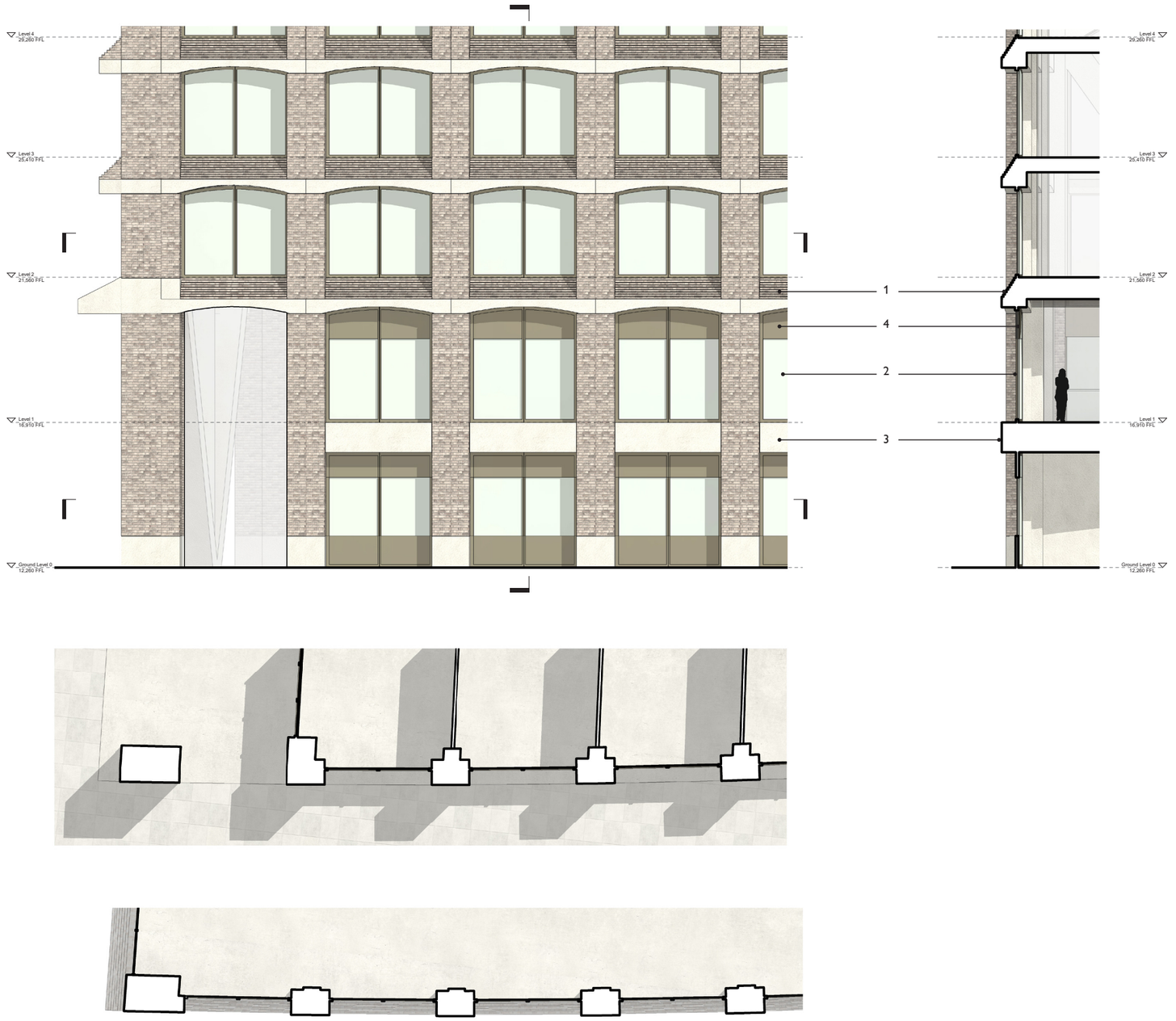


Fig. 4.8.2.1 West Façade - typical plan, section & elevation - bottom & middle

Fig. 4.8.2.2 West Façade detail view - bottom & middle

4.8 TYPICAL FAÇADE MODULATION

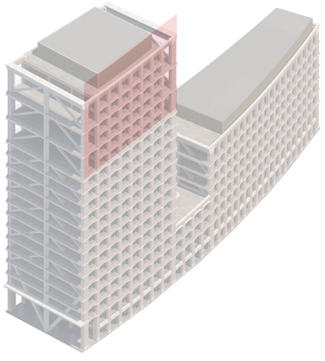
4.8.2 WEST BLOCK - SOUTH FAÇADE

Top - 14th to 19th Floor

The top of the building on the south façade is informed by the introduction of double height terraces on the west façade. These terraces create a double height void which occupy the first module on both the east and west side of the west block.

In addition, the double height terraces introduce a variation in the modulation of the façade , the brick piers start to span two floors and interrupt the concrete lintels every other floor increasing the verticality of the façade .

A thicker, linear and inverted stepped brick apron capped by a concrete spandrel marks the last arbitrable floor and conclude the façade .The metal clad box on the roof is setback from the façade but still visible.



Material Key:

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3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

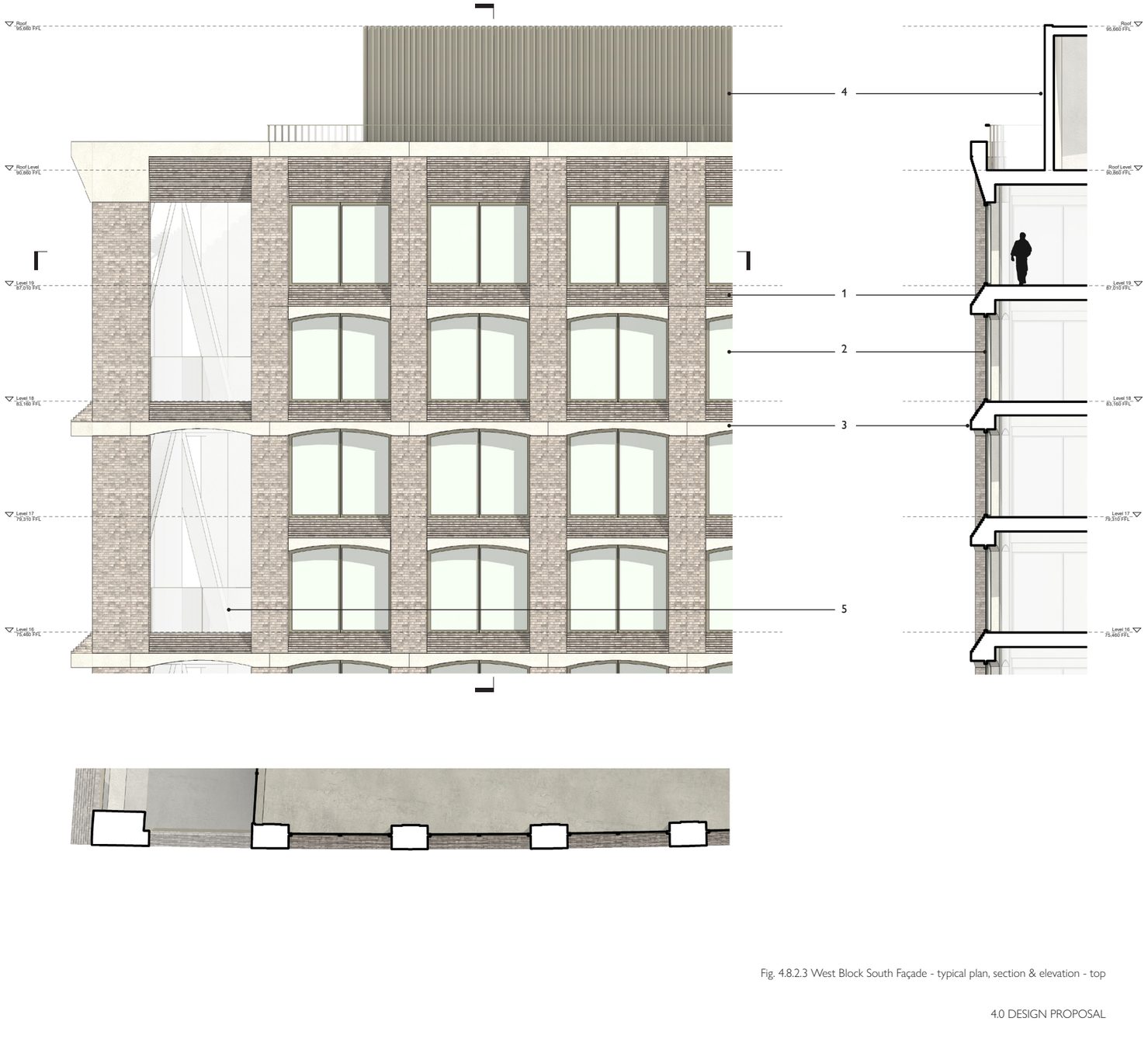


Fig. 4.8.2.3 West Block South Façade - typical plan, section & elevation - top

Fig. 4.8.2.4 West Block South Façade detail view - top

4.8 TYPICAL FAÇADE MODULATION

4.8.3 WEST BLOCK - NORTH FAÇADE

Bottom - Ground to 3rd Floor

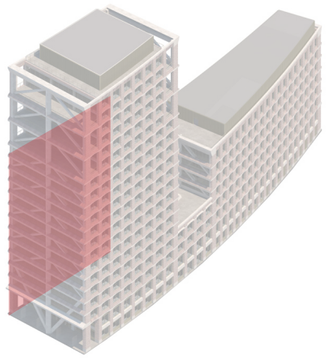
The organisation of the bottom north façade is the same as applied to the bottom south façade, however there is one substantial difference between the two façades.

The glazing is flushed with the brick in order to maximize the solar gain, a solution that eliminates the whole depth of the façade making it almost like a continuous flat sheet that provides a different experience compared to the south side.

Middle - 4th to 13th Floor

The principle of having a flat façade seen in the bottom is applied to the middle portion of the façade as well. As for the bottom the organisation and modulation of the north façade is the same as the south façade. The glazing however is flushed with the brick in order to minimize shading and allow more light into the building considering that on the north façade there is no direct sun exposure.

This architectural solution allows for a variation on the stepped brick apron which is flipped upside down to align with the glazing at the top. In this way the apron is more visible form street level and creates a different relationship with the concrete lintel and a different type of shadows compared to the south façade.



Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

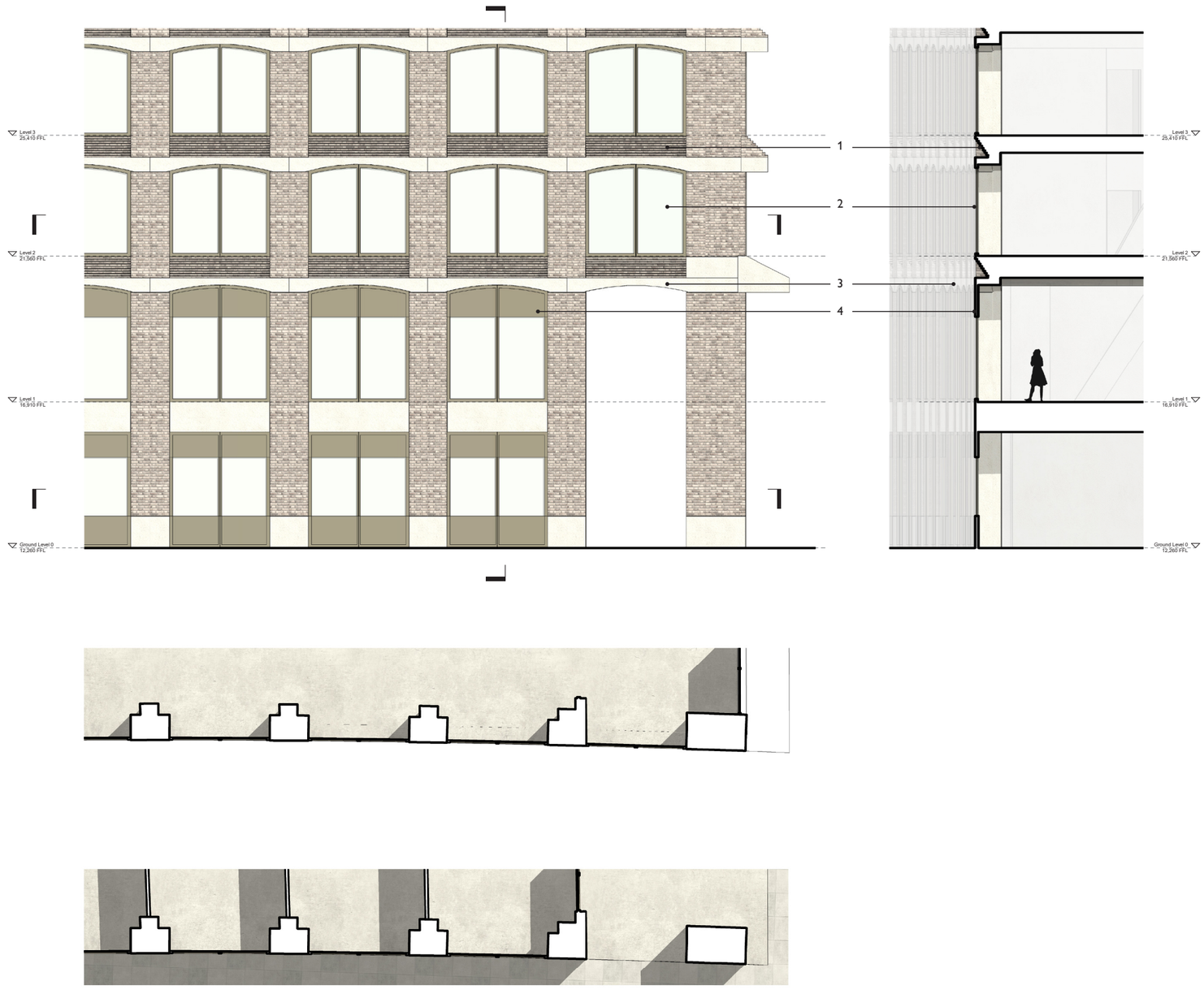


Fig. 4.8.3.1 West Block North Façade - typical plan, section & elevation - bottom & middle

Fig. 4.8.3.2 West Block North Façade detail view - bottom & middle

4.8 TYPICAL FAÇADE MODULATION

4.8.3 WEST BLOCK - NORTH FAÇADE

Top - 14th to 19th Floor:

As for the bottom and the middle portion, the top part of the north façade reflects the same organisation of the south façade. The new architectural quality introduced in the lower portion is maintained here.

As for the south façade, the last module of the top arbitrable floor is a thicker linear stepped brick apron, which is tilted upside down compared to others in the façade, creating a different condition with the concrete spandrel at the top.

The façade is concluded by the metal clad box on the roof.

Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

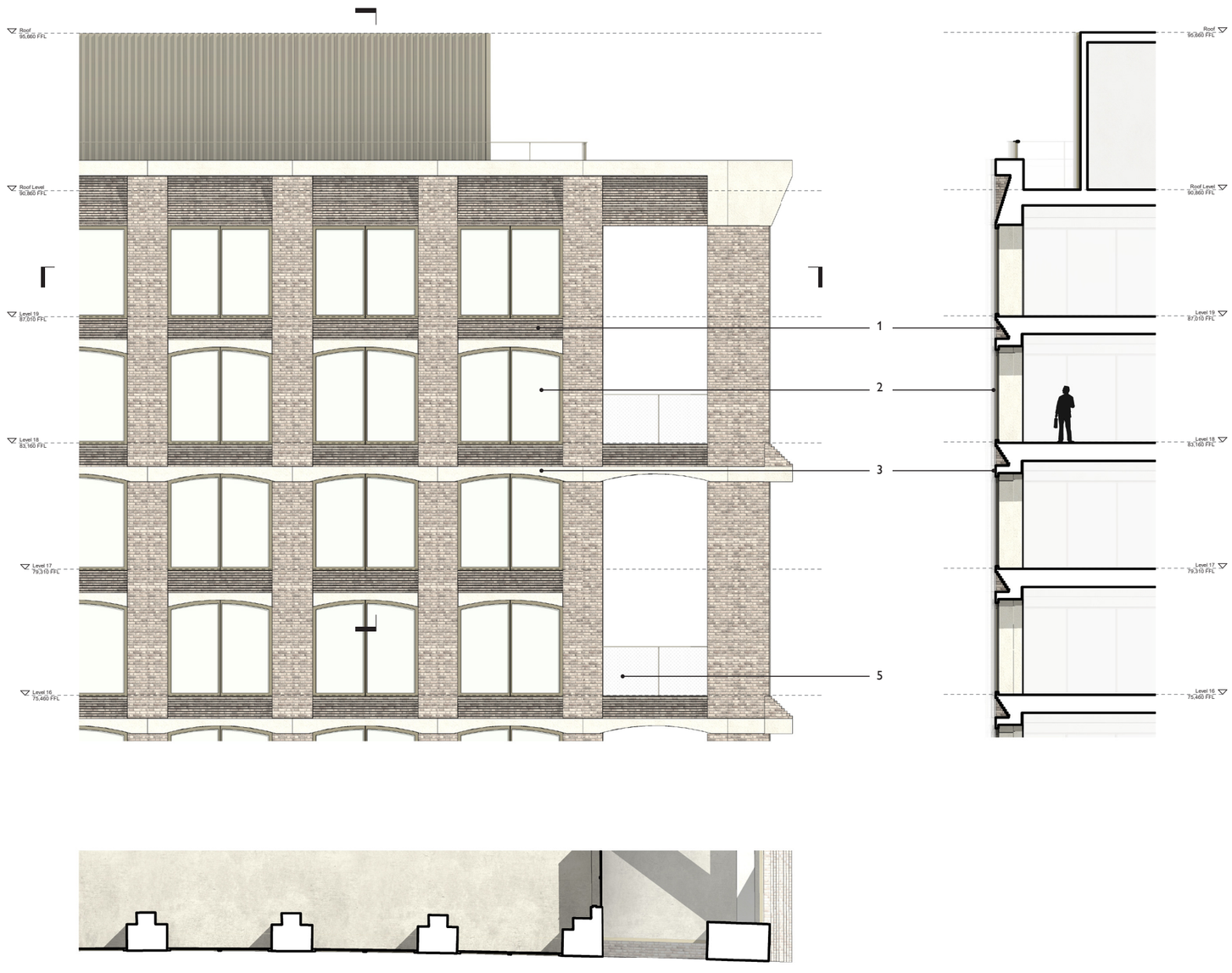


Fig. 4.8.3.3 West Block North Façade - typical plan, section & elevation - top

Fig. 4.8.3.4 West Block North Façade detail view - top

4.8 TYPICAL FAÇADE MODULATION

4.8.4 EAST BLOCK - SOUTH FAÇADE

Bottom - Ground to 3rd Floor

The bottom part of the south façade of the east block is organized and composed in the same way as the one of the west block.

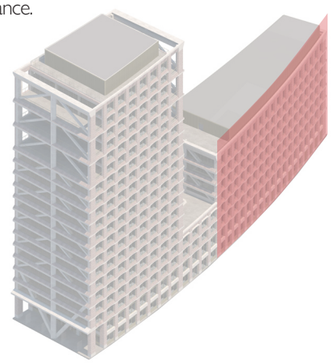
The main difference consists in the size of the module which is smaller compared to the one on the west block. A smaller module absorbs and emphasises the curvilinear shape of the site which is more marked in the eastern part.

Middle - 4th to 7th Floor

As for the bottom part, this portion of the façade is also structured in the same way of the west block. The smaller module also emphasised by the arched framed shape of the concrete lintel which appear more marked even if it maintains the same proportion of the one on the west block.

Top - 8th Floor

The reduced height of the east block informs a different treatment of the top part of the façade, which omits the double height terraces. The west block is a thicker, linear stepped brick apron capped with a concrete spandrel finishes the façade. The metal clad box on the roof adapts its proportion to the east block assuming a more horizontal appearance.



Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

Fig. 4.8.4.1 East Block South Façade - typical plan, section & elevation - bottom, middle & top

Fig. 4.8.4.2 East Block South Façade detail view - bottom, middle & top

4.8 TYPICAL FAÇADE MODULATION

4.8.5 EAST BLOCK - NORTH FAÇADE

Bottom - Ground to 3rd Floor

The bottom part of the north façade of the east block is organized and composed is the same way as the one of the west block.

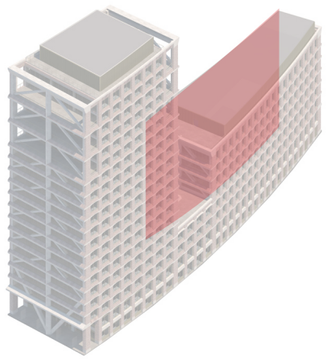
Again, the only difference consists in the size of the module which is smaller compared to the one on the west block. A smaller module along with the glazing flushed to the brick emphasises the concave movement generated by the shape of the site.

Middle - 4th to 7th Floor

Similarly to the bottom part, this portion of the façade is also structured in the same way of the west block. The smaller module also emphasize the arched shape of the concrete lintel which appear more marked even if it maintains the same proportion of the one on the west block.

Top - 8th Floor

As for the west block, a thicker, a linear stepped brick apron capped with a concrete spandrel finishes the façade. The metal clad box on the roof, with its horizontal proportion, displays a new feature not visible anywhere else which is a long horizontal glazed opening that manifests the presence of the restaurant inside.



Material Key:

1 Brick

2 Glazing unit in bronze coloured frame

3 Concrete

4 Bronze coloured balustrade/metal work

5 Bronze coloured balustrade with solid wind screen

Fig. 4.8.5.1 East Block South Façade - typical plan, section & elevation - bottom, middle & top

Fig. 4.8.5.2 East Block South Façade detail view - bottom, middle & top

4.8 TYPICAL FAÇADE MODULATION

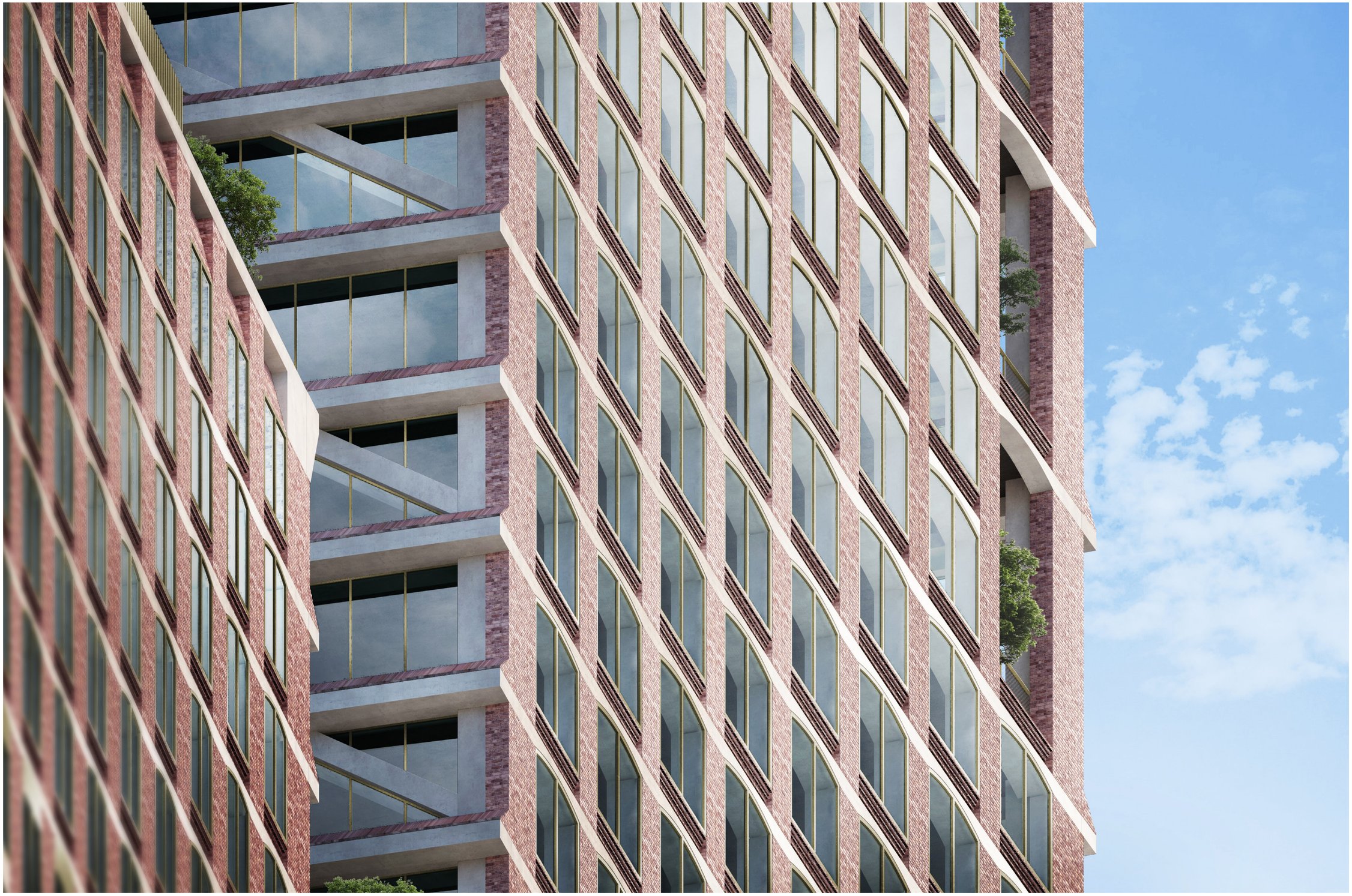


Fig. 4.8.5.3 North Façade detail view

Fig. 4.8.5.4 South Façade detail view

4.9 MATERIALITY

The material palette used throughout the scheme seeks to reinforce the building's relationship with its surroundings. With many of the contextual buildings, the majority of façades are clad with brick, providing a sense of solidity and permanence.

Continuous concrete lintels enrich the façades, introducing rhythm and articulation, whilst the inclusion of gentle arches bring movement and variety to the modular organisation.

These lintels are directly inspired by the window heads present on many of the area's contextual. Additionally, bronze coloured metalwork is proposed for window frames and balustrades.

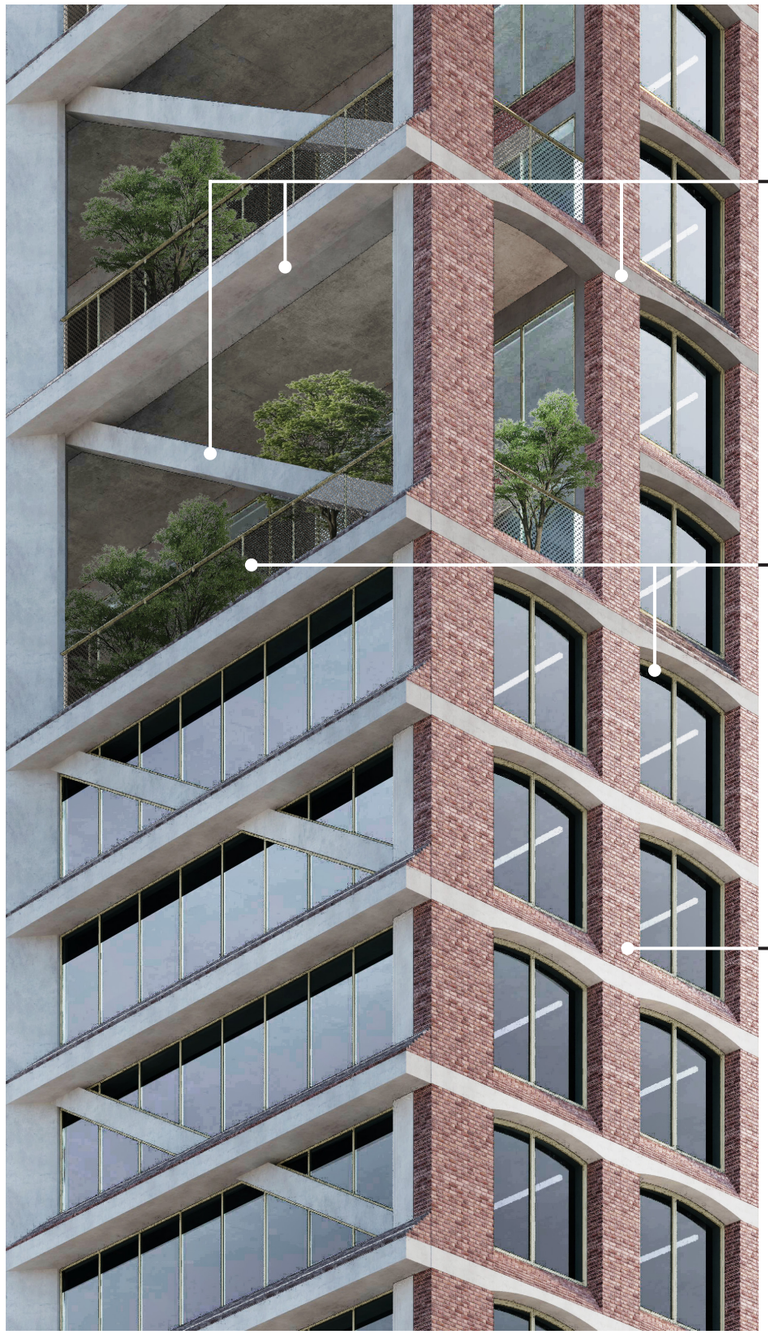
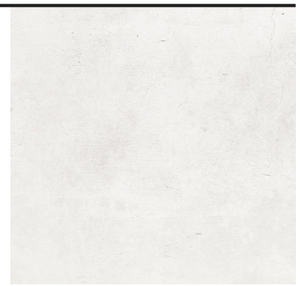


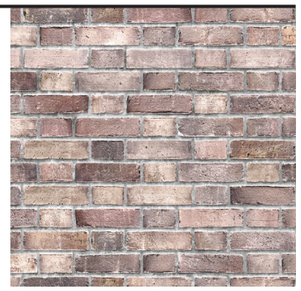
Fig. 4.9 South - West Façade detail view

CONCRETE - SOFT FINISH
Window lintel; Soffits; Bracing



BRASS FINISH METALWORK
Window frames; Railings

BRICK
Outer façade



4.10 DETAILING

When experienced from proximity, the building reveals a refined and rich level of detailing. Several architectural details prevalent in the surrounding built environment were used as starting point for the design of the proposed façades .

Window heads, continuous lintels, intricate brickwork and industrial façades with large openings were each reinterpreted in a contemporary design that combines context with innovation.

In detailing the façades, materials often come together to provide a unique combination of colours and textures. Variation in the articulation of the modules, in combination with the use of different depths, generates plays of light and shadow that emphasize the diverse detailing across the scheme.

Fig. 4.10.1 Window on Electric Avenue

Fig. 4.10.2 Prince of Wales building façade detail

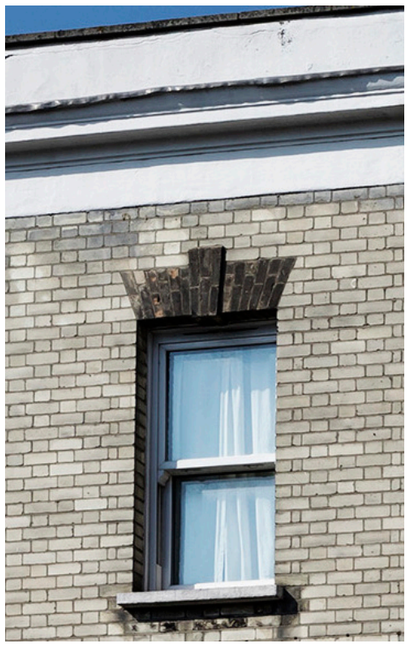


Fig. 4.10.3 Window on Vining Street

Fig. 4.10.4 West Block South Façade detail view

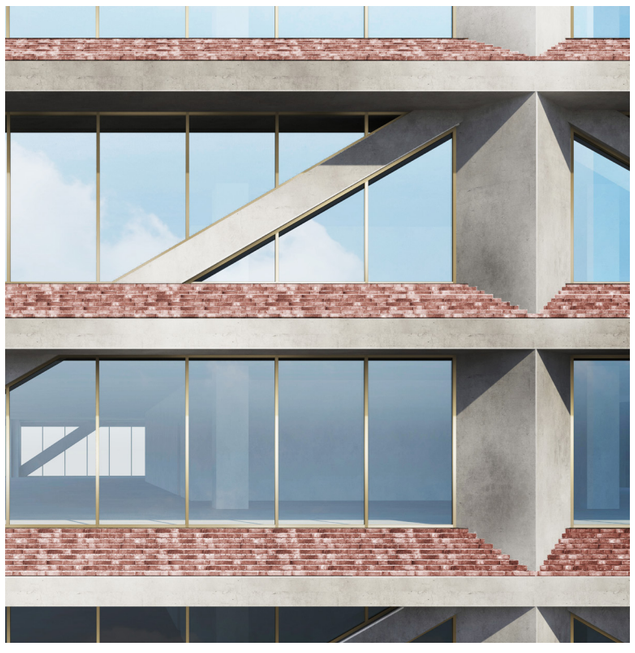


Fig. 4.10.5 West Façade detail view

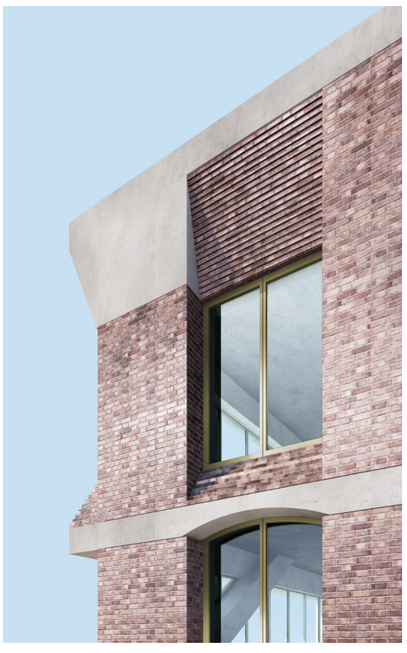


Fig. 4.10.6 East Block South Façade detail view

4.11 SIGNAGE

The façade at ground a has been designed to accommodate appropriate signage to each retail unit. Every module has a metal band with an upper component that can be customised by each unit tenant.

This strategy has been used for the inside façade of the units, providing a higher degree of visibility. In the event of retail areas being occupied by more than one client, this feature will allow for clear differentiation between each of the entrances.

The allocation of a specific area for signage on the façade allows easy maintenance or replacement when the tenancy of the retail spaces changes.

Fig. 4.11 Internal Market Façade indicative signage diagram

Proposed Internal Signage

4.12 ENERGY & SUSTAINABILITY

The project took an integrated approach to sustainability, considering all aspects of design as well as social and economic positive impacts.

- Heat recovery
- Heat recovery incorporated in air handling units
- Water efficiency
- Target to achieve at least 40% improvement over BREEAM water benchmark through combination of efficient fittings and leak control
- Efficient electric MEP
- All-electric MEP solution to minimize impact on air quality. Lower carbon emissions taking advantage of grid decarbonisation
- Life cycle carbon
- Construction products selected depending on their environmental impact and consumption over the life-cycle of the building
- BREEAM - Aim to achieve Excellent
- Comfortable outdoor spaces
- Microclimate created to enhance outdoor comfort.
- Semi-outdoor spaces to provide comfortable alternative in peak seasons
- Façade optimization
- Façade optimised for balanced daylight and energy demand. Depth used strategically across the façades to provide shading during summer and lights during winter. Glazing ratios and glass properties selected to maximize daylight and balance cooling and heating demand
- Strategic ecology
- Species of recognised wildlife value used in strategic landscaped areas. Aim to maximize access to nature, urban greening and public realm area
- Place making
- Implemented public realm as multi-use destination for local community. Extended market to boost local community
- Provide office space to support economic growth and diversity

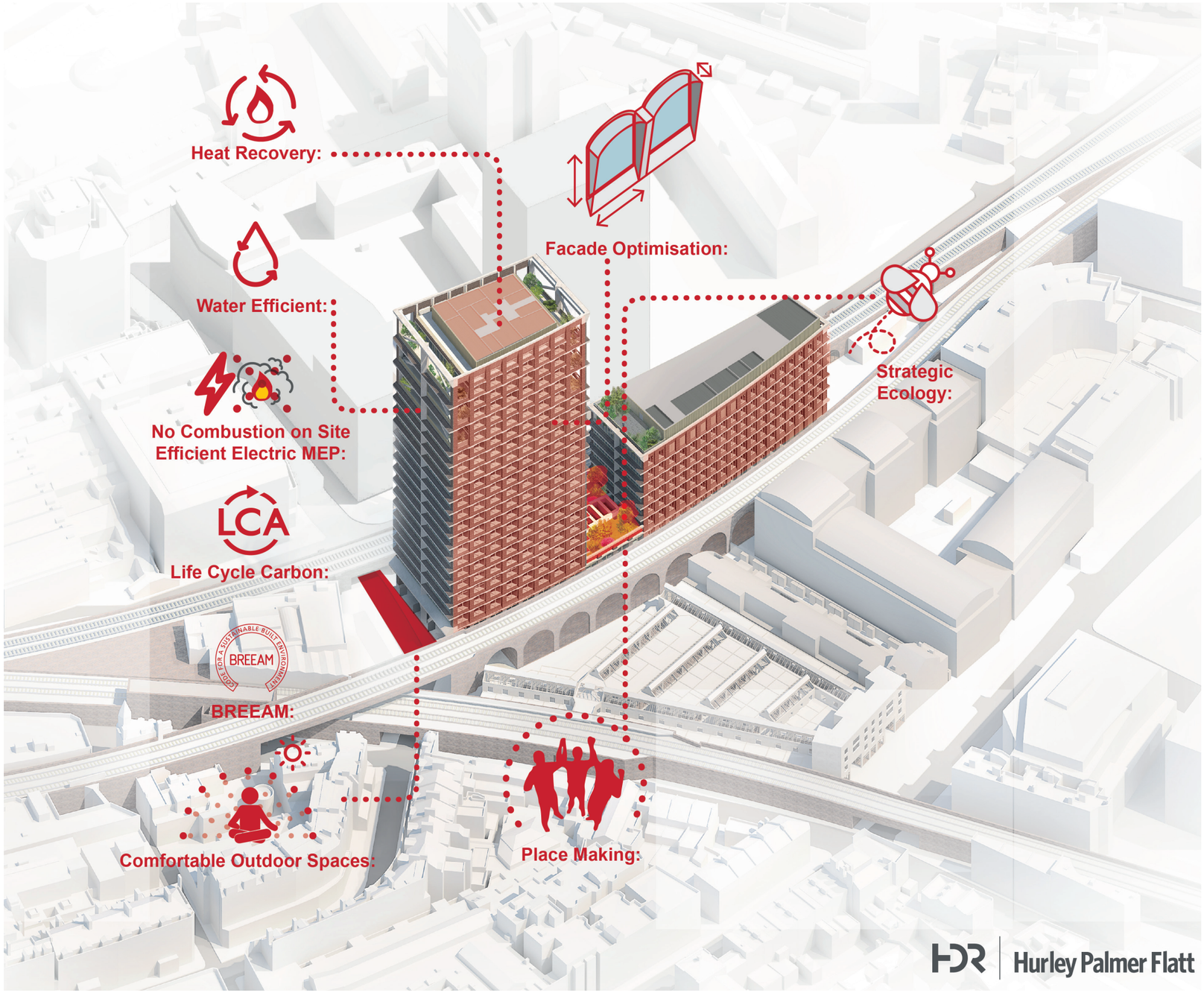


Fig. 4.12 Sustainability diagram

4.13 MAIN TOWNSCAPE VIEWS

SELECTED VIEWS

All the relevant views are considered in our THVIA, however three key views have been extrapolated

- View 05 - View from Brockwell Park
- View 13 - View from Brixton Hill - Lambeth Town Hall
- View 26 - View from Electric Avenue East

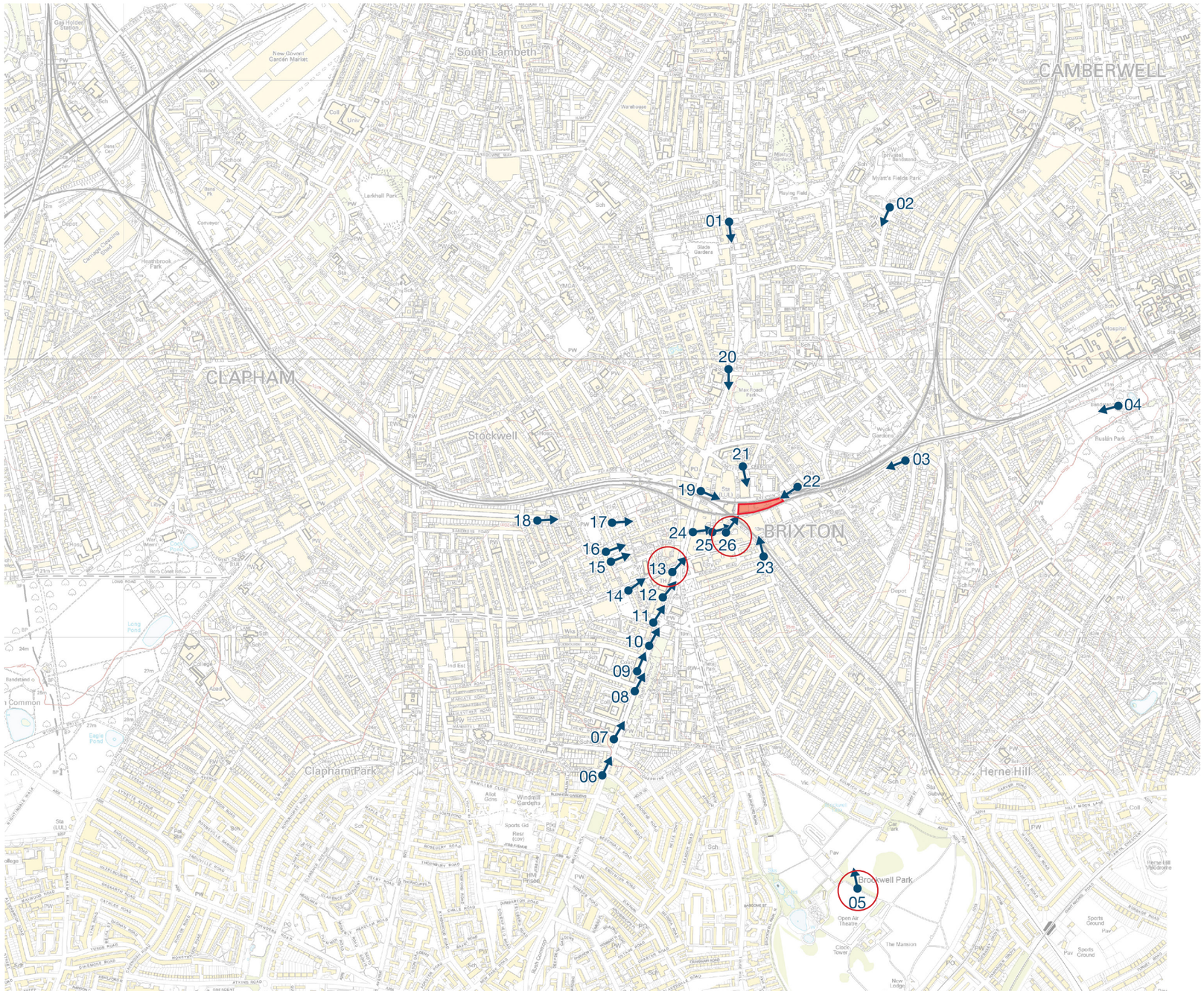


Fig. 4.13.1 Tested views location plan

IN THE SURROUNDING RESIDENTIAL AREAS, THE UPPER PARTS OF THE PROPOSED TALL BUILDING WOULD BE VISIBLE ON THE STREETS ORIENTATED TOWARDS THE SITE AND IN VIEWS ACROSS THE OPEN SPACES OF PARKS. IT WOULD PROVIDE A LOCAL LANDMARK FOR THE URBAN CENTRE OF BRIXTON JUST BEYOND THESE AREAS, AND FOR THE OVERGROUND STATION AND MARKET COMPLEX IN PARTICULAR. IT WOULD BE OF THE HIGHEST DESIGN QUALITY, APPROPRIATE TO A BUILDING WITH THIS LEVEL OF VISIBILITY WITHIN THE TOWNSCAPE.

(Text abridged from the Townscape, Built Heritage and Visual Impact Assessment by Tavernor Consultancy)

4.13 MAIN TOWNSCAPE VIEWS

VIEW 05 - VIEW FROM BROCKWELL PARK

This view is from Brockwell Park looking north, from an elevated position in the northern part of the park, with an expanse of open parkland in the foreground. The park is a Grade II Registered Park and forms the large part of the Brockwell Park Conservation Area. The Brixton Conservation Area Appraisal cites that distant views of roofscape – numerous turrets and towers – can be appreciated from Brockwell Park. The park is predominantly an open area, with a mature tree belt surrounding the perimeter, demarcating the boundary of the park and the main road beyond. In the midground of the view, but still within the park boundary, is the 'Moderne' Brockwell Lido designed by Harry Rowbotham and T. L Smithson for the LCC in 1937; it is now Grade II listed. In the distance, the view takes in a wide panorama which includes a large number of London's landmark buildings. On the left of the frame, the chimneys of Battersea Power Station are visible. Moving east, the emerging cluster of tall buildings at Vauxhall is seen, with the St George Wharf Tower and Aykon building at its current tallest points. Right of this group are the Millbank Tower and BT Tower and, closer to the viewpoint, the distinctive red top of International House in their foreground and the white top of the redeveloped Wayland House in Stockwell (now Park Heights). More distant are the new towers lining the Albert Embankment. Further right, the London Eye has a more isolated presence in this perspective, with the tops of the Shell Centre development seen right again. The South Bank Tower, One Blackfriars and tops of towers at the Elephant and Castle are also visible. St Paul's Cathedral can be discerned immediately left of the barrel-vaulted roof Meath House on Dulwich Road. Right of that building, the towers in the City form a close group beyond the Shard and Guy's Hospital. Much closer to the park, on the far right of the panorama, are two tall (c. 20 storey) residential buildings. This photograph was taken in winter, when the skyline is at its most visible.

Fig. 4.13.2 Existing view from Brockwell Park

Text from THVIA - Tavernor Consultancy

VIEW 05 - VIEW FROM BROCKWELL PARK

The proposed tall building would provide a new landmark for central Brixton on the skyline. It would largely obscure International House. It would appear at a similar height to the towers in Vauxhall, to the left, but would be evidently much closer to the viewpoint. The upper part of the building would have inset terraces with planting, softening the building's profile on the skyline and visually connecting it with the green landscape of the park. The concrete lintels would extend beyond the brickwork, articulating the building's silhouette. The top of the proposed lower building would appear to its right, partly concealing the Park Heights tower in Stockwell, adding to the rich layers of built form which already characterises the distant skyline and helping to root the taller office building to its context visually. Central Brixton will be clearly marked on the skyline with a high quality new building, and the primacy of that urban centre within its context would be reinforced, with the towers of Vauxhall and beyond clearly forming secondary features on the skyline.

Fig. 4.13.3 Proposed view from Brockwell Park

Text from THVIA - Tavernor Consultancy

4.13 MIAN TOWNSCAPE VIEWS

VIEW 13 - VIEW FROM BRIXTON HILL - LAMBETH TOWN HALL

At this northernmost point on Brixton Hill, the crossroads ahead create a pausing point for pedestrians. The breadth of the roadways and the landscaped space of Windrush Square provide an open view. Behind this point is the Grade II Listed Old Town Hall; it has a significant presence at this junction, although is out of this field of view. Distinctive corner buildings address the junction and provide this central space with a clear identity through their similar scale, materials and curvilinear elements, and through the different and particular character of each building. This is particularly so of the late Victorian corner tower with clock and copper dome at 518-522 Brixton Road on the left, the Grade II Listed Ritzy cinema overlooking Windrush Square and the Grade II Listed old Town Hall building behind this viewpoint. Directly ahead, the Moderne 467 Brixton Road (locally listed) is simpler in design and has a less articulated roofline, but the broad sweep of its red brick main frontage has a similar distinctive presence at the corner of Coldharbour Lane and Brixton Road. The mature Plane Tree in Windrush Square, on the right, also has a positive landmark presence. The traffic in the foreground dominates the viewer's experience at this junction.

Fig. 4.13.4 Existing view from Brixton Hill - Lambeth Townhall

Text from THVIA - Tavernor Consultancy

VIEW 13 - VIEW FROM BRIXTON HILL - LAMBETH TOWN HALL

The upper levels of the proposed office building would appear beyond the locally listed 467 Brixton Road and the adjacent Premier Inn. It would be legibly more distant than the foreground buildings, the distinctive corner identity of which would remain clear and would continue to characterise and dominate the view. The brick material and distinctive top of the proposed building, with generous terraces set within its frame, would introduce a new landmark element to the central Brixton skyline. It would appear related to the existing skyline character and visibly rooted in the rail viaducts, and the overground Station and Market spaces located there. The greater height of the proposed building, in relation to its context, would be noticeable at this point and its clear visibility - against the sky and beyond the horizontal parapet of 467 Brixton Road – would signal the urban character of Brixton in the 21st century and would be of a scale appropriate to the importance of its location. It would be of an appropriate exceptional design quality and situated in a location which would reinforce the legibility of the area, in terms of wayfinding and urban identity. Existing landmarks in view would remain visible and the character of the existing spaces and buildings at this main junction would continue to define the particular sense of place experienced here.

Fig. 4.13.5 Proposed view from Brixton Hill - Lambeth Townhall

Text from THVIA - Tavernor Consultancy

4.13 MAIN TOWNSCAPE VIEWS

VIEW 26 - VIEW FROM ELECTRIC AVENUE EAST

This view moves forward (east) of view 25, to the point when the road bends northwards. The viewing position is within the Brixton Conservation Area. The buildings which line Electric Avenue provide a coherent backdrop to the jumble of the street market and add to its vibrancy through the red brick and white mouldings of their upper levels. At ground level the shops and their awnings catch the eye. In the centre of the view, painted metal bridge structures indicate the rail lines which cross ahead. Pope's Road is not visible; the road name has been painted in large lettering to draw shoppers to the streets and markets further north.

Fig. 4.13.6 Existing view from Electric Avenue

Text from THVIA - Tavernor Consultancy

VIEW 26 - VIEW FROM ELECTRIC AVENUE EAST

At this point, Electric Avenue is on axis with the west end of the Site and very close to it. The proposed tall building would be clearly visible ahead. The visual texture of the design – the stepped brickwork, projecting lintels and recessed terraces towards the top – and its exceptional quality would be clearly legible. Its pale brick and concrete detailing will complement the materiality and colouration of buildings locally, and the regular rhythm and arched shape of the openings will respond to both the 19th century terraces in the foreogrund and the rail viaduct near the Site. The proportions of the proposed tall building, its angled orientation - responding to the adjacent viaducts - and the layering of rail lines at its base will provide a positive termination to the view at the same clearly separated from the foreground scene. The activity and immediacy of the street market on Electric Avenue itself would still dominate the viewer's experience. The proposed tall building would mark the continuation of that market activity on Pope's Road, and the new public space and access to the overground Station adjacent, drawing pedestrians northwards through the viaduct. This will be an exceptionally high quality building which will provide a memorable local landmark and which will clearly belong to the particular urban character of central Brixton.

Fig. 4.13.7 Proposed view from Electric Avenue

Text from THVIA - Tavernor Consultancy

ACCESS AND SERVICING

5

5.1 ACCESS TO THE BUILDING

5.1.1 PEDESTRIAN ACCESS

Main access to the building is on Pope's Road, with secondary access for servicing located on the east/west side street.

Retail

Retail main entrances are located on Pope's Road, with secondary access for servicing located on the east/west side street. Units are accessible from the internal market streets.

Office

Office lobbies are situated within the market space on both the east and west side. In order to access the lobbies, users will need to enter the market via the access points on Pope's Road.

Vehicular Access

Main vehicular access to the building is located at the service yard on Valentia place as per the servicing for the existing building.

Fig. 5.1.1 Ground floor axonometric diagram - pedestrian access

5.2 ACCESS WITHIN THE BUILDING

5.2.1 ACCESS WITHIN THE BUILDING

The building has two main cores respectively serving the west and east blocks, with two additional smaller service cores in the east block.

West Block Core includes:

- Four passenger lifts, serving all office floors
- Two escape stairs, serving all floors
- One additional lift, serving all floors

East Block Core A includes:

- Three passenger lifts, serving all office floors
- One escape stair, serving all floors

East Block Core B1 includes:

- One lift, serving all floors
- One escape stair, serving all floors

East Block Core B2 includes:

- Technical risers

Key

Main Cores

Fig. 5.2.1 Ground floor axonometric diagram - cores location

5.2 ACCESS WITHIN THE BUILDING

5.2.2 MARKET ACCESS

The market will be publicly accessible throughout opening hours

The main market access points will be located at ground floor on Pope's Road, whilst the retail spaces allocated at first floor will be accessible via a staircase sited in the publicly accessible open area at the centre of the market.

Alternatively, they can be accessed via a lift within the east block.

Fig. 5.2.2 Ground & first floor axonometric diagram - market access

5.2.3 OFFICE ACCESS

Office users will need to access the building via the main market access points located at ground floor on Pope's Road.

Two separate small office lobbies are located on the east and west side of the ground floor, in order to guarantee direct access to the cores serving the two blocks.

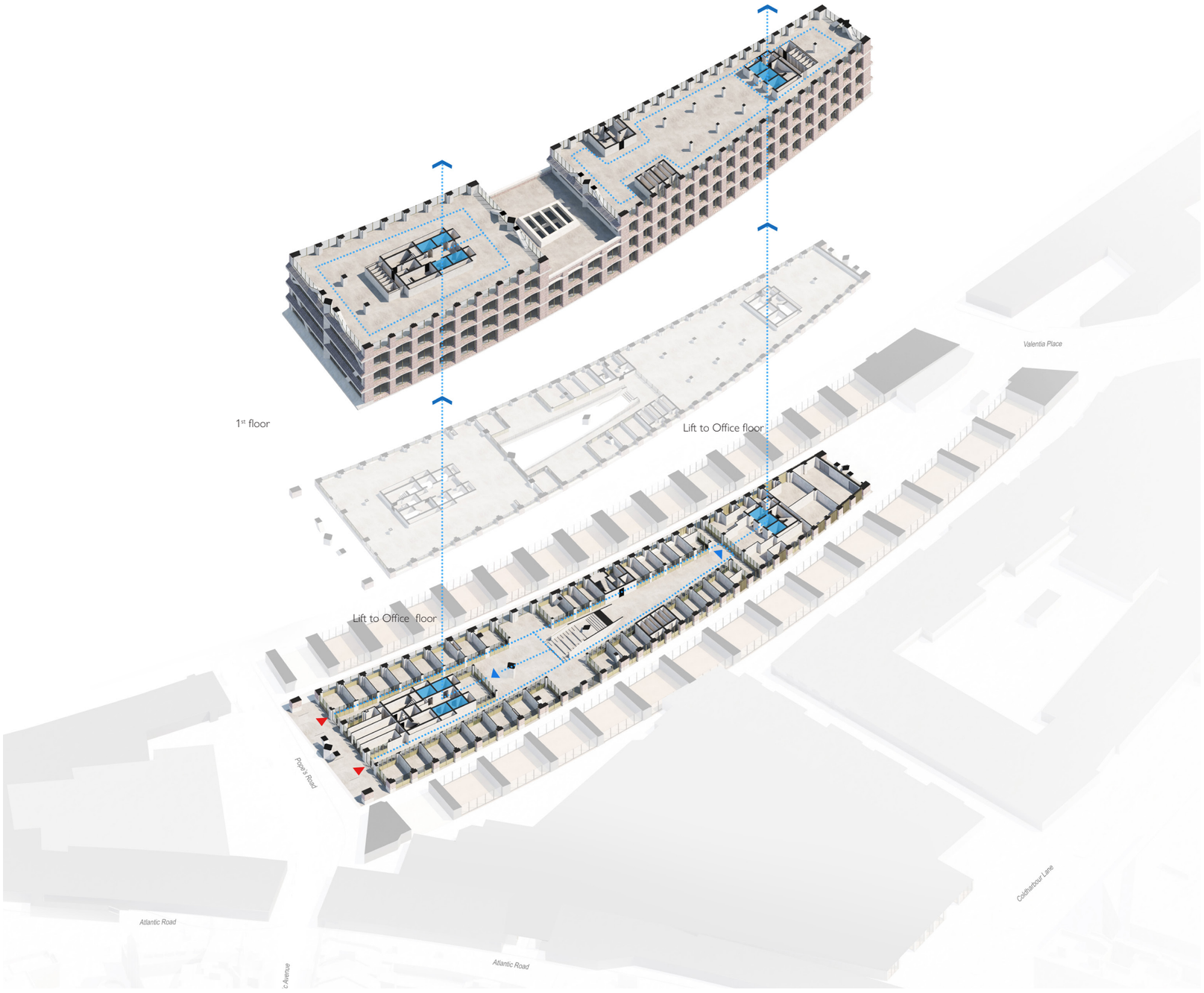


Fig. 5.2.3 West Block axonometric diagram - office access

5.3 SERVICING AND DELIVERIES

5.3.1 MARKET DELIVERIES STRATEGY

On-site deliveries to the market will be will be enabled through Pope's Road at early morning hours before the access becomes restricted to pedestrians only at 8.00am.

If, due to subsequent development, Pope's Road is rendered unusable, deliveries will be enabled through usage of the service yard, located on Valentia Place.

Deliveries to ground floor units can be carried out via the internal market streets, whilst first floor units can be served using lifts located in the east block.

Fig. 5.3.1 Ground & first floor axonometric diagram - market deliveries strategy

5.3.2 OFFICE DELIVERIES STRATEGY

Deliveries to offices will be provided throughout the day via the service yard located on Valentia Place. The deliveries can then be distributed to the different office floors using the lifts located respectively in the West Block Core and in the East Block Core A.

The lifts in these cores are shared passenger/goods lifts. Larger goods deliveries will be timed according to offices' management strategy to ensure lifts usage is properly scheduled and managed.

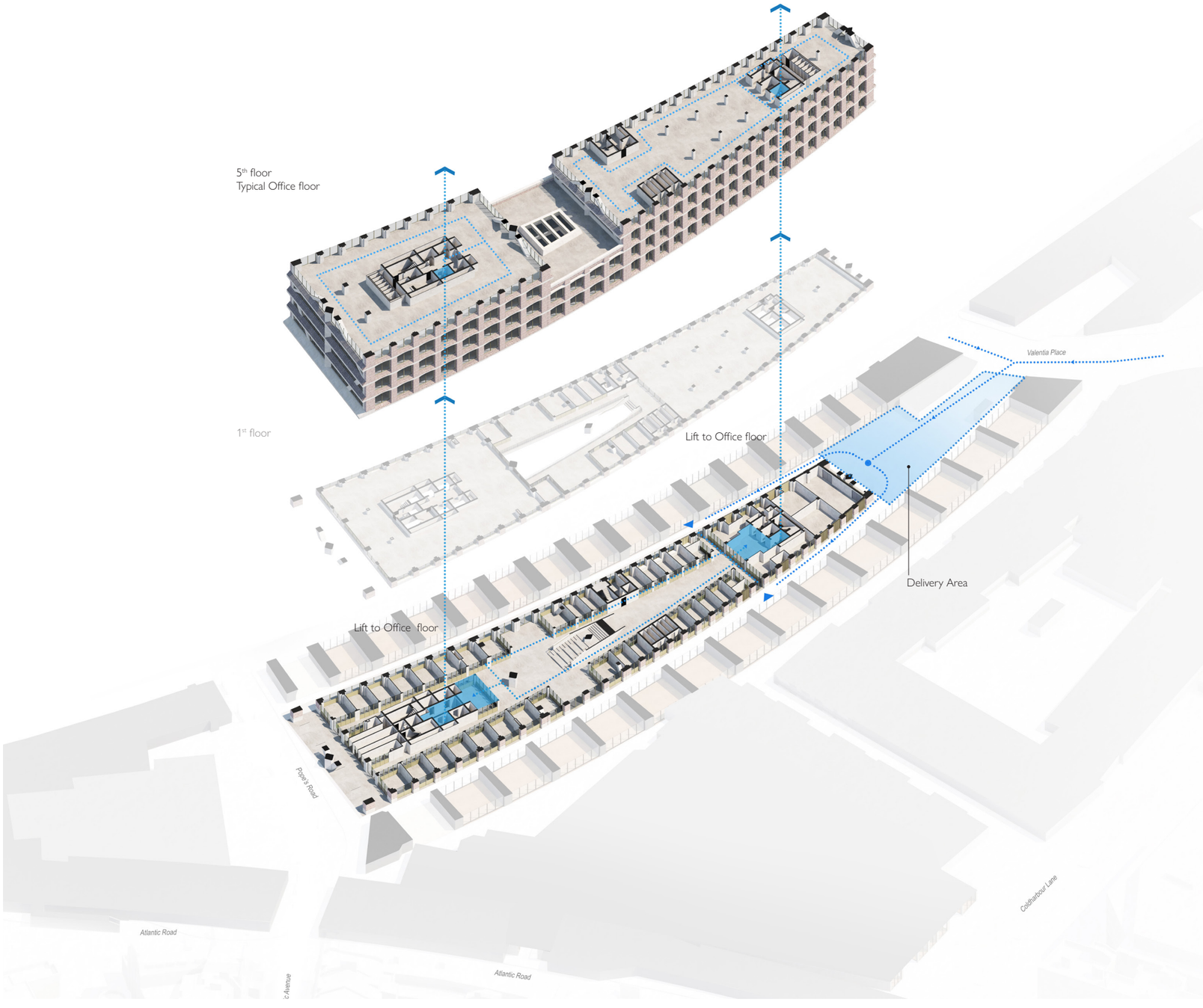


Fig. 5.3.2 Ground & first floor axonometric diagram - office deliveries strategy

5.3 SERVICING AND DELIVERIES

5.3.3 MARKET REFUSE/ WASTE STRATEGY

Retail units will have their own self-contained waste storage in a dedicated bin storage room located on the eastern side of the ground floor, directly facing Valentia Place.

Waste will be collected in the market area at several times during the day by dedicated personnel, it will be brought down to the ground floor via a goods lift located east block, and collected within the bin storage room.

Valentia Place acts as a dedicated service yard where waste will be collected during specific time slots throughout the day.

Fig. 5.3.3 Ground & first floor axonometric diagram - market refuse / waste strategy

5.3.4 OFFICE REFUSE/ WASTE STRATEGY

Office waste and recycling will be collected on each floor of the east and west blocks and brought down to ground floor using the goods lift present in each respective core.

From there, office management will then transfer waste and re-cycling materials to the bin storage room located on the eastern side of the ground floor, directly facing Valentia Place.

Collection will then be made directly from Valentia Place during specified time slots throughout the day.

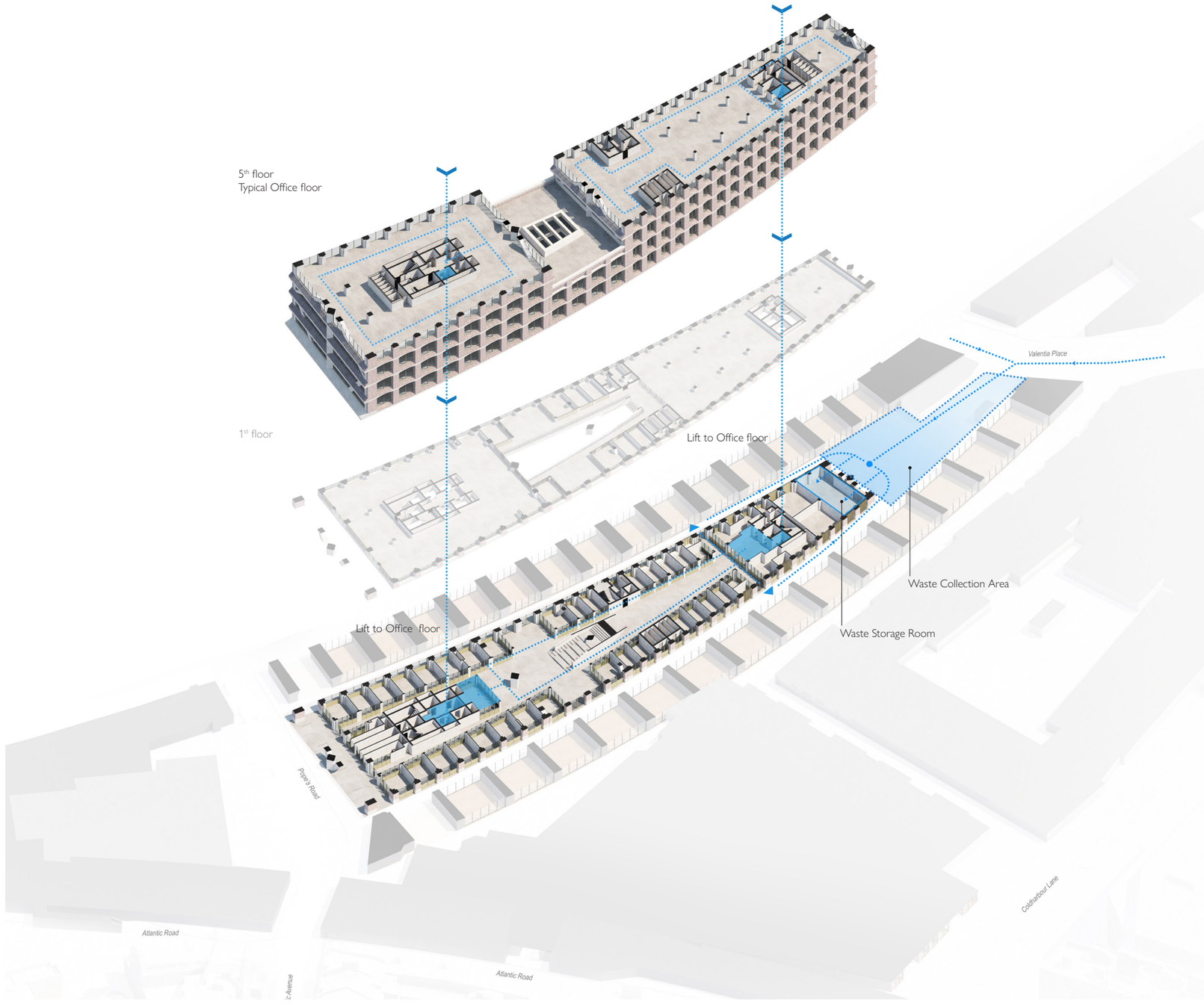


Fig. 5.3.4 Ground & first floor axonometric diagram - office refuse / waste strategy

5.4 BICYCLE PARKING

5.4.1 BICYCLE PARKING

Bicycle parking provisions will be provided at ground floor level and at basement 01 level.

At ground level, short stay bicycle parking spaces will be provided below the first arch of the railway to the north. All other bicycle parking provisions are located a basement 01 level in two dedicated rooms, which can be accessed directly from Pope's Road via a lift located in the west block core.

Locker and shower facilities are also located at basement 01 level, directly accessible from the bicycle parking rooms.

Fig. 5.4.1 Ground & basements axonometric diagram - office & market bicycle parking

5.5 PUBLIC TOILET ACCESS

5.5.1 PUBLIC TOILET ACCESS

Our proposal will provide accessible and well maintained toilet provision to the public free of charge.

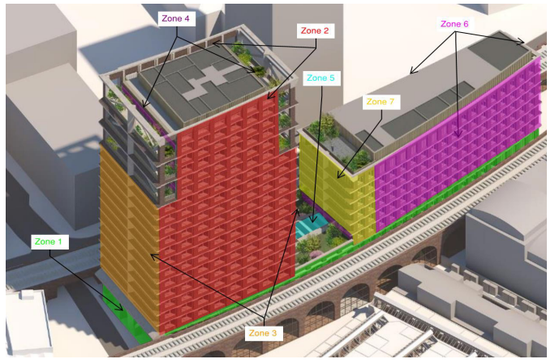
The new toilet provision will be located at basement 01 level and will be directly accessible from Pope's Road via a lift located in the west block core.

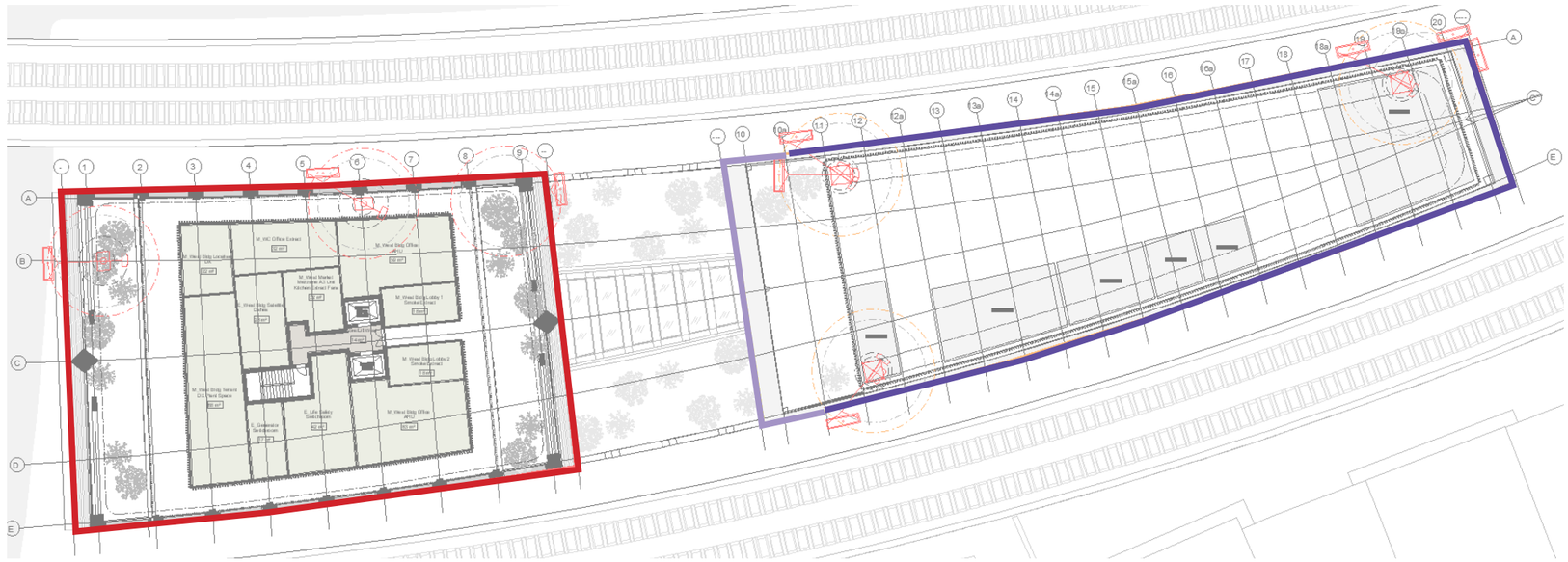
Fig. 5.5.1 Ground & basements axonometric diagram - public toilet access

5.6 FACADE ACCESS AND MAINTENANCE STRATEGY

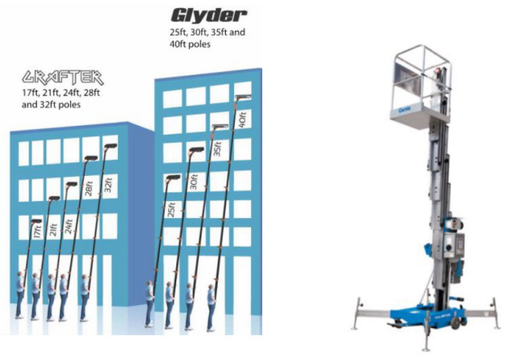
The building has different facade conditions which require different equipments and strategies for the access and maintainance of all of them.

Below are the different zones of the facade highlighted and the different strategies explained in the subsequent diagrams.



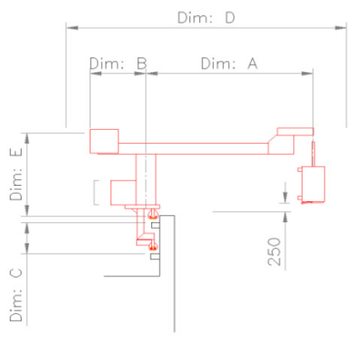


ZONE 1, 4, 5



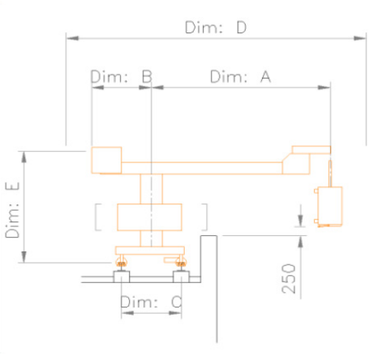
Direct access from ground. Direct access to the adjacent façades and where required, use of extended cleaning tools, such as water-fed poles. AWP, small MEWP or towers for where hands on access is required.
L14-19 Balconies: Access direct from floor level, max 12m based on a 5:1 ratio.
Internal Skylight: Access from ground by small MEWP.

ZONE 2,3



BMU system at west block roof level, parapet mounted, with a reach of 4.5m and cradle for cleaning and light maintenance works. BMU to park as low and discreet as possible when not in use. BMU allows full access to the West Block facades , except L14 L19 terraces/balcony on the east & west elevations.

ZONE 6,7



BMU system at east block roof level, track mounted, with a reach of 5m and cradle for cleaning and light maintenance works. BMU to park as low and discreet as possible when not in use. BMU Allows full access to the East Block facades, except the east west elevation.

Fig. 5.6 Facade Maintenance Strategy diagram - office & market bicycle parking

CONCLUSION

6

Our proposal consolidates extensive research and understanding of Brixton and its history. This draws from its past as a prosperous and innovative urban location with the introduction transport infrastructures, to the developments that still value Brixton as a centre for regenerative growth with mixed retail, music, leisure and cultural facilities today.

The distinctive culture and heritage that has become emblematic in defining Brixton's vibrant characteristics are continuously celebrated. The unique community is recognised by growing numbers of visitors the existing economy attracts.

Pope's Road is as an opportunity to collectively think about Brixton's present context alongside its ongoing and sustainable growth. The design proactively engages with this momentum, in line with the Borough's regeneration plan. It supports their ambition to mark Brixton as a leading place for growth and economic prosperity as a benefit for the local and wider community.

In recognising the advantages of local investment and by retaining Brixton's own assets, our response integrates community qualities by embedding them first hand as part of the project. The marketplace extension and new office ecosystem in return will provide a flexible 20,0000 sqft work space for small, medium and large enterprises. This also aims to encourage and support a range of local employment opportunities and create an estimated 2,000 new jobs in various creative, tech and cultural sectors.

In conjunction, this will enable £2,000,000 in available business rates, with £3,400,000 of extra investment spent per year by the local authority. The increase of both an employment and community centric space, will expand Brixton's legacy and facilitate a new and publicly shared area for mixed commercial and civic use; participation and activity.

Our design explores the principal of an improved and public accessible environment by maximising porosity. To give ease to navigation, a user friendly and enhanced space will increase permeability and open up a new public realm in the vicinity of Brixton Station. This will also create a welcoming and safer neighbourhood for all with natural surveillance heightened. The addition of a rooftop restaurant for Brixton further adds to its rich destination and visitor experience.

This proposal has been strategically developed in consultation with relevant stakeholders. We are committed and confident that the new Pope's Road and mixed-use development, consolidates local investment, acknowledges positive change and creates new and locally derived opportunities for the future of Brixton and its wider community.

Fig.6.1 View of the Publicly Accessible Open Space

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